



**Resources Department
Town Hall, Upper Street, London, N1 2UD**

AGENDA FOR THE PLANNING COMMITTEE

Members of Planning Committee are summoned to a meeting, which will be held in the Council Chamber, Town Hall, Upper Street, N1 2UD on **6 December 2022 at 7.30 pm.**

Enquiries to : Zoe Lewis
Tel : 020 7527 3486
E-mail : democracy@islington.gov.uk
Despatched : 28 November 2022

Welcome:

Members of the public are welcome to attend this meeting.

Consideration of Planning Applications – This is a formal agenda where decisions are taken on planning applications submitted to the Council. Public speaking rights on these items are limited to those wishing to comment on specific applications. **If you wish to speak at the meeting please register by calling the Planning Department on 020 7527 2278 or emailing enquiriesplanning@islington.gov.uk.**

<u>Committee Membership</u>	<u>Wards</u>	<u>Substitute Members</u>
Councillor Klute (Chair) Canalside;	- St Peter's and	Councillor Chowdhury - Barnsbury;
Councillor North (Vice-Chair) Canalside;	- St Peter's and	Councillor Gilgunn - Tollington;
Councillor Poyser (Vice-Chair)	- Hillrise;	Councillor Jegorovas-Armstrong - Highbury;
Councillor Clarke	- Tufnell Park;	Councillor Kay - Midmay;
Councillor Convery	- Caledonian;	
Councillor Hamdache	- Highbury;	
Councillor Hayes	- Clerkenwell;	
Councillor Ibrahim	- Arsenal;	
Councillor Jackson	- Holloway;	
Councillor McHugh James';	- St Mary's and St	

Quorum: 3 councillors



A. Formal Matters

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1. Introductions
2. Apologies for Absence
3. Declarations of Substitute Members
4. Declarations of Interest

If you have a **Disclosable Pecuniary Interest*** in an item of business:

- if it is not yet on the council's register, you **must** declare both the existence and details of it at the start of the meeting or when it becomes apparent;
- you may **choose** to declare a Disclosable Pecuniary Interest that is already in the register in the interests of openness and transparency.

In both the above cases, you **must** leave the room without participating in discussion of the item.

If you have a **personal** interest in an item of business **and** you intend to speak or vote on the item you **must** declare both the existence and details of it at the start of the meeting or when it becomes apparent but you **may** participate in the discussion and vote on the item.

- *(a) **Employment, etc** - Any employment, office, trade, profession or vocation carried on for profit or gain.
- (b) **Sponsorship** - Any payment or other financial benefit in respect of your expenses in carrying out duties as a member, or of your election; including from a trade union.
- (c) **Contracts** - Any current contract for goods, services or works, between you or your partner (or a body in which one of you has a beneficial interest) and the council.
- (d) **Land** - Any beneficial interest in land which is within the council's area.
- (e) **Licences** - Any licence to occupy land in the council's area for a month or longer.
- (f) **Corporate tenancies** - Any tenancy between the council and a body in which you or your partner have a beneficial interest.
- (g) **Securities** - Any beneficial interest in securities of a body which has a place of business or land in the council's area, if the total nominal value of the securities exceeds £25,000 or one hundredth of the total issued share capital of that body or of any one class of its issued share capital.

This applies to **all** members present at the meeting.

5. Order of Business
6. Minutes of Previous Meeting

To
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1.	Old Barnsbury, The Barnsbury Estate, N1 0TJ	3 - 56
2.	Vorley Road Bus Stand and 4 Vorley Road, London, N19 5JH	57 - 190
3.	14-22 Vale Royal and 184 York Way, London, N7 9AD	191 - 276

C. Consideration of other planning matters

D. Urgent non-exempt items (if any)

Any non-exempt items which the Chair is of the opinion should be considered as a matter of urgency and to consider whether the special circumstances included in the report as to why it was not included on and circulated with the agenda are acceptable for recording in the minutes.

E. Exclusion of press and public

To consider whether, in view of the nature of the remaining items on the agenda, it is likely to involve the disclosure of exempt or confidential information within the terms of the Access to Information Procedure Rules in the Constitution and, if so, whether to exclude the press and public during discussion thereof.

F. Confidential/exempt items

G. Urgent exempt items (if any)

Any exempt items which the Chair agrees should be considered urgently by reason of special circumstances. The reasons for urgency will be agreed by the Chair and recorded in the minutes.

Date of Next Meeting: Planning Committee, 10 January 2023

Please note all committee agendas, reports and minutes are available on the council's website: www.democracy.islington.gov.uk

WEBCASTING NOTICE

This meeting will be filmed by the Council for live and/or subsequent broadcast on the Council's website. The whole of the meeting will be filmed, except where there are confidential or exempt items, and the footage will be on the website for 12 months. A copy of it will also be retained in accordance with the Council's data retention policy.

If you participate in the meeting you will be deemed by the Council to have consented to being filmed. By entering the Council Chamber you are also consenting to being filmed and to the possible use of those images and sound recordings for webcasting and/or training purposes. If

you do not wish to have your image captured you should sit in the public gallery area, overlooking the Chamber.

In addition, the Council is obliged by law to allow members of the public to take photographs, film, audio-record, and report on the proceedings at public meetings. The Council will only seek to prevent this should it be undertaken in a disruptive or otherwise inappropriate manner.

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PROCEDURES FOR PLANNING COMMITTEE

Planning Committee Membership

The Planning Committee consists of ten locally elected members of the council who will decide on the applications for planning permission.

Order of Agenda

The Chair of the Planning Committee has discretion to bring forward items, or vary the order of the agenda, where there is a lot of public interest.

Consideration of the Application

After hearing from council officers about the main issues of the proposal and any information additional to the written report, the Chair will invite those objectors who have registered to speak for up to three minutes on any point relevant to the application. If more than one objector is present for any application then the Chair may request that a spokesperson should speak on behalf of all the objectors. The spokesperson should be selected before the meeting begins. The applicant will then be invited to address the meeting also for three minutes. These arrangements may be varied at the Chair's discretion.

Members of the Planning Committee will then discuss and vote to decide the application. The drawings forming the application are available for inspection by members during the discussion.

Please note that the Planning Committee will not be in a position to consider any additional material (e.g. further letters, plans, diagrams etc.) presented on that evening. Should you wish to provide any such information, please send this to the case officer a minimum of 24 hours before the meeting. If you submitted an objection but now feel that revisions or clarifications have addressed your earlier concerns, please write to inform us as soon as possible.

What Are Relevant Planning Objections?

The Planning Committee is required to decide on planning applications in accordance with the policies in the Development Plan unless there are compelling other reasons. The officer's report to the Planning Committee will refer to the relevant policies and evaluate the application against these policies. Loss of light, openness or privacy, disturbance to neighbouring properties from proposed intrusive uses, over development or the impact of proposed development in terms of size, scale, design or character on other buildings in the area, are relevant grounds for objection. Loss of property value, disturbance during building works and competition with existing uses are not. Loss of view is not a relevant ground for objection, however an unacceptable increase in sense of enclosure is.

For further information on how the Planning Committee operates and how to put your views to the Planning Committee please call Zoe Lewis on 020 7527 3486. If you wish to speak at the meeting please register by calling the Planning Department on 020 7527 2278 or emailing enquiriesplanning@islington.gov.uk.

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COMMITTEE AGENDA

1 14 - 22 Vale Royal and 184 York Way

London
N7 9AD

2 Old Barnsbury, The Barnsbury Estate

N1 0TJ

3 Vorley Road Bus Stand & 4 Vorley Road, London N19 5JH

1 14 - 22 Vale Royal and 184 York Way

London
N7 9AD

Application Number: P2022/1847/FUL

Ward: Caledonian

Proposed Development: Alterations, extensions and partial change of use to create mixed use (Classes B8 and E(g)) buildings arranged over ground, first, second, part third, part fourth and part fifth floors and associated plant delivered in two phases

Application Type: Full Planning Application

Case Officer: Thomas Broomhall

Name of Applicant: Access Self-Storage

Recommendation:

2 Old Barnsbury, The Barnsbury Estate

N1 0TJ

Application Number: P2022/2454/FUL

Ward: Barnsbury

Proposed Development: Full planning application for external alterations to all existing buildings (including replacement windows, doors and communal entrances) and landscaping, environmental and wayfinding works (including new bin and cycle stores, new railings to ramps, stepped access to existing buildings, play facilities, lighting, signage and gates).

Application Type: Full Planning Application

Case Officer: Nicholas Linford

Name of Applicant: N/A

Recommendation:

3 Vorley Road Bus Stand & 4 Vorley Road, London N19 5JH

Application Number: P2022/1221/FUL

Ward: Junction

Proposed Development: Demolition of existing part 1, part 2 storey community use building; the construction of 72 new dwellings with associated private amenity space for affordable and private homes, provided in three blocks of 13 storeys, 8 storeys, and 2 to 6 storeys, improvements to the public realm, the provision of a Medical Centre and a library, associated bicycle parking, mobility scooter stores, refuse stores and improvements to the public realm

RE-CONSULTATION: THE APPLICATION REPRESENTS A DEPARTURE FROM THE DEVELOPMENT PLAN

Application Type: Full Planning Application

Case Officer: Brett McAllister

Name of Applicant: L B Islington

Recommendation:

PLANNING COMMITTEE REPORT

Development Management Service
 Planning and Development Division
 Community Wealth Building Department

PLANNING COMMITTEE		AGENDA ITEM
Date:	6 December 2022	

Application number	P2022/2454/FUL
Application type	Full Planning Application
Ward	Barnsbury
Listed building	Adjacent to Grade II Listed 16-62 Barnsbury Road
Conservation area	N/A
Development Plan Context	Within 50m of Barnsbury Conservation Area Article 4(2) Barnsbury Conservation Area Local Cycle Route Major Cycle Route Local view from Archway Road Local view from Archway Bridge Article 4 Direction (A1-A2) Rest of Borough Rail land ownership – TfL Tunnels London Underground Zones of Interest
Licensing Implications	N/A
Site Address	Old Barnsbury, The Barnsbury Estate, N1 0TJ
Proposal	Full planning application for external alterations to all existing buildings (including replacement windows, doors and communal entrances) and landscaping, environmental and wayfinding works (including new bin and cycle stores, new railings to ramps, stepped access to existing buildings, play facilities, lighting, signage and gates).

Case Officer	Nicholas Linford
Applicant	Newlon Housing Association
Agent	Lichfields – Anna Vincent

1. RECOMENDATION

The Committee is asked to resolve to **GRANT** planning permission subject to the conditions set out in Appendix 1.

2. SITE PLAN (site highlighted in red)

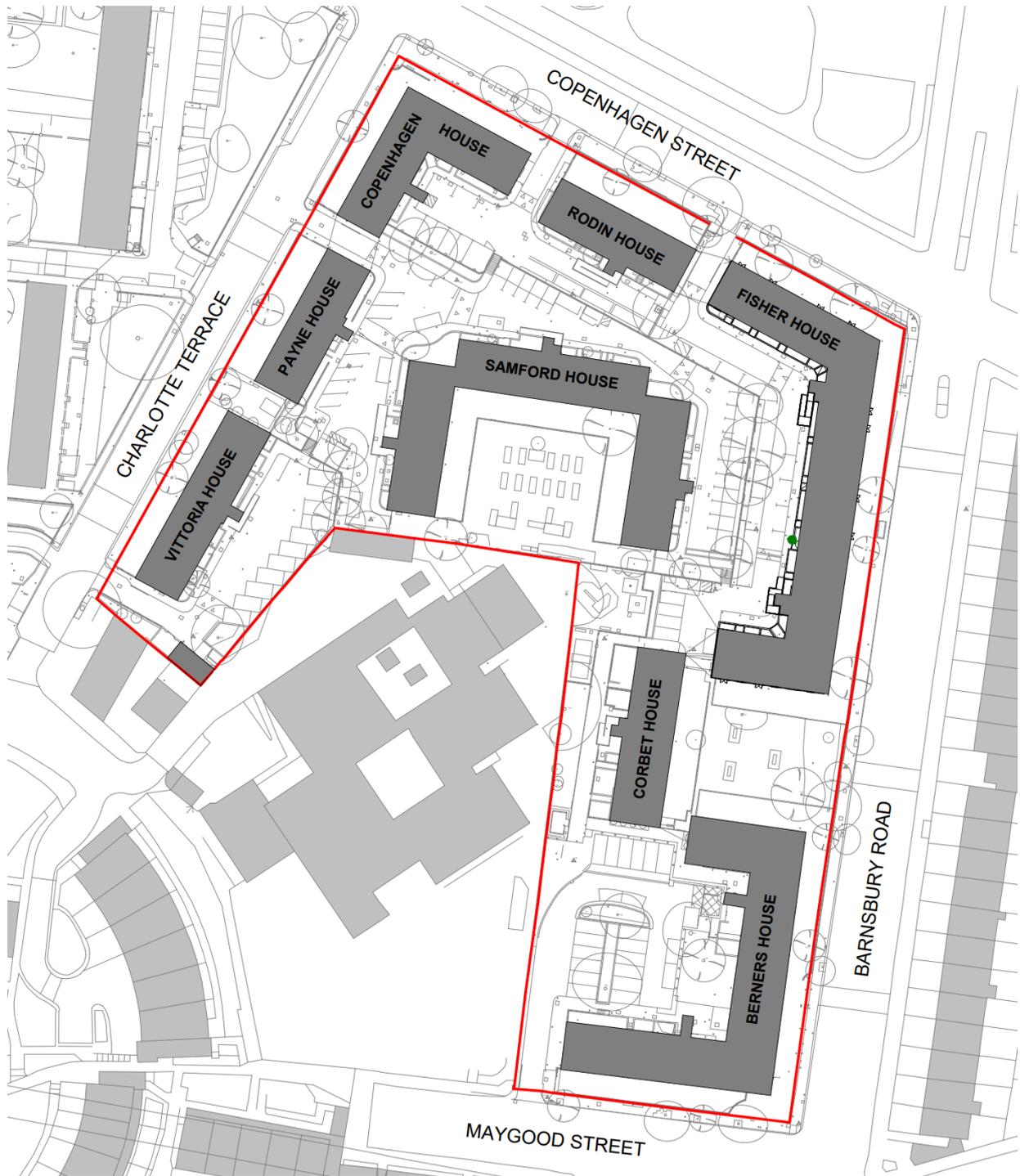


Image 1: Site plan showing block names

3. PHOTOS OF SITE



Image 2: Aerial image of the site, looking North



Image 3: Old Barnsbury as seen from Copenhagen Street



Image 4: Old Barnsbury as seen from Barnsbury Road



Image 5: Old Barnsbury as seen from within the estate



Image 6: Old Barnsbury as seen from within the estate

4. SUMMARY

- 4.1 The proposal seeks planning permission for external alterations to all buildings and landscaping throughout the Old Barnsbury Estate.
- 4.2 The external works include the replacement of all windows with new double-glazed windows, the replacement of all front doors, and the creation of new communal entrances with improved accessibility to each block on the estate.
- 4.3 The landscaping improvements include new hard and soft landscaping, upgraded and expanded play space, new bin and cycle stores, and the enhancement of existing pedestrian routes through the estate with associated wayfinding improvements. Improvements to the lighting scheme, new boundary treatments and security upgrades are also proposed. A reduction in the number of car parking spaces on the estate will allow for increased greening of the landscape.
- 4.4 Following consultation with residents, stakeholders and the Local Planning Authority, the applicant held a residents ballot in February 2021. The ballot related to a vision for the whole estate, being Old and New Barnsbury, with a Landlord Offer to residents. The ballot proposals included the redevelopment of New Barnsbury and upgrade/refurbishment works to Old Barnsbury. The ballot included the resident of both Old and New Barnsbury, returning an endorsement of 79% in favour of the proposals on a 72% turnout.

- 4.5 A separate planning application (ref: P2022/1898/FUL) has been submitted for the part of the Estate known as the New Barnsbury Estate. For timing reasons, the applications for Old and New Barnsbury are not formally linked through the planning process and the Planning Committee will be asked to make a decision on the New Barnsbury planning application at an earlier date (currently scheduled for 29th November 2022).
- 4.6 The proposals would provide for more amenable and efficient homes as a result of the new fenestration. Additionally, the fenestration proposals would bring a level of design coherence to the estate that has not been seen since the original windows were replaced.
- 4.7 The application also delivers a number of benefits, including landscape improvements and a rationalisation of car parking. Additionally, the application is considered to constitute a sustainable form of development due to the improvements it would provide in energy efficiency, green infrastructure and sustainable transport.
- 4.8 The proposals would not alter the form, scale, character, or appearance of the existing buildings on the Old Barnsbury Estate. The proposals would also not change the number of homes on the estate, the mix of unit sizes or tenures. The proposal is therefore not considered to have an unacceptable impact on neighbouring residential amenity in terms of loss of daylight, sunlight, outlook, privacy, noise and disturbance or an increased sense of enclosure.
- 4.9 For the reasons given above and explained in more detail in the subsequent sections of this report, the proposal is considered to be acceptable and in accordance with relevant planning policy and is therefore recommended for approval subject to conditions.

5. SITE AND SURROUNDINGS

- 5.1 The application site relates to the Old Barnsbury Estate which is contained within an area bounded by Charlotte Terrace to the west, Copenhagen Street to the north and Barnsbury Road to the east. The southern boundary is formed of both Vittoria Primary School and Maygood Street.
- 5.2 To the west of Charlotte Terrace is New Barnsbury which is a related area of the estate but does not fall within the scope of this planning application. The Old Barnsbury Estate is approximately 800m from its closest point at Charlotte Terrace to Kings Cross Station, approximately 750m from its closest point at the junction of Charlotte Terrace and Copenhagen Street to Caledonian Road and Barnsbury Station, and approximately 420m from its closest point at the junction of Barnsbury Road and the Maygood Street to Angel Station.
- 5.3 Built in the 1930s, the Old Barnsbury Estate contains 275 homes in 8 x five storey red brick mansion blocks (Berners House, Corbet House, Fisher House, Samford House, Roding House, Copenhagen House, Payne House, and Vittoria House), which face inwards rather than address the street – the result of communal front gardens being converted into private gardens for ground floor units sometime after the estate was originally built.
- 5.4 The communal outside space on the Old Barnsbury Estate is predominantly given over to car parking and other areas of hard landscaping, with much of the estate's hard surfacing being uneven and in need of repairs. Additionally, across the estate there are exposed refuse bins and cycle parking which lead to a cluttered appearance. Where landscaping has been provided, it is often unusable and used to separate areas rather than to provide amenity space.
- 5.5 There are two existing areas of green space within Old Barnsbury, one which fronts Barnsbury Road and is overlooked by Berners, Corbet and Fisher House. The other is to the rear of Samford House and provides community allotment space. Both green spaces provide limited access, with only those allocated a key able to sue them.

- 5.6 The play space provision on Old Barnsbury is undersized and is solely for the use of younger children. The play space is in an area of the estate which does not benefit from large amounts of natural surveillance and being split in to two areas, it is difficult to supervise. The existing play equipment is in a poor state and in need of replacement.
- 5.7 Old Barnsbury Estate suffers from considerable land level changes, resulting in stairs and convoluted ramps being provided to make the estate somewhat accessible. These levels complicate the layout and functionality of the estate and contribute to the estate's security and accessibility issues.
- 5.8 Old Barnsbury includes access points along its western, northern and southern boundaries, with five access routes into the estate. Two access routes lead off Charlotte Terrace, one runs in between Vittoria House and the former Public House and the other running between Copenhagen and Payne House. The two access routes along Copenhagen Street run either side of Roding House, however once inside the estate there is no connection for vehicles between the two due to level changes. There is one access route via Maygood Street which is the most direct route into the estate for residents travelling along Barnsbury Road or from the Angel. Existing site levels across Old Barnsbury make pedestrian routes challenging from an accessibility perspective. Typically, level access to blocks is only achievable via one access route, with other routes featuring steps or convoluted journeys. From a safety and security perspective poor lighting, lack of CCTV and little to no natural surveillance make some existing routes uninviting places to walk. In addition to level changes, poor wayfinding signage makes the estate difficult to navigate.
- 5.9 The surrounding townscape is predominantly residential but diverse in form. To the north and west, the estate is bounded by the Barnsbury Conservation Area, with four storey Grade II listed Georgian terraces to the west of the estate on Barnsbury Road and Barnard Park to the north of the estate. To the west of the estate is the New Barnsbury Estate, where four to eight storey buildings face on to Old Barnsbury. To the South of the estate is the two storey Vittoria Primary School, a 1960s building, and the two storey job centre building which also sits within the Barnsbury Conservation Area.

6. PROPOSAL (IN DETAIL)

- 6.1 The application seeks planning permission for the refurbishment of the Old Barnsbury Estate. These refurbishment works include external alterations to all buildings on the estate, along with various landscaping improvements.
- 6.2 The proposed external alterations include the replacement of all windows with new double-glazed windows, the replacement of all front doors, and the creation of new communal entrances with improved accessibility to each block on the estate.
- 6.3 A number of landscaping changes are proposed, and these are listed below:
- Pedestrian improvements, including the enhancement of existing routes to provide an accessible route through the estate;
 - General greening of the landscaping, including the removal of some car parking spaces to provide new planting, upgrades to Barnsbury Road Park, and the creation of a shared courtyard space next to Berners House;
 - New paving and seating areas;
 - New bin and cycle stores;
 - Improvements to play facilities;
 - Improvements to lighting scheme;
 - Accessibility improvements, including a new accessible ramp and the provision of new railings;

- New signage and wayfinding improvements;
- New gates with fob access, and;
- New boundary treatments.

6.4 The proposed development is programmed to be phased over a period of 6 years. The proposed works will not alter the form, scale, character, or appearance of the existing buildings, nor will they change the mix of unit sizes or tenure at Old Barnsbury.

6.5 In addition to the external alterations and landscaping improvements, the applicant intends to provide internal improvements to the flats on the estate, including new kitchens and bathrooms, and also to carry out repair works to the roofs, brickwork, and access decks where needed. While these did form part of the Landlord Offer to residents as part of the Ballot, these works do not require planning permission and therefore do not form part of this application. However, it is understood that the applicant proposes to carry these works out alongside the works which they are seeking consent for.

Amendments

6.6 During the application process a number of amendments to the scheme were submitted, including:

- The applicant previously proposed the creation of a new green walkway providing a publicly accessible pedestrian route from Barnsbury Road, through Old Barnsbury, to the New Barnsbury Estate. This proposal was removed in response to a petition which raised concerns regarding security; and
- The applicant's original boundary treatment proposals involved the installation of 1.5 meter-high fences fronting on to the external streets to the site. These proposals were revised in response to comments from the Council's Design Officer and now involve the retention of the existing iron fences and the planting of new defensible hedges behind these.

7. RELEVANT HISTORY:

7.1 This section sets out relevant planning applications that have been considered by the Council within and adjoining the Barnsbury Estate. Applications relating to the retail units within Old Barnsbury, as well as applications for advertisement consent, tree works in a conservation area, and street furniture have not been included.

TP Ref	Address	Proposal	Outcome	Date
831934	Barnsbury Estate, London, N1	The carrying out of landscaping and other rehabilitation works to the open areas around the blocks and the alteration of the ground floor communal entrance refuse storage areas.	Approved with conditions	23/03/1984
861114	Vittoria House, Barnsbury Estate, London, N1	Erection of dry goods/lumber store.	Approved with conditions	10/11/1986

981749	Barnsbury Estate, London, N1	Construction of 12 no. lift shafts at the eight blocks of Old Barnsbury Estate.	Approved with conditions	20/10/1998
P000032	Barnsbury Estate, London, N1	OLD BARNSBURY ESTATE: Creation of balconies at Berners House, Copenhagen House, Corbet House, Fisher House, Payne House, Roding House, Samford House & Vittoria House. NEW BARNSBURY ESTATE: Formation of pitched roofs at Adrian House, Aldrick House, Amory House, 1-19 Charlotte Terrace, Crispe House, Ewen House, Jocelin House, Kenwick House, Mavor House, Redmond House, Ritson House, Thorpe House, Thurston House & Venn House.	Approved with conditions	22/08/2000
P030160	Barnsbury Estate, London, N1	Installation of a timber security fencing 1.6m high to estate boundary	Approved with conditions	17/04/2003
P2012/0289/FUL	Carriageway [outside Molton House] at Charlotte Terrace, London N1 0JJ	Installation on the carriageway of a Barclays Cycle Hire docking station, containing a maximum of 58 docking points for scheme cycles plus a terminal.	Approved with conditions	28/01/2013
P2021/3658/FUL	Barnard Park	Refurbishment and improvement works to park to include: replacement of existing Redgra pitch with 9-a-side pitch and associated retaining walls; creation of new grass area for amenity recreation and sports use; removal of existing park buildings and erection of new community hub building; new and reconfigured entrances and footpaths; installation of adult gym equipment; park furniture; removal and	Approved with conditions	15/03/2022

		replacement of trees; and landscaping throughout.		
P2022/1898/FUL	Barnsbury Estate, London, N1	<p>Hybrid planning application seeking detailed permission for Phase 1a, 1b and 3a and outline planning permission for all future phases 1c, 2a, 2b and 3b comprising:</p> <p>In full, demolition of existing buildings and undertaking of groundworks and the erection of 427 residential units (Use Class C3) within seven buildings of 3, 7, 9, 11, 12 and 13 storeys comprising 148 x 1 bed, 203 x 2 bed, 58 x 3 bed, 13 x 4 bed and 5 x 5 bed over 50672sq.m floorspace in phases 1a, 1b and 3a; hard and soft landscaping works including public open space and new public realm, access and highway formation and alterations, car and cycle provision with associated ancillary works.</p> <p>Outline planning permission (all matters reserved) for the demolition of all remaining buildings, undertaking of ground works and the construction of a phased redevelopment of new buildings of up to 9 storeys and incorporating up to 76100sq.m to include up to 523 residential units (Use Class C3), up to 1500sq.m of replacement retail and town centre floorspace (Use Class E), and 1275sq.m of replacement community use floorspace including community centre and nursery (Use Class F2), within phases 1c, 2a, 2b and 3b, hard and soft landscaping, new public open space, access and highway formation and alterations, new pedestrian</p>	Pending consideration	

		means of access, car and cycle parking provision and ancillary works.		
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8. CONSULTATION

Public Consultation

- 8.1 Letters were sent to occupants of **589** properties on the Old Barnsbury Estate, the New Barnsbury Estate, the Half Moon Crescent Estate, Barnsbury Road, Charlotte Terrace, Copenhagen Street, Denmark Grove, Dewey Road, and Maygood Street on 23/08/2022. Site notices and a press advert were also displayed on 25/08/2022. The public consultation on the application therefore expired on 25/09/2022. However, it is the Council's practice to continue to consider representations made up until the date of a decision.

At the time of the writing of this report 3 letters of objection had been received from the public in response to the consultation. In addition to this, a petition with 136 signatures has also been received which raises objection to the proposed 'green route' through the estate on the basis that it will make the estate less safe and less green.

The issues raised are summarised as follows (*with the paragraphs in brackets indicating where in the report the respective points have been addressed*):

- 8.2 The issues raised in the objections and petition are summarised below (*with the paragraphs in brackets indicating where in the report the respective points have been addressed*):

Objections:

- The reduction in car parking spaces includes no plan put in place for designated blue Badge spaces and no Electric Vehicle Charging Points (*para 10.87*);
- Concerns regarding the consultation process and how results are reported, including the small-sized Barnsbury Residents' Group's views being used as representative of the estate as a whole (*para 10.110*);
- Newlon have stopped carrying out cyclical works prior to this application being submitted (*para 10.111*);
- Detail of the phasing of works is limited and it is not clear whether changes will be block by block or all carried out at the same time (*para 10.112*);
- Nothing has been shared with Old Barnsbury residents about commitments to the quality and specific timescales for the intended works (*para 10.112 – 10.113*);
- Scope of refurbishment works doesn't go far enough. A full re-wire with additional sockets and cheaper lighting should form part of the core offering to each tenant property along with a complete re-fit of broadband provision to the estate to help address the digital divide (*para 10.113*).
- The 'green walkway' is unnecessary and raises concerns over the safety of residents;
- Sections of the proposed pathway risk people being trapped between two closed gates;

- Lockable gates and fob access are not sufficient measures to prevent security risk - fobs can be lost or shared, and gates can be faulty;
- It is not clear that enhancing existing routes has been considered;
- Areas of the proposed pathway will not be visible and will give rise to dog fouling;
- Widening the path at the back of Samford House will result in removal of trees and foliage and the privacy they provide to the school and residents of Samford House;
- The June 2022 Old Barnsbury resident newsletter did not show clearly the route of the proposed pathway through old Barnsbury and yet contained various points in support of the proposal;

Officer comment: *A number of the issues raised (the 7 latter bullet points above) relate to the applicant's original proposal to introduce a 'green walkway' through the estate. This aspect of the proposal has now been omitted from the application with enhancements to existing routes through the estate proposed instead.*

Petition:

- Making the allotment area public and encouraging traffic through the estate removes the few communal areas that local residents have a sense of ownership over. (*para 10.49*).
- Residents are concerned that the new route would make the estate less safe. Anti-social behaviour is already reported on the Estate and opening up the path will attract more people within the estate. (*para 10.71-10.74*)
- Given Newlon's poor record at maintaining the Estate, the path will likely end up being gated up most of the time. (*para 10.74*)
- The creation of the path will take out a large amount of existing green spaces on the estate, both outside and within the allotment area. The area along the boundary wall with the school is currently full of dense foliage and plantation and this area is used to store all the tools and composting bins. Creating the path would the grass removed, plants along the wall destroyed and allotment spaces reduced. (*para 10.45*)
- Several stairs that connect different levels of the Estate remain permanently blocked off. Residents welcome increased connectivity within the Estate but believe Newlon should reopen the stairs and make the paths already in use greener. (*para 10.48*)

External Consultees

- 8.3 **Islington Swifts Group:** The Preliminary Ecological Appraisal and Bat Survey recommend nest boxes for birds on trees and buildings, and lighting suitable for bats - the bird boxes appear to be mentioned in the planning statement but there is no other sign of these on elevations, landscape documents, or in the lighting plan.
- 8.4 **London Fire Brigade:** No response received.
- 8.5 **London and Middlesex Archaeological Society:** No response received.
- 8.6 **London Underground:** No response received.

8.7 **Metropolitan Police:** Raise concern regarding the opening up of the estate via the 'green walkway' and noted that increased neighbourhood permeability could result in greater crime rates. Noted that the 'green walkway' should be fobbed, with access only for residents. Recommended that where public routes through the estate are provided, there should be sufficient lighting, CCTV and wayfinding for pedestrian safety. Requested the incorporation of a relevant planning condition upon any approval for the proposal to achieve Secured by Design (SBD) accreditation.

8.8 **Officer comment:** *The proposed 'green walkway' has been removed from the application.*
Thames Water: Regarding water, Thames Water advised that the development boundary falls within a Source Protection Zone for groundwater abstraction and encouraged the applicant to engage with the Environment Agency's approach to groundwater protection and to discuss the implication for their development with a suitably qualified environmental consultant.

Regarding waste, Thames Water advised that if the developer follows the sequential approach to the disposal of surface water then they would have no objection. Thames Water expect the developer to demonstrate what measures will be undertaken to minimise groundwater discharges into the public sewer and recommended an informative.

8.9 **Thornhill Square Association:** No response received.

8.10 **TfL Land Use Planning:** No response received.

8.11 **UK Power Networks:** No response received.

Internal Consultees

8.12 **Access and Inclusive Design Officer:** Welcomed the proposals to increase levels of accessibility across Old Barnsbury, however raised a number of queries regarding details of the proposed measures. It was agreed that these details could be provided via conditions.

Whilst the Council's Inclusive Design Officer confirmed general support for the proposals, they noted the lack of safe, secure, accessible cycle parking and that this is at odds with the general aim of increasing accessibility across the estate.

8.13 **Biodiversity Officer:** Confirmed general support for the scheme subject to further details of the proposed landscaping. Raised concerns at the low UGF score and the proposals to introduce meadow planting in publicly accessible areas. Noted that two bat roosts have been found on the estate, which is significant for Islington, and noted that nesting boxes to support the bat population should be provided.

8.14 **Building Control:** Raised no concerns, however raised a number of queries regarding the compliance of certain proposed features. The applicant confirmed that they would provide these details to Building Control once a manufacturer is confirmed. No further comments were received.

8.15 **Design and Conservation Officer:** Considers the works to largely constitute cyclical maintenance and although the proposals do not represent the highest quality design, they will undoubtedly provide for more amenable and efficient homes as a result of the new fenestration.

No objections were raised to the proposed new windows and door or the proposed balcony railings treatment.

Regarding the entrance lobbies, the officer noted that because the blocks remain accessed from the rear of the buildings as opposed to off the street, the entrances continue suffer from being located next to the parking areas, bins and bike stores. Regarding the refurbishment of the entrance lobbies, the officer noted that a utilitarian treatment is proposed which does not present a warm and welcoming return home. However, the officer also noted that given the poor quality and appearance of the existing entrances, their replacement is unlikely to represent a grounds of refusal.

The reduction to onsite car parking is welcomed, although noted that the levels of reduction, and reconfiguration of the spaces, are not sufficiently extensive to enable a significant greening of the estate or a meaningful increase in private and communal amenity space for residents.

The officer raised a number of concerns regarding the proposed boundary treatment, noting that the proposal to erect 1.5m high boundary walls directly facing the street edge deadens the public realm and presents a hostile and threatening pedestrian environment as a direct result. **Officer Comment:** *The applicant has omitted the fences and instead introduced plating to the rear of existing fencing, to which the Urban Design Officer raised no objections.*

The officer notes that the proposal makes reference to 'roof works' with few specifics. To ensure the character of Old Barnsbury is retained, the officer has requested that a condition be placed on any permission which requires the retention of the chimneys.

8.16 **Energy Officer:** No response:

8.17 **Highways Officer:** No response:

8.18 **Planning Policy Officer:** No response:

8.19 **Public Protection Officer:** Reviewed the proposals and raised no objections but requested the addition of conditions requiring details regarding the lighting scheme.

8.20 **Refuse and Recycling Officer:** Reviewed the proposals and raised no objections to the proposed development subject to further details of the refuse enclosures and site waste collection strategy being provided.

8.21 **Sustainability Officer:** Reviewed the proposals and welcomed the proposed drainage improvements, noting that they will have some impact on reducing the rate and volume of surface water discharging into the existing onsite drainage network but the reduction in runoff rates would not be significant. However, the officer confirmed that because the application relates only to external alterations to existing buildings, it is not required to achieve the standards in local policy relating to reducing runoff rates and attenuation storage. The officer requested that a condition be added to any permission to secure confirmation of the proposals and details of a SuDS maintenance plan.

The officer welcomed the introduction of double-glazed windows which will improve performance, energy efficiency and insulation.

The officer welcomed the proposals to improve biodiversity but requested further information regarding the meadow planting, green walls, and nesting boxes, to ensure they are successful.

8.22 **Tree Officer:** Reviewed the proposals and raised no objections but requested the addition of conditions requiring details regarding tree protection and tree planting.

9. RELEVANT STATUTORY DUTIES & DEVELOPMENT PLAN CONSIDERATION & POLICIES

9.1 Details of all relevant policies and guidance notes are attached in Appendix 2. This report considers the proposal against the following development plan documents.

National Guidance

9.2 Islington Council (Planning Committee), in determining the planning application has the main following statutory duties to perform:

- To have regard to the provisions of the development plan, so far as material to the application and to any other material considerations (Section 70 Town & Country Planning Act 1990);
- To determine the application in accordance with the development plan unless other material considerations indicate otherwise (Section 38(6) of the Planning and Compulsory Purchase Act 2004) (Note: that the relevant Development Plan is the London Plan and Islington's Local Plan, including adopted Supplementary Planning Guidance.)
- As the development is within or adjacent to a conservation area(s), the Council has a statutory duty in that special attention shall be paid to the desirability of preserving or enhancing the character or appearance of the area (s72(1)).

9.3 National Planning Policy Framework 2021 (NPPF): Paragraph 10 states: "at the heart of the NPPF is a presumption in favour of sustainable development.

9.4 The National Planning Policy Framework 2021 seeks to secure positive growth in a way that effectively balances economic, environmental and social progress for this and future generations. The NPPF is a material consideration and has been taken into account as part of the assessment of these proposals

9.5 Since March 2014 Planning Practice Guidance for England has been published online.

9.6 In considering the planning application account has to be taken of the statutory and policy framework, the documentation accompanying the application, and views of both statutory and non-statutory consultees.

9.7 The Human Rights Act 1998 incorporates the key articles of the European Convention on Human Rights into domestic law. These include:

- Article 1 of the First Protocol: Protection of property. Every natural or legal person is entitled to the peaceful enjoyment of his possessions. No one shall be deprived of his possessions except in the public interest and subject to the conditions provided for by law and by the general principles of international law.
- Article 14: Prohibition of discrimination. The enjoyment of the rights and freedoms set forth in this Convention shall be secured without discrimination on any ground such as sex, race, colour, language, religion, political or other opinion, national or social origin, association with a national minority, property, birth, or other status.

9.8 Members of the Planning Committee must be aware of the rights contained in the Convention (particularly those set out above) when making any Planning decisions. However, most Convention rights are not absolute and set out circumstances when an interference with a person's rights is permitted. Any interference with any of the rights contained in the Convention

must be sanctioned by law and be aimed at pursuing a legitimate aim and must go no further than is necessary and be proportionate.

- 9.9 The Equality Act 2010 provides protection from discrimination in respect of certain protected characteristics, namely: age, disability, gender reassignment, pregnancy and maternity, race, religion or beliefs and sex and sexual orientation. It places the Council under a legal duty to have due regard to the advancement of equality in the exercise of its powers including planning powers. The Committee must be mindful of this duty inter alia when determining all planning applications. In particular, the Committee must pay due regard to the need to: (1) eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under the Act; (2) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it; and (3) foster good relations between persons who share a relevant protected characteristic and persons who do not share it.
- 9.10 In line with Section 72(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990, in assessing the proposals hereby under consideration, special regard has been given to the desirability of preserving the Conservation Area, its setting and any of its features of special architectural or historic interest.
- 9.11 In line with Section 66(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990, special regard has been given to the desirability of preserving the adjoining listed buildings, their setting and any of their features of special architectural or historic interest.

Development Plan

- 9.12 The Development Plan is comprised of the London Plan 2021, Islington Core Strategy 2011, Development Management Policies 2013, Finsbury Local Plan 2013 and Site Allocations 2013. The policies of the Development Plan are considered relevant to this application and are listed at Appendix 2 to this report.

Emerging Policies

- 9.13 The Regulation 19 draft of the Local Plan was approved at Full Council on 27 June 2019 for consultation, with consultation on the Regulation 19 draft taking place from 5 September 2019 to 18 October 2019. The Draft Local Plan was subsequently submitted to the Secretary of State for Independent Examination in February 2020. The Examination Hearings took place between 13 September and 1 October 2021, with consultation on Main Modifications running from 24 June to 30 October 2022.
- 8.1 In line with the NPPF, Local Planning Authorities may give weight to relevant policies in emerging plans according to:
- the stage of preparation of the emerging plan (the more advanced its preparation, the greater the weight that may be given);
 - the extent to which there are unresolved objections to relevant policies (the less significant the unresolved objections, the greater the weight that may be given); and
 - the degree of consistency of the relevant policies in the emerging plan to this Framework (the closer the policies in the emerging plan to the policies in the Framework, the greater the weight that may be given).
- 8.2 Given the advanced stage of the draft plan and the conformity of the emerging policies with the Framework it is considered that policies can be afforded moderate to significant weight depending on the significance of objections to main modifications.
- 8.3 Emerging policies relevant to this application are set out below:

Policy PLAN1 Site appraisal, design principles and process
Policy SP2 Kings Cross and Pentonville Road
Policy H1 Thriving communities
Policy SC2 Play space
Policy G1 Green infrastructure
Policy G2 Protecting open space
Policy G4 Biodiversity, landscape design and trees
Policy G5 Green roofs and vertical greening
Policy S1 Delivering sustainable design
Policy S2 Sustainable design standards
Policy S4 Minimising greenhouse gas emissions
Policy S8 Flood risk management
Policy S9 Integrated water management and sustainable drainage
Policy T2 Sustainable transport choices
Policy T3 Car free development
Policy T4 Public realm
Policy T5 Delivery, servicing and construction
Policy DH1 Fostering innovation and conserving and enhancing the historic environment
Policy ST2 Waste

10. ASSESSMENT

- 10.1 Whilst this planning application constitutes a Major development due to the site area alone, there is no proposed uplift in floorspace, no additional homes, and no changes of use. Therefore, it is considered that the application does not give rise to any land use implications, nor is it considered that the application would be subject to any relevant planning obligations.
- 10.2 The main issues arising from this proposal relate to:
- Design and Conservation and Heritage Considerations
 - Landscaping
 - Ecology
 - Neighbouring Amenity
 - Accessibility
 - Transport
 - Sustainability
 - Quality of Accommodation

Design and Appearance

- 10.3 The National Planning Policy Framework states that the Government attaches great importance to the design of the built environment and that good design is a key aspect of sustainable development. All proposals for development in Islington are expected to be of good quality design, respecting their urban context in accordance with planning policy and guidelines.
- 10.4 London Plan Policy D3 states that development should: 'enhance local context by delivering buildings and spaces that positively respond to local distinctiveness through their layout, orientation, scale, appearance and shape, with due regard to existing and emerging street hierarchy, building types, forms and proportions.' Policy D3 also states development should respond to the existing character of a place by identifying the special and valued features and characteristics that are unique to the locality and be of high quality, with architecture that pays attention to detail, and gives thorough consideration to the practicality of use, flexibility, safety and building lifespan through appropriate construction methods and the use of attractive, robust materials which weather and mature well.

- 10.5 Islington's Core Strategy Policy CS8 states that the scale of new development will reflect the character of a surrounding area. Policy CS9 states that high quality architecture and urban design are key to enhancing and protecting Islington's built environment, making it safer and more inclusive. Moreover, where areas of Islington suffer from poor layout, opportunities will be taken to redesign them by integrating new buildings into surviving fragments of historic fabric and by reconfiguring spaces based on streets and perimeter blocks, particularly in housing estates. Finally, the aim is for new buildings to be sympathetic in scale and appearance and to be complementary to the local identity.
- 10.6 Islington's Development Management Policy DM2.1 requires all forms of development to be of a high quality, incorporating inclusive design principles while making positive contributions to the local character and distinctiveness of an area, based upon an understanding and evaluation of its defining characteristics. All new developments are required to improve the quality, clarity and sense of space around or between buildings, reinforce and complement local distinctiveness and create a positive sense of place.
- 10.7 Policy DH1 (Fostering innovation while protecting heritage) of the Draft Islington Local Plan states that all development must be designed to be contextual, connected, sustainable and inclusive. Policy DH2 (Heritage assets) of the Draft Islington Local Plan outlines that Development within conservation areas and their settings must conserve and enhance the significance of the area and should be of a high quality.
- 10.8 Finally, Islington's Urban Design Guide (2017) provides guidelines and principles for good urban design.

Principle of Development

- 10.9 The site contains eight existing blocks of flats, a considerable amount of hardstanding, and limited play space and landscaping. No demolition of the existing buildings is proposed.
- 10.10 The buildings facing onto the site on the opposite side of Barnsbury Road are Grade II listed and the estate is bounded by the Barnsbury Conservation Area to the north, east, and south.
- 10.11 Historic England's guidance on the *Setting of Heritage Assets* notes that setting may include:
- Consideration of setting in urban areas, given the potential numbers and proximity of heritage assets, often overlaps with considerations both of townscape/urban design and of the character and appearance of conservation areas..... the numbers and proximity of heritage assets in urban areas mean that the protection and enhancement of setting is intimately linked to townscape and urban design considerations. These include the degree of conscious design or fortuitous beauty and the consequent visual harmony or congruity of development, and often relates to townscape attributes such as enclosure, definition of streets and spaces and spatial qualities as well as lighting, trees, and verges, or the treatments of boundaries or street surfaces*
- 10.12 The site is considered to form part of the setting of heritage assets, both the setting of individual assets (listed buildings), and the overall character and appearance of the historic streetscape and townscape hierarchy (the conservation area).
- 10.13 The proposed works include the replacement of windows and doors, upgraded balconies and boundary treatments, and landscaping works within the estate. It is considered that these works would not change the bulk, height, or massing of the buildings within Old Barnsbury, nor is it considered that the proposals would fundamentally change the character of Old Barnsbury or detrimentally impact the setting of the surrounding listed buildings and conservation area.

10.14 Noting that the proposals are intended to refurbish the existing buildings with a consistent approach, and given that there would not be a fundamental change to the scale and massing of the buildings, it is considered that subject to ensuring an acceptable design quality, the proposed works would not detract from the character and appearance of the conservation area or the setting of the adjacent Grade II Listed Buildings.

Replacement Windows and Doors

10.15 The original street facing windows on Old Barnsbury have all previously been replaced with white UPVC windows of an inconsistent design. This results in a number of different styles across the estate buildings and these bear no relationship to one another other than the size of overall opening. Additionally, the Old Barnsbury Estate buildings did not originally include balconies and the original windows to living spaces were retrofitted with white UPVC balcony doors in the early 2000s. The condition and performance of the existing windows and balcony doors are poor and contribute to a low level of design quality across the estate.



Image 7: Existing street-facing windows, balconies, and balcony doors

10.16 The application proposes to replace all street facing windows and balcony doors with double glazed, composite aluminium framed units in order to improve energy efficiency and insulation. The new windows have been designed to meet modern building regulations. They do not replicate what would have originally been timber sash windows, but the mullions are arranged to echo those of the original pattern. The windows are to be twin side hung with a lower fixed panel. They are to be recessed slightly to add depth and visual integrity to the façade.



Image 8: Proposed street-facing windows, balconies, and balcony doors

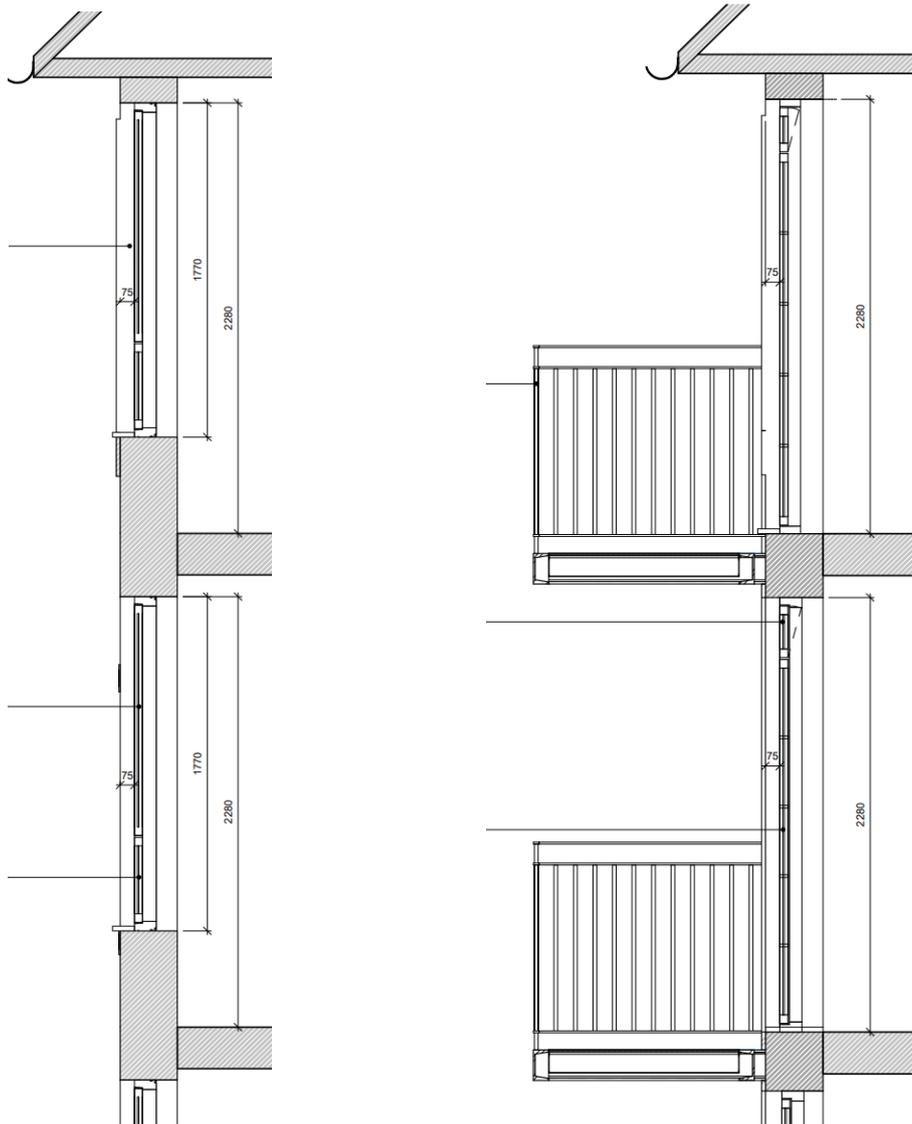


Image 9 and 10: Proposed street-facing window sections (left image) and proposed balcony door sections (right image)

10.17 The windows and balcony doors are to be double glazed aluminium composite, coloured dark green. The balcony railings are to remain unchanged although they will be painted to match the colour of the new balcony doors and windows.

10.18 Regarding window replacements, the Islington Urban Design Guide notes:

5.127 Where window replacement is sought in existing buildings, this should normally be done in the original style and materials... and applied universally across the elevation to ensure consistency.

5.130 Regardless of the framing material, replacement window design should deliver a context appropriate and refined profile that creates a satisfactory solid/void relationship between wall and window (glazed to unglazed area) and ensures sufficient daylight penetration to the interior of the building.

- 10.19 Whilst the proposed windows and balcony doors do not exactly match the original windows, the street-facing windows and balcony doors have been designed to reference the original window designs, whilst introducing a new slimmer window frame that allows for increased daylight penetration. Furthermore, the window design allows for compliance with building control regulations and increased energy performance. The proposed replacement windows and balcony doors would introduce a coherent design to the buildings across the estate and would also improve the setting of the listed buildings on Barnsbury Road and the Barnsbury Conservation Area.
- 10.20 Facing in to the estate, it is proposed to replace all windows and front doors. The existing windows which face into the estate are white UPVC and feature top openings of varying sizes to reflect function and size of room. The windows sit flush with the external wall and feature a thick frame which reduces the amount of potential daylight within.
- 10.21 It is proposed that the existing windows be replaced with aluminium composite windows with white frames to match the repainted brickwork inside. The proposed window frames are thinner than the existing, allowing for increased daylight penetration. Additionally, the proposed frames will feature an integrated ventilation fan to ensure that the kitchens and bathrooms they serve can function as intended.
- 10.22 The existing front doors are made of timber and there are a variety of different types and colours across the estate. It is proposed to replace all front doors with steel-faced composite doors. These doors feature a fanlight to allow for increased permeation of daylight and are to be provided in various complimentary colours. No objections are raised to the proposed new windows and doors facing into the estate and it is considered that they will result in more efficient homes with an increased level of residential amenity and greater coherence of design.

New Communal Entrances

- 10.23 Currently, the Old Barnsbury buildings are accessed from within the internal areas of the estate, meaning residents must navigate parking courts and bins and bike stores to access their homes. The application assesses the feasibility of moving the building entrances to the street side of the buildings in the Design and Access Statement but ultimately finds that it is not possible. Whilst it is regrettable that the opportunity has been missed to better address the estate's urban edge conditions whilst simultaneously greening spaces to the rear of the blocks for private and communal amenity, it is not unacceptable to keep the building entrances in their current position.
- 10.24 The applicant proposes to refurbish the existing entrance lobbies, replacing the existing doors and lighting, painting the canopies, and providing new signage and additional glazing. Currently, the building entrances have narrow windows which look defensive and unwelcoming. The proposals to introduce additional glazing to these entrances allows for increased natural surveillance and also allows more natural light into internal entrance foyers. Additionally, improved lighting and signage will aid in wayfinding on the estate and better materials will mean that the buildings perform more efficiently. However, despite this, it is considered that the proposed entrance lobbies have been subject to a utilitarian treatment more suited to an office building than to blocks of flats.
- 10.25 Whilst it is regrettable that a better design has not been proposed, the proposed communal foyers do provide 'logical, legible' entrances in line with Islington's Development Management Policy DM3.4. Further details of the proposed entrances, including details of signage, will be secured via Condition 5.



Image 11 – Existing entrances on the estate



Image 12 – CGI showing proposed communal entrance treatment

Boundary Treatment

- 10.26 When built, the Old Barnsbury Estate had communal gardens which faced on to the surrounding streets. Sometime after this, the communal gardens were converted into private gardens for ground floor residents. The surrounding streetscape has suffered as a result of the private gardens facing on to the street, with different approaches having been taken to boundaries to gardens, with many having added screening to the boundary railings for privacy. To create a more coherent boundary treatment and to increase security and privacy for ground floor residents, the applicant proposes to upgrade the boundary treatment of the street facing gardens across the Old Barnsbury Estate. Islington's Urban Design Guide notes the importance of active frontages and notes that:

5.55... Any boundary treatment should be designed in a manner that is appropriate to the existing street and should provide an open aspect.

- 10.27 The boundary treatment originally proposed by the applicant included the erection of 1.5m high boundary walls directly facing the street edge including to Charlotte Terrace, Copenhagen Street, and Barnsbury Road. It was considered that this approach would have had an unacceptable impact on the public realm, creating a hostile and threatening pedestrian environment as a direct result. The application has been subsequently amended to retain the characterful cast iron railing facing Charlotte Terrace, Copenhagen Street and Barnsbury Road and provide a hedge behind it. This boundary treatment was considered more acceptable by the Council's Design and Conservation Officer. Further details of the boundary treatment, and planting are recommended to be secured via a landscaping condition (No. 7).

Additional External Works

- 10.28 In addition to the external alterations, the applicant intends to carry out repair works to the roofs, brickwork, and access decks where needed. It is considered that these works of 'making good' do not require planning permission, however little detail has been provided as to what these repair works include and the extent to which they will be applied. The Council's Design and Conservation Officer notes that the Old Barnsbury Estate is characterised by its large, handsome and rhythmic chimneys. In order to ensure the protection of the chimneys, it is recommended that a condition (4) be added to any planning permission that requires the retention of these important features.

Assessment of Significance

- 10.29 As referenced above, the application property is adjacent to the Grade II Listed terraces on Barnsbury Road and is bounded by the Barnsbury Conservation Area. The Old Barnsbury Estate is on the other side of the road to the listed buildings and is not included within the boundaries of the conservation area.
- 10.30 In accordance with s66(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990, the local planning authority has a duty in considering whether to grant planning permission for development which affects a listed building or its setting to have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.
- 10.31 Historic England's listing description confirms 16-62 Barnsbury Road as three storeys over basement, with attic dormers. The description identifies a number of features which contribute to its listing including sash windows, arched and radial glazing bars, and cast-iron railings.
- 10.32 Regarding the street-facing proposals contained within this application, it is considered that the proposal to replace the street-facing windows and balcony doors with new ones designed to

reference the original features of the Old Barnsbury buildings would be an improvement on the existing situation and an enhancement to the setting of the Grade II listed terrace houses and neighbouring conservation area. It is also not considered that the proposed changes to the boundary treatment of the street-facing gardens would cause harm to the setting of the listed buildings or conservation area.

Conclusion

- 10.33 Section 72 (1) of the of the Planning (Listed Buildings and Conservation Areas) Act 1990 requires the Local Authority to pay special attention to the desirability of preserving or enhancing the character or appearance of Conservation Areas.
- 10.34 Under s66(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990, the local planning authority has a duty in considering whether to grant planning permission for development which affects a listed building or its setting to have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses. This is also the wording of the duty the local planning authority has when considering applications for listed building consent.
- 10.35 Given the above, the proposals are considered not to negatively impact the character and appearance of the surrounding conservation area or listed buildings. Whilst the proposed communal entrances do not represent high quality design, the proposed replacement windows reference the estate's original design and provide a coherent design to these characterful mansion block typologies. The proposed works will also undoubtedly provide for more amenable and efficient homes. The application therefore complies with the NPPF 2021, policies D4 and HC1 of the London Plan 2021, Policy CS8 and CS9 of the Islington Core Strategy 2011, policies DM2.1 and DM2.3 of the Islington Development Management Policies 2013, policies DH1 and DH2 of the Draft Islington Local Plan, and the guidance contained within the Urban Design Guide 2017.
- 10.36 Whilst the design proposals are supported in principle, further details of materials are recommended to be required by Condition 3.

Landscaping

- 10.37 Islington's Core Strategy Policy CS15 on open space and green infrastructure states that the council will provide inclusive spaces for residents and visitors and create a greener borough by protecting all existing local spaces, including open spaces of heritage value, as well as incidental green space, trees and private gardens. Development Management Policy DM6.5 and Emerging Local Plan policy G4 state that development should protect, contribute to and enhance the landscape, biodiversity and growing conditions of the development site and surrounding areas. Developments are required to maximise provision of soft landscaping, including trees, shrubs and other vegetation. Furthermore, developments are required to minimise any impacts on trees, shrubs and other significant vegetation. At the same time any loss of or damage to trees, or adverse effects on their growing conditions, will only be permitted where there are over-riding planning benefits.
- 10.38 Regarding open space, Development Management Policy DM6.3 states that development is not permitted on semi-private amenity spaces, including open space within housing estates and other similar spaces in the borough not designated as public open space, unless the loss of amenity space is compensated, and the development has over-riding planning benefits. Moreover, both Development Management Policies DM2.1 and DM8.4, and emerging Local Plan policy PLAN1, encourage greater permeability by improving movement through areas and seeking an improved pedestrian environment following Islington's 'Streetbook' SPD.

- 10.39 Old Barnsbury is currently dominated by car parking and hardstanding circulation space for cars. There is 4,712sqm of open space on the estate, of which 1,126sqm is semi-private communal garden managed by the community via the allotment club and only 379sqm is public open space. The remaining space is either inaccessible buffer planting or private gardens. Most of the private gardens face directly onto public areas with no buffer planting onto the surrounding roads causing privacy and security issues.
- 10.40 Old Barnsbury suffers from poor pedestrian permeability and accessibility is limited. This is a result of the estate being split across four levels with only stepped access connecting them. Each level is individually gated and a brick retaining wall divides them. The access gate between levels two and three has been permanently closed off and high fences close parts of the public realm off due to historic crime prevention measures.
- 10.41 The landscaping proposals involve the general greening of the estate, with the removal of 28 car parking spaces resulting in an uplift of 569sqm of open space. Landscaping interventions are proposed in the areas of hardstanding to create greener areas with greater amenity and biodiversity value which will allow for the estate open space to be better used and more pedestrian friendly.



Image 13: Proposed landscaping strategy

10.42 Looking at the landscape strategy in more detail, the removal of parking spaces to the south of the site creates space for a new shared courtyard in front of Berners House with a new area of doorstep play. Though the existing Bamsbury Road Park will remain largely the same size, new

planting, seating, and play features are proposed. At the request of residents, there are no proposals to change the community allotments, although the southern part of the space is to be opened up to all residents as part of proposals to increase residents' access to green spaces. The existing play area is enlarged and includes a rain garden and new planting. The improvements of pedestrian routes across the estate increase the estate's level of accessibility. New boundary treatments and biodiverse planting across the estate is also proposed. Additional greening is to be achieved via green roofs and vertical greening to the proposed new refuse stores where possible.

- 10.43 New hard landscaping is also proposed, with permeable surfaces being included at various locations across the estate. In addition, new seating is to be provided, including playable seating which will allow for informal play opportunities.
- 10.44 London Plan Policy G5 requires that all major development include urban greening as a fundamental element of site and building design. Part B of Policy G5 notes that, the Mayor recommends a target score of 0.4 for developments that are predominately residential. Paragraph 8.5.1 of the supporting text notes that urban greening measures are required in new development. Given that this application relates to external alterations to existing buildings and associated landscaping, with no planned demolition or uplift in floorspace, it is considered that the UGF score of 0.4 is not of great relevance. However, Core Strategy policy CS15, Development Management policy DM6.5, and emerging policies G1, G3, G4, and G5 promote the maximisation of opportunities to 'green' the borough and, as such, there is still an expectation that the applicant should achieve as high a UGF score as possible.
- 10.45 The submitted Landscaping Strategy notes that there is an existing Urban Greening Factor of 0.182 on the Old Barnsbury Estate. Taken together, the proposed landscaping works would lead to a UGF score of 0.241 – an uplift of 0.059. Whilst this uplift in UGF is welcomed, it is noted that the submitted Landscaping Strategy claims an additional 1000sqm of open space could be released in the future with the removal of more car parking. It is disappointing that this could not be proposed at this stage. Furthermore, concern is raised that 0.021 of the proposed UGF score comes from permeable paving which has no biodiversity benefits. Additionally, 0.039 of the UGF uplift comes from meadow planting and species-rich grassland, much of which is to be planted near play space and in areas intended as accessible open space. The Council's Ecology Officer and Sustainability Officer have both raised concern over this as this type of planting is delicate and, unless the meadow planting is fenced off, footfall from residents could damage the planting and result in any biodiversity benefits being lost. To ensure the urban greening factor is maximised, a minimum UGF of 0.241 shall be secured via condition (7), along with details of a landscaping management plan to ensure that all planting maintains its biodiversity value (8).
- 10.46 The proposed landscaping strategy results in an uplift of over 500sqm of open space, with biodiversity and SUDS improvements. Whilst it is regrettable that more car parking spaces could not be removed to provide further landscaping, the quality of the landscape strategy and the overall planning benefits derived from the proposal ensure that the application complies with Core Strategy Policy CS15 and Development Management Policy DM6.3. While the landscape strategy is supported in principle, further details would be required by condition (7) in the event of permission being granted to ensure that the final design of the landscaping is of the highest quality and properly implemented.

Pedestrian Improvements

- 10.47 As part of the proposed landscaping works, the applicant seeks to create new pedestrian routes through the estate, providing new accessible ramps and creating new links to the estate's green spaces. Originally, the applicant intended to provide a new east-west public link 'green walkway' through Old Barnsbury, linking the New Barnsbury Estate and Barnsbury Road, however this proposal was subject to objections from estate residents. This route would have opened up the

estate's Barnsbury Park on Barnsbury Road to the public, supporting local policy which seeks to open up areas of green space to the wider public in order to address the borough's open space deficiencies.

- 10.48 Following the Council's consultation, a petition with 136 signatures was received objecting to this route on the grounds of estate safety and anti-social behaviour. In response to this petition, the applicant has amended the proposals to omit the 'green walkway' and instead seeks to provide pedestrian improvements for estate residents whilst also opening up green spaces to all estate residents. These spaces are the allotments south of Berners House and 'Barnsbury Park' behind Corbet House, and they would be accessible to all Old Barnsbury residents via the use of fobs.
- 10.49 Whilst it is regrettable that the opening up of this route to the public is no longer proposed, it is acknowledged that due to the arrangement of the buildings around this particular space, it's wider public use could result in safety concerns. Currently, access to the area south of Samford House is restricted to members of the allotment club. By creating a residents-only route through this area, estate permeability and access to green space is improved for residents of Old Barnsbury.
- 10.50 Rather than the east-west route, which was objected to by residents, the applicant now proposes to improve an existing route through Old Barnsbury from Maygood Street in the south, to Copenhagen Street in the north. Currently, there are no step-free routes through Old Barnsbury. The proposals to improve this route includes the provision of new ramps which would make the existing pedestrian route accessible for all. As such, it is considered that in addition to increasing estate permeability for residents, the proposed pedestrian improvements would have the effect of increasing permeability for the public too. In addition, the new pedestrian routes are to be provided with permeable paving, and landscaping surrounding the routes will contribute to the wider greening of the estate and an improved pedestrian environment in line with local policy. Further details of the pedestrian improvements on the estate will be secured via Condition 19.

Play Space

- 10.51 Policy DM3.6 requires all housing development of more than 10 dwellings to make provision of play based on anticipated child yield. Emerging Local Plan Policy SC2 requires at Part (C) states 'all major residential development must make appropriate on-site provision for free to-use publicly accessible play space, which is suitable for children and young people of all ages and abilities. Provision must be proportionate to the anticipated increase in child population as a result of development proposals. All proposed provision of new play space within development sites must be designed in partnership with Islington Council, in line with any relevant best practice standards.'
- 10.52 Paragraph 3.167 notes in the supporting text for Policy SC2 that 'the Council will take into consideration the London Plan benchmark for 10sqm of play space to be provided per child as a starting point but it is recognised that in Islington's context, delivery at this level may not always be feasible.'
- 10.53 Using the GLA's Play Space Calculator with the existing units on the estate, there are an estimated 273.6 children on the estate, resulting in a requirement for 2735.9sqm of child play space as a minimum. However, as there is no increase in the number of units on the estate proposed, there is no requirement for the applicant to increase the play space provision
- 10.54 There is currently 213sqm of play space on the Old Barnsbury Estate. This play space is of a poor quality and underused with a lack of natural surveillance due to high fences and planting. The development proposal includes a total of 554sqm of play space – an uplift of 341sqm – which is achieved by expanding the size of the existing dedicated playspace and providing three

new doorstep play locations across the estate. It is noted that a pedestrian through-route takes up a considerable amount of the area considered play space, meaning the area of useable play space is less than the stated 554sqm. However, whilst this is the case and although 554sqm of play space falls well below the minimum child play space noted above, it is accepted that existing site constraints mean that providing further play space would prevent other commitments on the site from being achieved. In addition, Barnard Park has good quality play facilities and is less than 100m from the centre of Old Barnsbury. Further details of any playspace provided within the development would be required by Condition 7.



Image 14: Proposed play space showing pedestrian route through south-eastern corner

Trees

- 10.55 DM6.5 states that Developments must protect, contribute to and enhance the landscape, biodiversity value and growing conditions of the development site and surrounding area, including protecting connectivity between habitats. Emerging Local Plan Policy G4 states any loss of or damage to trees or other significant planting, or adverse effects on their growing conditions or survival, will only be permitted where it is demonstrably unavoidable in order to meet other relevant Local Plan policy requirements (as agreed with the Council). Developments are required to maximise the provision of soft landscaping, including trees, shrubs and other vegetation, and maximise biodiversity benefits, including through the incorporation of wildlife

habitats that complement surrounding habitats and support the council's Biodiversity Action Plan.

- 10.56 There are 130 individual trees, 8 shrub groups, 14 groups, and 2 hedges included in the submitted Arboricultural Impact Assessment. The proposal involves the removal of 2 low quality trees, a semi mature birch and an over-mature pyracantha in order to enable the development. It is also proposed that 3 stumps and 1 low quality group of topped laurels will be removed (category U).
- 10.57 All remaining trees will be retained and protected. The submitted Arboricultural Impact Assessment has been considered and assessed by the Council's Tree Preservation Officer who has accepted the assumptions and recommendations within it. All existing retained trees would be protected during the construction process to ensure their health and future growth and details of all works to trees will required by condition to ensure they meet required standards.
- 10.58 A total of 13 new trees will be planted, leading to a net gain of 11 trees on the estate. The planting of these trees would be required by the landscaping condition. The proposal increases the canopy cover overall on site. As such, the open space, landscape, and tree planting strategy is considered a qualitative enhancement. Whilst the proposed tree protection and tree planting details are supported in principle, further details shall be required via Conditions 11 and 12.

Ecology

- 10.59 Policy DM6.3 of the Islington Development Management Policies 2013 states that planning permission will not be given for any schemes which adversely affect designated SINCs of Metropolitan or Borough Grade I, Grade II or local importance. Part C of policy DM6.3 states that any other site of significant biodiversity value will be strongly protected. Emerging Local Plan Policy G4 states that all developments must protect, enhance and contribute to the landscape, biodiversity value and growing conditions of the development site and surrounding area, including protecting and enhancing connectivity between habitats.
- 10.60 The submitted Bat Survey notes that two roosts were found on the Old Barnsbury Estate, which the Council's Ecology Officer has advised is significant for Islington. As such, it will be necessary for the applicant to obtain a licence from Natural England before any works are carried out which may disturb the bats using these roosts. An informative has been added to remind the applicant of this. Additionally, as the roosts will be lost due to planned works, the Ecology Officer has advised that mitigation will be required in the form of bat boxes, landscape improvements and a sensitive lighting scheme.
- 10.61 To ensure that the proposed development complies with local policy, conditions will be added to the decision notice requiring details of nesting boxes (10), including locations and specifications, and also details of the lighting scheme (14) to ensure that it does not have an adverse impact on local wildlife. Additionally, the applicant will be required to address how biodiversity will be protected during the construction phase within the Construction Environment Management Plan (CEMP) required by Condition 6.

Neighbouring Amenity

- 10.62 Paragraph 127 of the National Planning Policy Framework states that planning decisions should ensure that developments would have a high standard of amenity for existing and future users. All new developments are subject to an assessment of their impact on neighbouring amenity in terms of loss of daylight, sunlight, privacy and an increased sense of enclosure. A development's likely impact in terms of air quality, dust, safety, security, noise and disturbance is also assessed

- 10.63 Part D of policy D3 of the London Plan 2021 states that development proposals should deliver appropriate outlook, privacy and amenity, the design of the development should also help prevent or mitigate the impacts of noise and poor air quality.
- 10.64 Policy DM2.1 of the Development Management Policies Document 2013 identifies that satisfactory consideration shall be given to noise and the impact of disturbance, vibration, as well as overshadowing, overlooking, privacy, direct sunlight and daylight receipt, over-dominance, sense of enclosure and outlook.
- 10.65 Emerging Local Plan policy PLAN1 notes that all development must provide a good level of amenity, and should consider the impact of noise, disturbance, hours of operation, vibration, pollution (such as air, light and noise), fumes, overshadowing, overlooking, privacy, direct sunlight and daylight, over-dominance, sense of enclosure and outlook.
- 10.66 This application does not propose the addition or removal of any dwellings or window locations. While the proposals would result in existing fencing set behind railings to private amenity spaces being removed, the proposals include defensible-hedge planting to ensure these amenity spaces remain private.
- 10.67 A condition (6) is recommended to secure a construction environmental management plan to ensure impacts of construction are mitigated.
- 10.68 A condition (14) is also recommended requiring details of the proposed lighting throughout the estate. This will ensure that lighting levels are acceptable.
- 10.69 As such, it is not considered that the proposals would have any impact on overlooking or privacy, outlook or sense of enclosure, overshadowing, or daylight and sunlight levels.

Safety and Security

- 10.70 Development Management Policy DM2.1 and emerging Local Plan policy PLAN1 requires for developments to be safe and inclusive, enhance legibility with a clear distinction between public and private space and to include safety in design, such as access, materials and site management strategies. One of the key objectives of this proposal is to create a safe and secure environment for residents of the estate.
- 10.71 The application proposes a number of measures across the estate to enhance security and deter anti-social behaviour (ASB), including new CCTV and fob access to building entrances, a new lighting scheme, and the improvements to open space making a more welcoming space and improving natural surveillance. These features were welcomed by the Crime Prevention Officer and further details of security measures will be secured via condition 15.
- 10.72 The applicant's original proposal included a pedestrian route through the site. This was not supported by the Crime Prevention Officer and the applicant has subsequently omitted this from the proposals. Instead of creating a new publicly accessible route, the proposal would instead open up two previously inaccessible green spaces to estate residents; Barnsbury Road Park and the centrally located allotments. It is proposed that these spaces be gated and accessible only to residents via the use of fobs.
- 10.73 These gates would replace two existing gates with key access in similar locations. The use of fobbed access gates in these locations would not be contrary to the aims of local policy which seek to provide inclusive communities and improve the permeability and legibility of sites because these spaces were previously inaccessible. Furthermore, the addition of resident only gates was supported by the Crime Prevention Officer. It is therefore considered that the introduction of resident-only gates in these locations would provide an appropriate balance

between acknowledging the safety concerns of residents whilst improving estate permeability and the pedestrian experience.



Image 15: Existing and proposed gates across the estate

10.74 Whilst the addition of residents-only gates to the allotments and Bamsbury Road Park is supported, a condition (15) will be added any permission requiring details of an operational management plan which outlines gate maintenance, opening hours, fob access arrangements, and confirmation of which residents will have access to which areas of the estate.

Air Quality, Noise, Disturbance and Construction

- 10.75 In the event that permission is granted, approval would be on condition that construction impacts are suitably mitigated through the submission of and adherence to a Construction Environmental Management Plan secured via Condition 6.

Amenity Conclusion

- 10.76 Overall, the application is considered to have acceptable amenity impacts and would comply with policy DM2.1 of the Development Management Policies 2013.

Accessibility

- 10.77 Policy GG1 of the London Plan 2021 requires that development must support and promote the creation of a London where all Londoners, including children and young people, older people, disabled people, and people with young children, as well as people with other protected characteristics, can move around with ease and enjoy the opportunities the city provides. Further, it supports and promote the creation of an inclusive London where all Londoners can share in its prosperity, culture and community, minimising the barriers, challenges and inequalities they face.
- 10.78 The Inclusive Design principles are set out within policy D5 of the London Plan which states that development proposals should achieve the highest standards of accessible and inclusive design. It should:
- 1) be designed taking into account London's diverse population;
 - 2) provide high quality people focused spaces that are designed to facilitate social interaction and inclusion;
 - 3) be convenient and welcoming with no disabling barriers, providing independent access without additional undue effort, separation or special treatment;
 - 4) be able to be entered, used and exited safely, easily and with dignity for all
 - 5) be designed to incorporate safe and dignified emergency evacuation for all building users. In all developments where lifts are installed, as a minimum at least one lift per core (or more subject to capacity assessments) should be a suitably sized fire evacuation lift suitable to be used to evacuate people who require level access from the building.
- 10.79 At a local level, Islington's Development Management Policy DM2.2 requires all new developments to demonstrate that they: i) provide for ease of and versatility in use; ii) deliver safe, legible and logical environments; iii) produce places and spaces that are convenient and enjoyable to use for everyone, and iv) bring together the design and management of a development from the outset and over its lifetime.
- 10.80 The Council's Inclusive Design SPD further sets out detailed guidelines for the appropriate design and layout of existing and proposed new buildings.
- 10.81 The Council's Inclusive Design Officer was consulted on the inclusive design principles of the proposal and confirmed general support for the proposed designs. The introduction of a new ramp and railings to create an accessible route through the estate is welcomed, as is the introduction of new signage and wayfinding measures.

- 10.82 Further details related to the scheme's accessibility, including details of the communal entrances, landscaping, cycle parking, refuse stores, and pedestrian/cyclist prioritisation, will be secured via conditions in the event of planning permission being granted.
- 10.83 In conclusion, the proposed development would comply with the relevant policies in delivering an inclusive environment that is safe, convenient, and inclusive for all future users.

Highways and Transportation

Car Parking

- 10.84 Core Strategy Policy CS10 (Sustainable development), Part H, requires car free development. Development Management Policy DM8.5 (Vehicle parking), Part A (Residential parking) requires new homes to be car free, including the removal of rights for residents to apply for on-street car parking permits.
- 10.85 Draft Local Plan Policy T1: Enhancing the public realm and sustainable transportation requires that applicants must provide appropriate information to allow proper assessment of transport impacts and show how these impacts can be addressed. This policy goes on to state that all new development will be car-free, which will contribute to the strategic aim for a modal shift to sustainable transport modes.
- 10.86 Wheelchair accessible parking should be provided in line with Development Management Policy DM8.5 (Vehicle parking), Part C (Wheelchair accessible parking).
- 10.87 There are currently 85 car parking spaces on the estate, of which 76 are used by permit holders. It is proposed to retain 57 of these parking spaces, including the six existing spaces dedicated for blue badge holders. It is welcomed that the car parking spaces on the estate will be reduced in number in accordance with Islington's Development Management Policies, however it is noted that it is not specified where the accessible car parking spaces are to be provided. Details of the car parking rationalisation strategy and the locations of accessible car parking spaces shall be secured via Condition 18. Furthermore, a condition is recommended requiring a parking management plan to be submitted that would provide a strategy for making further improvements to the site at the point parking permits are relinquished.

Pedestrian/Cycle Improvements

- 10.88 Core Strategy Policy CS10 (Sustainable design), Part H seeks to maximise opportunities for cycling. Development Management Policy DM8.4 (Walking and cycling), Part D requires the provision of secure, sheltered, integrated, conveniently located, adequately lit, step-free and accessible cycle parking. Policy T2 of the emerging Local Plan promotes sustainable transport choices and requires all new developments to incentivise walking and cycling.
- 10.89 As the proposals do not include any uplift in units, there is no requirement to provide further cycle parking spaces.
- 10.90 Notwithstanding this, there is currently a lack of secure cycle parking on Old Barnsbury and residents lock bikes to fencing around the estate. 54 secure, sheltered bicycle parking spaces and 34 visitor bicycle parking spaces are proposed to be located across the estate, near the existing cores of each block. The number of secure cycle parking spaces proposed falls below the 528 spaces required by Policy T5 of the London Plan for new units, however, as noted above there is no policy requirement for the provision of these.
- 10.91 The proposed accessible cycle parking spaces are detailed as Sheffield stands outside of the secure bike sheds and no storage for mobility scooters and prams has been proposed. While

further provision of cycle parking would be preferable, it is accepted that there is limited space to provide secure cycle parking on Old Barnsbury due to existing site constraints and Newlon's commitment to maintaining car parking spaces for all permit holders. Furthermore, because no new units are proposed and there is no uplift in floorspace, planning policy relating to cycle parking requirements do not apply. Although the cycle parking strategy is supported in principle further details would be required by condition (16) in the event of permission being granted to ensure that the final design of the cycle parking is of the highest quality and meets inclusive design requirements.

- 10.92 The proposal provides an improved pedestrian environment by providing an enhanced definition between public and private space, and includes improved accessible access as well as safety and security measures. The enhancement of existing routes and spaces within Old Barnsbury provides an enhanced pedestrian environment.

Refuse and Recycling

- 10.93 London Plan Policy T7 states that seeks to ensure that development proposals facilitate safe, clean and efficient deliveries and servicing. Part C of Policy DM8.6 of Islington's Development Management Policies requires major developments to provide details of refuse and recycling collection, including locations for collection vehicles to wait and locations of refuse and recycling bin stores. Paragraph 5.2 of the Islington Street Environment Services 'Recycling and Refuse Storage Requirements' provides advice in relation to acceptable refuse and recycling provision for new residential units.
- 10.94 There are eight existing allocated refuse points within Old Barnsbury, although these are poorly located with issues of fly-tipping and non-compliant distances between bins and refuse collection points. The proposed works include reorganisation and enhancement of communal refuse storage to reduce bin drag distances. Bins will be provided within secure stores which are clearly visible and better integrated into the estate.
- 10.95 Currently, refuse bins are dragged from their locations within the estate to public highway for collection. It is proposed that this strategy is maintained. The refuse and recycling proposals have been reviewed by the Council's Waste Officer who confirmed that they have no objections to the proposals at this stage, however requested a condition (17) be added to secure details of the proposed refuse enclosures and a site waste management plan.

Construction Management

- 10.96 A Construction Management Plan will be required to demonstrate how the development would not result in congestion on the highway. The document will require details in accordance with the guidance of the Code of Construction Practice for Construction Sites (CoPCS).

Sustainability

- 10.97 The NPPF confirms that the purpose of the planning system is to contribute to the achievement of sustainable development, and standards relevant to sustainability are set out throughout the NPPF. Paragraph 148, under section 14. 'Meeting the challenge of climate change, flooding and coastal change', highlights that the planning system should support the transition to a low carbon future in a changing climate, taking full account of flood risk and coastal change. It should help to: shape places in ways that contribute to radical reductions in greenhouse gas emissions, minimise vulnerability and improve resilience; encourage the reuse of existing resources, including the conversion of existing buildings; and support renewable and low carbon energy and associated infrastructure.

- 10.98 London Plan Policy S12 stipulates that major developments should be net zero-carbon and that a detailed energy strategy should demonstrate how the zero-carbon target will be met. S12 also says that major developments should achieve on-site carbon reductions of at least 35% beyond Building Regulations and where the zero-carbon target cannot be fully achieved on-site, the shortfall should be provided through a contribution to the borough's carbon offset fund. Policy S14 states that major development proposals should demonstrate how they will reduce the potential for internal overheating and reliance on air conditioning. Policy S113 states that developments should aim to achieve greenfield run-off rates and ensure that surface water run-off is managed as close to its source as possible. There should be a preference for green over grey features and proposals for impermeable surfacing should be resisted unless they can be shown to be unavoidable.
- 10.99 All development is required to demonstrate that it has minimised onsite carbon dioxide emissions by maximising energy efficiency, supplying energy efficiently and using onsite renewable energy generation (CS10). Major developments should achieve a total (regulated and unregulated) CO2 emissions reduction of at least 27% relative to total emissions from a building which complies with Building Regulations 2013 (39% where connection to a Decentralised Heating Network is possible). Typically, all remaining CO2 emissions should be offset through a financial contribution towards measures which reduce CO2 emissions from the existing building stock (CS10).
- 10.100 The Core Strategy also requires developments to address a number of other sustainability criteria such as climate change adaptation, SUDS, sustainable transport, sustainable construction and the enhancement of biodiversity. Development Management Policy DM7.1 requires for development proposals to integrate best practice sustainable design standards and states that the council will support the development of renewable energy technologies, subject to meeting wider policy requirements. Details and specifics are provided within Islington's Environmental Design SPD. Development Management Policy DM7.4 also requires major developments to comply with Islington's Code of Practice for Construction Sites and to achieve relevant water efficiency targets as set out in the BREEAM standards.
- 10.101 Whilst this application relates to a major development due to the site area, the proposals relate to external alterations only and there is no planned demolition, uplift in floorspace or changes to the accommodation schedule. As such, it would not be reasonable to meet all the sustainability requirements set out in local policy for major developments. Rather, all efforts have been made to ensure the proposals meet the sustainability requirements set out for all developments in the borough.

Replacement of Windows

- 10.102 It is proposed to replace all existing windows on the estate with new, double-glazed windows. It is understood that existing windows and balcony doors are in poor condition, leading to issues with usability, damp, condensation, heat loss and draughts. The proposal would improve the thermal efficiency of the flats within the estate, which is an environmental benefit and one which is encouraged by Core Strategy policy CS10, Development Management policy DM7.2 and Islington's Urban Design Guide
- 10.103 The existing window frames on the estate are uPVC, a material which has a high level of embodied carbon. It is proposed that the replacement window frames be made from aluminium composite. Aluminium composite frames, in addition to being more durable and requiring less maintenance than uPVC frames, have a lower embodied carbon than uPVC and can be recycled at the end of their lifetime.
- 10.104 The Urban Design Guide notes that 'the U-value of double-glazed windows is significantly lower than single glazing, regardless of framing materials' and as such, it should be considered that

double glazed aluminium framed windows can achieve the same level of thermal efficiency as uPVC framed windows.

10.105 Given the above assessment, the use of aluminium framed windows is considered to represent a marked improvement against the existing uPVC windows and would be in line with policy objectives which requires all developments to minimise the environmental impact of materials through use of sustainably sourced, low impact and recycled materials.

SUDS

10.106 Policy CS10 of the Islington Core Strategy requires all development to demonstrate that it is designed to be adapted to climate change, particularly through design which minimises overheating and incorporates sustainable drainage systems. Policy DM6.6 of the Islington Development Management Policies is concerned with flood prevention and requires that schemes must be designed to reduce surface water runoff to a 'greenfield rate', where feasible.

10.107 The application site is Flood Risk Zone 1 and therefore has a low probability of flooding from tidal or fluvial sources. In terms of SUDS, the applicant proposes the following features to provide interception of the first 5mm of rainfall:

- Small scale green roofs are proposed to new cycle and refuse stores across the Site
- Permeable / porous paving is proposed to replace impermeable surfacing on existing parking bays, footpaths, and public realm areas
- Rain gardens are proposed in existing low points within the landscaped areas where traditional gully/drainage inlets are currently used to drain surface water runoff. Rain gardens are also proposed to drain existing private road catchments where possible.

10.108 As noted above, due to the nature of the proposals, the Council's Sustainability Officer has confirmed that they do not consider it necessary for the application to achieve the standards set out in local policy relating to reducing runoff rates and attenuation storage. Nevertheless, these features are all welcomed, and it is considered that they would have some impact on reducing the rate and volume of surface water discharging into the existing onsite drainage network. To ensure that these proposals are effective, conditions will be added to the decision notice which requires confirmation of the proposals, details of a SuDS maintenance plan (9), and details of all proposed green roofs (13).

Other Matters

10.109 The submitted Design and Access Statement includes details of additional sustainability measures which may come forward in a future application. These proposals include adding insulation to the building walls, upgrading to triple glazed windows, introducing alternative heating and ventilation systems, and adding solar PV panels. These do not form part of the application and therefore cannot be considered here.

10.110 A number of representations have been received raising concerns over the consultation process and how results are reported. The applicant has submitted a Statement of Community Involvement which is a validation requirement but details the engagement undertaken. Notwithstanding this, notification of the planning application was undertaken in accordance with statutory requirements and the Islington Statement of Community Involvement.

10.111 Representations have been received which note that Newlon have not been carrying out cyclical works prior to this application being submitted. It is not considered that this is relevant to the consideration of this planning application.

10.112 Representations have been received which object to the lack of detail regarding the phasing of the proposed works. A phasing plan is included within the submitted Design and Access Plan. It is not considered that further details are required.

10.113 Representations have been received regarding the refurbishment works. The internal works do not form part of this application and it is not considered that these details are relevant to the consideration of this planning application.

11. SUMMARY AND CONCLUSION

Summary

11.1 The proposed new fenestration and doors at upper level are well-considered and respond sympathetically to the existing architecture on the estate and the adjacent conservation area. While concerns are raised regarding the design quality of the communal entrance doors, details are recommended to be secured by condition. The external works would result in more energy efficient buildings which would be a material benefit to residents and would support local policy which seeks to make developments as sustainable as possible.

11.2 The proposed landscaping improvements would result in a greener, more biodiverse estate with improved sustainable drainage. Improved pedestrian routes and new cycles stores, and the rationalisation of parking spaces across the estate, support local policy which promotes sustainable modes of transport such as walking and cycling. Additional security and lighting measures will ensure that Old Barnsbury is safer for residents and members of the public using the improved routes within the estate. Additional wayfinding and inclusive design improvements would mean that the estate is more accessible. The increased play provision is also considered a planning benefit.

11.3 For the reasons given above and explained in more detail in the assessment section of this report, the proposal is considered to be acceptable and in accordance with relevant planning policy and is therefore recommended for approval subject to conditions.

Conclusion

11.4 It is recommended that planning permission be granted subject to conditions as set out in Appendix 1 - RECOMMENDATIONS.

APPENDIX 1 – RECOMMENDATIONS

RECCOMENDATION A

That the grant of planning permission be subject to **conditions** to secure the following, and that there is delegated to each of the following: the Head of Development Management the Team Leader Major Applications and the Team Leader Planning Applications to make minor changes (additions removals or amendments) to the conditions:

1	<p>COMMENCEMENT (3 YEAR CONSENT PERIOD)</p> <p>CONDITION: The development hereby permitted shall be begun not later than the expiration of three years from the date of this permission.</p> <p>REASON: To comply with the provisions of Section 91(1) (a) of the Town and Country Planning Act 1990 as amended by the Planning and Compulsory Purchase Act 2004 (Chapter 5).</p>
2	<p>APPROVED PLANS LIST</p> <p>CONDITION: The development hereby approved shall be carried out in accordance with the following approved plans:</p> <p>OBE-PTE-ZZ-00-DR-A-10001 Rev. C1; OBE- PTE- ZZ-00-DR-A-10002 Rev. C1; OBE- PTE- ZZ-ZZ-DR-A-10300 Rev. C1; OBE- PTE- ZZ-ZZ-DR-A-10200 Rev. C1; OBE- PTE- VC-ZZ-DR-A-10300 Rev. C1; OBE- PTE- VB-ZZ-DR-A-10300 Rev. C1; OBE- PTE- VB-ZZ-DR-A-10301 Rev. C1; OBE- PTE- VO-ZZ-DR-A-10300 Rev. C1; OBE- PTE- VF-ZZ-DR-A-10300 Rev. C1; OBE- PTE- VF-ZZ-DR-A-10301 Rev. C1; OBE- PTE- VP-ZZ-DR-A-10300 Rev. C1; OBE- PTE- VR-ZZ-DR-A-10300 Rev. C1; OBE- PTE- VS-ZZ-DR-A-10300 Rev. C1; OBE- PTE- VS-ZZ-DR-A-10301 Rev. C1; OBE- PTE- VV-ZZ-DR-A-10300 Rev. C1; OBE- PTE- ZZ-ZZ-DR-A-10301 Rev. C1; OBE- PTE- ZZ-ZZ-DR-A-10201 Rev. C1; OBE- PTE- VB-ZZ-DR-A-10302 Rev. C1; OBE- PTE- VB-ZZ-DR-A-10303 Rev. C1; OBE- PTE- VC-ZZ-DR-A-10301 Rev. C1; OBE- PTE- VO-ZZ-DR-A-10301 Rev. C1; OBE- PTE- VF-ZZ-DR-A-10302 Rev. C1; OBE- PTE- VF-ZZ-DR-A-10303 Rev. C1; OBE- PTE- VP-ZZ-DR-A-10301 Rev. C1; OBE- PTE- VR-ZZ-DR-A-10301 Rev. C1; OBE- PTE- VS-ZZ-DR-A-10302 Rev. C1; OBE- PTE- VS-ZZ-DR-A-10303 Rev. C1; OBE- PTE- VV-ZZ-DR-A-10301 Rev. C1; OBE- PTE- ZZ-ZZ-DR-A-10400 Rev. C1; OBE- PTE- ZZ-ZZ-DR-A-10401 Rev. C1; OBE- PTE- VV-00-DR-A-10400 Rev. C1; 732-FH-OB-00-DP-L-101 Rev. B; 732-FH-OB-00-DP-L-201 Rev. B; 732-FH-OB-00-DP-L-301 Rev. B; 732-FH-OB-00-DP-L-401 Rev. B; 732-FH-XX-00-SK-L-501 Rev. A; 732-FH-XX-00-SK-L-502 Rev. A; 732FH-XX-00-DP-L-601; 500800-A101; 500800-A102; 500800-A103; 500800-A104.</p> <p>Cover Letter prepared by Lichfields, dated 22nd June 2022;</p> <p>Planning Statement prepared by Lichfields, dated June 2022;</p> <p>Planning Statement Addendum prepared by Lichfields, dated 11th November 2022;</p> <p>Design and Access Statement prepared by Pollard Thomas Edwards, dated 17th June 2022;</p> <p>732-FH-RP-02 – Landscape Statement prepared by Farrar Huxley, dated June 2022;</p> <p>732-FH-RP-03 Landscape Addendum prepared by Farrar Huxley, dated November 2022;</p> <p>SHA 769 Arboricultural Impact Assessment Report prepared by Sharon Hosegood Associates, dated June 2022;</p> <p>SHA 769 Arboricultural - Tree Survey prepared by Sharon Hosegood Associates, dated 21st March 2022;</p> <p>SHA 769 TSP Rev.B – Tree Survey Plan prepared by Sharon Hosegood Associates, dated 31st March 2022;</p> <p>Statement of Community Involvement prepared by Newlon Housing Trust, dated June 2022;</p> <p>Statement of Community Involvement – Appendices prepared by Newlon Housing Trust, dated June 2022;</p>

	<p>Statement of Community Involvement Addendum prepared by Newlon Housing Trust, dated November 2022; Preliminary Ecological Appraisal prepared by Greengage, dated June 2022; Biodiversity Impact Assessment prepared by Greengage, dated June 2022; Bat Survey Report prepared by Greengage, dated June 2022; Lighting Assessment prepared by Light Follows Behaviour, dated June 2022; Flood Risk Assessment and Surface Water Drainage Strategy Rev.2 prepared by Stantec, dated 20th June 2022.</p> <p>REASON: To comply with Section 70(1) (a) of the Town and Country Act 1990 as amended and the Reason for Grant and also for the avoidance of doubt and in the interest of proper planning.</p>
3	MATERIALS (DETAILS)
	<p>CONDITION: Details and samples of all facing materials shall be submitted to and approved in writing by the Local Planning Authority prior to works commencing on site. The details and samples shall include:</p> <ul style="list-style-type: none"> a) Window, balcony doors and front door treatment (including sections and reveals); b) Communal entrances; c) Boundary treatments; d) Any other materials to be used. <p>The development shall be carried out strictly in accordance with the details so approved and shall be maintained as such thereafter into perpetuity.</p> <p>REASON: In the interest of securing sustainable development and to ensure that the resulting appearance and construction of the development is of a high standard and preserves the character and appearance of the surrounding area.</p>
4	RETENTION OF CHIMNEY STRUCTURES (COMPLIANCE/DETAILS)
	<p>CONDITION: Notwithstanding the plans hereby approved, no changes to the chimney structures at the site shall be permitted unless otherwise approved in writing by the local planning authority.</p> <p>REASON: In the interest of preserving the character and appearance of the surrounding area.</p>
5	COMMUNAL ENTRANCES (DETAILS)
	<p>CONDITION: Notwithstanding the plans hereby approved, details of all communal entrances shall be submitted and approved in writing by the Local Planning Authority prior to the installation of any new windows. These details shall include scaled elevation drawings, details of signage, details of inclusive design features, and confirmation of location of control panels.</p> <p>A contrast of 15 LRV points shall be provided between door frames and the doors. All signage will also achieve a contrast of 30 LRV points.</p> <p>The communal entrances shall be implemented strictly in accordance with the details approved prior to the completion of works and shall be maintained as such thereafter.</p> <p>REASON: To ensure that the resulting appearance and construction of the development is of a high standard, preserves the character and appearance of the surrounding area, and ensures the development is accessible.</p>

6	CONSTRUCTION ENVIRONMENTAL MANAGEMENT PLAN (DETAILS)
	<p>CONDITION: A Construction Environmental Management Plan assessing the environmental impacts (including (but not limited to) highways impacts, biodiversity, noise, air quality including dust, smoke and odour, vibration and TV reception) of the development (including demolition) shall be submitted to and approved in writing by the Local Planning Authority prior to any works commencing at the site.</p> <p>The report shall assess impacts during the construction phase of the development on nearby residents and other occupiers together with means of mitigating any identified impacts.</p> <p>The development shall be carried out strictly in accordance with the details so approved and no change therefrom shall take place without the prior written consent of the Local Planning Authority.</p> <p>REASON: In the interests of residential amenity, highway safety and the free flow of traffic on streets, and to mitigate the impacts of the development on biodiversity. The imposition of this prior to commencement planning condition is considered necessary to prevent commencement of works until the requirements have been met because the timing of compliance is fundamental to the decision to grant planning permission.</p>
7	LANDSCAPING (DETAILS)
	<p>CONDITION: Notwithstanding the submitted detail and the development hereby approved a landscaping scheme shall be submitted to and approved in writing by the Local Planning Authority prior to the completion of the installation of windows on the final block or five years after the decision date, whichever is sooner. The scheme shall include the following details:</p> <ul style="list-style-type: none"> a) existing and proposed underground services and their relationship to both hard and soft landscaping; b) proposed trees: their location, species, size and section showing rooting area; c) soft plantings: including grass and turf areas, vertical greening, shrub and herbaceous areas; d) topographical survey: including earthworks, ground finishes, top soiling with both conserved and imported topsoil(s), levels, drainage and fall in drain types; e) enclosures and boundary treatment: including types, dimensions and treatments of walls, fences, screen walls, barriers, rails, retaining walls and hedges; f) hard landscaping: including ground surfaces, kerbs, edges, ridge and flexible pavings, unit paving, furniture, steps and if applicable synthetic surfaces; g) inclusive design principles adopted in the landscaped features; h) wayfinding and signage, including a map showing accessible routes; i) all playspace equipment and structures; j) scaled elevations of all gate structures on the estate; and k) any other landscaping feature(s) forming part of the scheme. <p>The submitted details shall show that the landscaping scheme will achieve an Urban Greening Factor of at least 0.241.</p> <p>All landscaping in accordance with the approved scheme shall be completed / planted during the first planting season following practical completion of the development hereby approved in accordance with the approved planting. The landscaping and tree planting shall have a two year maintenance / watering provision following planting and any existing tree shown to be retained or trees or shrubs to be planted as part of the approved landscaping scheme which are removed, die, become severely damaged or diseased within five years of completion of the development shall be replaced with the same species or an approved alternative to the satisfaction of the Local Planning Authority within the next planting season.</p>

	<p>The development shall be carried out strictly in accordance with the details so approved and shall be maintained as such thereafter.</p> <p>REASON: In the interest of biodiversity, sustainability, playspace and to ensure that a satisfactory standard of visual amenity is provided and maintained.</p>
8	LANDSCAPING MAINTENANCE PLAN
	<p>CONDITION: Prior to the commencement of the landscaping works hereby approved, a maintenance plan for the proposed landscaping scheme shall be submitted to and approved in writing by the Local Planning Authority.</p> <p>The maintenance plan shall include (but shall not be limited to) details of how the following landscaping features shall be maintained:</p> <ul style="list-style-type: none"> - Meadow planting - Vertical greening - Boundary hedges <p>REASON: In the interest of biodiversity value and visual amenity of the landscaping scheme is maintained.</p>
9	SUSTAINABLE DRAINAGE (DETAILS)
	<p>CONDITION: A detailed Sustainable Urban Drainage System (SUDS) scheme inclusive of detailed implementation and a maintenance and management plan of the SUDS scheme shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of landscaping works. Those details shall include:</p> <ul style="list-style-type: none"> (i) a timetable for its implementation, and (ii) a management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public body or statutory undertaker, or any other arrangements to secure the operation of the sustainable drainage scheme throughout its lifetime. <p>The submitted details shall include the scheme's peak runoff rate and storage volume.</p> <p>The scheme shall thereafter be managed and maintained in accordance with the approved details.</p> <p>REASON: To ensure that sustainable management of water and minimise the potential for surface level flooding.</p>
10	NESTING BOXES (DETAILS)
	<p>CONDITION: Details of swift and bat nesting boxes/bricks shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of landscaping works or five years after decision date, whichever is sooner.</p> <p>The submitted details shall show that the proposed nesting box strategy adequately replaces any habitats lost as a result of works on the estate.</p> <p>The nesting boxes/bricks shall be provided strictly in accordance with the details so approved, installed prior to the completion of works.</p> <p>REASON: To ensure the development provides the maximum possible provision towards creation of habitats and valuable areas for biodiversity.</p>

11	<p>TREE PROTECTION (DETAILS)</p> <p>CONDITION: Prior to commencement of any development at the site, a scheme for the protection of the retained trees, in accordance with BS 5837:2012, including a tree protection plan(s) (TPP) and an arboricultural method statement (AMS) shall be submitted to and approved in writing by the Local Planning Authority.</p> <p>Specific issues to be dealt with in the TPP and AMS:</p> <ul style="list-style-type: none"> a) Location and installation of services/ utilities/ drainage. b) Methods of demolition within the root protection area (RPA as defined in BS 5837: 2012) of the retained trees. c) Details of construction within the RPA or that may impact on the retained trees. d) a full specification for the installation of boundary treatment works. e) a full specification for the construction of any roads, parking areas and driveways, including details of the no-dig specification and extent of the areas of the roads, parking areas and driveways to be constructed using a no-dig specification. Details shall include relevant sections through them. f) Detailed levels and cross-sections to show that the raised levels of surfacing, where the installation of no-dig surfacing within Root Protection Areas is proposed, demonstrating that they can be accommodated where they meet with any adjacent building damp proof courses. g) A specification for protective fencing to safeguard trees during both demolition and construction phases and a plan indicating the alignment of the protective fencing. h) a specification for scaffolding and ground protection within tree protection zones. i) Tree protection during construction indicated on a TPP and construction and construction activities clearly identified as prohibited in this area. j) details of site access, temporary parking, on site welfare facilities, loading, unloading and storage of equipment, materials, fuels and waste as well concrete mixing and use of fires k) Boundary treatments within the RPA l) Methodology and detailed assessment of root pruning m) Reporting of inspection and supervision n) Methods to improve the rooting environment for retained and proposed trees and landscaping <p>The development thereafter shall be implemented in strict accordance with the approved details.</p> <p>REASON: Required prior to commencement of development to satisfy the Local Planning Authority that the trees to be retained will not be damaged during demolition or construction and to protect and enhance the appearance and character of the site and locality.</p>
12	<p>TREE PLANTING (DETAILS)</p> <p>CONDITION: prior to the commencement of landscaping works, full details of all proposed tree planting shall be submitted to and approved in writing by the Local Planning Authority. This will include planting and maintenance specifications, including cross-section drawings, use of guards or other protective measures and confirmation of location, species and sizes, nursery stock type, supplier and defect period. All tree planting shall be carried out in accordance with those details and at those times.</p> <p>Any trees that are found to be dead, dying, severely damaged or diseased within five years of the completion of the building works OR five years of the carrying out of the landscaping</p>

	<p>scheme (whichever is later), shall be replaced in the next planting season by specimens of similar size and species in the first suitable planting season.</p> <p>REASON: To comply with the duties indicated in Section 197 of the Town and Country Planning Act 1990 to safeguard and enhance the amenity of the area, to maximise the quality and usability of open spaces within the development, and to enhance its setting within the immediate locality.</p>
13	GREEN/BROWN BIODIVERSITY ROOF (DETAILS)
	<p>CONDITION: Prior to the commencement of landscaping works, details of the biodiversity (green/brown) roofs shall be submitted to and approved in writing by the Local Planning Authority.</p> <p>The green/brown roof shall:</p> <ol style="list-style-type: none"> Be biodiversity based with extensive substrate base (depth 80 -150mm); Contribute towards a 50% reduction in surface water run-off; and Be planted/seeded with a mix of species within the first planting season following the practical completion of the building works (the seed mix shall be focused on wildflower planting, and shall contain no more than a maximum of 25% sedum). <p>The biodiversity (green/brown) roofs should be maximised across the site and shall not be used as an amenity or sitting out space of any kind whatsoever and shall only be used in the case of essential maintenance or repair, or escape in case of emergency.</p> <p>The biodiversity roof(s) shall be carried out strictly in accordance with the details as approved, shall be laid out within 3 months of next available appropriate planting season after the construction of the building it is located on and shall be maintained as such thereafter.</p> <p>REASON: To ensure the development provides the maximum possible provision towards creation of habitats, valuable areas for biodiversity and minimise run-off.</p>
14	LIGHTING PLAN (DETAILS)
	<p>CONDITION: Full details of the lighting across the site shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of landscaping works.</p> <p>The details shall include the location and full specification of: all lamps; light levels/spill lamps, floodlights, support structures, hours of operation and technical details on how impacts on bat foraging will be minimised. The lighting measures shall be carried out strictly in accordance with the details so approved, shall be installed prior to the completion of works and shall be maintained as such thereafter.</p> <p>REASON: To ensure that any resulting general or security lighting is appropriately located, designed do not adversely impact neighbouring residential amenity and are appropriate to the overall design of the buildings as well as protecting the biodiversity value of the site.</p>
15	SECURITY MEASURES (Details)
	<p>CONDITION: Details of the site wide general safety and security measures shall be submitted to and approved in writing by the Local Planning Authority prior to the installation of any new gates.</p> <p>The details shall include the location and full specification of all proposed security measures, including (but not limited to):</p> <ul style="list-style-type: none"> - Timing of gate closures

	<ul style="list-style-type: none"> - Fob-access gates; - CCTV; - Security lighting. <p>The details shall also include an operational management plan detailing proposed maintenance measures, opening hours of gates, fob access arrangements, and confirmation of which residents will have access to which areas of the estate.</p> <p>REASON: In the interest of safety and security, to ensure the measures are appropriately located and to ensure that a satisfactory standard of visual amenity is provided and maintained.</p>
16	CYCLE PARKING (DETAILS)
	<p>CONDITION: Notwithstanding the approved plans, details of the bicycle storage, including details of accessible cycle storage, shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of landscaping works. The approved bicycle stores shall be provided prior to the completion of works and shall be maintained as such thereafter.</p> <p>REASON: To ensure adequate cycle parking is available and easily accessible on site, to promote sustainable modes of transport and to secure the high quality design of the structures proposed.</p>
17	REFUSE AND RECYCLING (DETAILS)
	<p>CONDITION: Details of the site-wide waste strategy for the development shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of landscaping works. The details shall include:</p> <ol style="list-style-type: none"> a) the layout, design, inclusive design measures, and appearance (shown in context) of the dedicated refuse/recycling enclosure(s); b) a site waste management plan outlining how refuse collections will take place. <p>The development shall be carried out and operated strictly in accordance with the details and waste management strategy so approved. The refuse enclosures shall be erected prior to the completion of works and shall be maintained as such thereafter.</p> <p>REASON: To secure the necessary physical waste enclosures to support the development and to ensure that responsible waste management practices are adhered to.</p>
18	CAR PARKING (DETAILS)
	<p>CONDITION: Details of a car parking rationalisation strategy shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of landscaping works.</p> <p>The strategy shall show a reduction of at least 28 parking spaces on the estate and shall detail how car parking spaces have been minimised to allow for the maximisation of landscaping across the estate. The details shall also demonstrate how sufficient accessible parking has been provided where it is required.</p> <p>REASON: In the interest of maximising the available space for landscaping.</p>
19	WALKING AND CYCLING (DETAILS)
	<p>CONDITION: Notwithstanding the plans hereby approved, further details of how priority will be given to walking and cycling shall be submitted to and approved in writing by the Local</p>

	<p>Planning Authority prior to the commencement of landscaping works or five years after decision date, whichever is sooner.</p> <p>The details shall include arrangements for the pedestrian routes through the estate, will demonstrate how pedestrian safety will be prioritised in car parking areas, and will demonstrate that opportunities to widen footways have been fully explored. The development shall be constructed strictly in accordance with the details so approved, shall be maintained as such thereafter and no change there from shall take place without the prior written consent of the Local Planning Authority.</p> <p>REASON: To ensure adequate provision for pedestrians and cyclists, to promote sustainable modes of transport and to secure the high-quality, inclusive design of the infrastructure proposed.</p>
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List of Informatives:

1	Construction works
	<p>INFORMATIVE: Noise from construction works is subject to control under the Control of Pollution Act 1974. You must carry out any building works that can be heard at the boundary of the site only between 08.00 and 18.00 hours Monday to Friday and 08.00 to 13.00 on Saturday and not at all on Sundays and Public Holidays. You are advised to consult the Pollution Team, Islington Council, 222 Upper Street London N1 1XR (Tel. No. 020 7527 3258 or by email pollution@islington.gov.uk) or seek prior approval under Section 61 of the Act if you anticipate any difficulty in carrying out construction other than within the hours stated above.</p>
2	Highways Requirements
	<p>INFORMATIVE: Compliance with sections 168 to 175 and of the Highways Act, 1980, relating to "Precautions to be taken in doing certain works in or near streets or highways". This relates, to scaffolding, hoarding and so on. All licenses can be acquired through streetworks@islington.gov.uk. All agreements relating to the above need to be in place prior to works commencing. Can be gained through streetworks@islington.gov.uk. Section 50 license must be agreed prior to any works commencing. Joint condition survey required between Islington Council Highways and interested parties before commencement of building works to catalogue condition of streets and drainage gullies. Contact highways.maintenance@islington.gov.uk.</p>
3	Highways (Additional)
	The Public footpath should not be obstructed at site entrances during construction.
4	Tree Works
	<p>INFORMATIVE: The following British Standards should be referred to:</p> <ul style="list-style-type: none"> a. BS: 3998:2010 Tree work – Recommendations b. BS: 5837 (2012) Trees in relation to demolition, design and construction – Recommendations.
5	Thames Water: Source Protection Zone
	<p>The development boundary falls within one of Thames Water's Source Protection Zone for groundwater abstraction. You are encouraged to read the Environment Agency's approach to groundwater protection (available at https://www.gov.uk/government/publications/groundwater-protection-position-</p>

	statements) and to engage a suitably qualified environmental consultant to discuss the implication of this on the hereby approved development.
6	Thames Water: Groundwater Risk Management
	A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. Thames Water expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 020 3577 9483 or by emailing trade.effluent@thameswater.co.uk . Application forms should be completed on line via www.thameswater.co.uk . Please refer to the Wholesale; Business customers; Groundwater discharges section.
7	Internal Works
	The proposed internal works should not prevent any of the works outlined on page 87 of the hereby approved Design and Access Statement, which aim to help the estate achieve net zero status, from being carried out in the future.
8	Bats
	The submitted Bat Survey notes that two roosts were found on the Old Barnsbury Estate, which is significant for Islington. It will be necessary to obtain a licence from Natural England before any works are carried out which may disturb the bats using these roosts.

APPENDIX 2 - RELEVANT DEVELOPMENT PLAN POLICIES AND GUIDANCE

This appendix lists all relevant development plan policies and guidance notes pertinent to the determination of this planning application.

1. National and Regional Guidance

The National Planning Policy Framework 2021 seeks to secure positive growth in a way that effectively balances economic, environmental and social progress for this and future generations. The NPPF is a material consideration and has been taken into account as part of the assessment of these proposals.

- NPPF (2021)

2. Development Plan

The Development Plan is comprised of the London Plan 2021, Islington Core Strategy 2011, Development Management Policies 2013 and Site Allocations 2013. The following policies of the Development Plan are considered relevant to this application:

A) The London Plan 2021 - Spatial Development Strategy for Greater London

Policy GG1 Building strong and inclusive communities

Policy GG3 Creating a healthy city

Policy GG6 Increasing efficiency and resilience

Policy D3 Optimising site capacity through the design-led approach

Policy D4 Delivering good design

Policy D5 Inclusive design
Policy D6 Housing quality and standards
Policy D8 Public Realm
Policy D11 Safety, security and resilience to emergency
Policy H8 Loss of existing housing and estate redevelopment
Policy S4 Play and informal recreation
Policy G4 Green infrastructure
Policy G4 Open Space
Policy G5 Urban greening
Policy G6 Biodiversity and access to nature
Policy G7 Trees and woodlands
Policy SI1 Improving air quality
Policy SI2 Minimising greenhouse gas emissions
Policy SI3 Energy infrastructure
Policy SI12 Flood risk management
Policy SI13 Sustainable drainage
Policy T1 Strategic approach to transport
Policy T2 Healthy Streets
Policy T4 Assessing and mitigating transport impacts
Policy T5 Cycling
Policy T6 Car Parking
Policy T6.1 Residential parking
Policy T7 Deliveries, servicing and construction

B) Islington Core Strategy 2011

Policy CS6 (King's Cross)
Policy CS8 Enhancing Islington's Character
Policy CS9 Protecting and enhancing Islington's built and historic environment
Policy CS10 Sustainable design
Policy CS11 Waste
Policy CS12 Meeting the Housing Challenge
Policy CS15 (Open Space and Green Infrastructure)
Policy CS16 (Play Space)
Policy CS18 Delivery and infrastructure

C) Development Management Policies June 2013

Policy DM2.1 Design
Policy DM2.2 Inclusive Design
Policy DM2.3 Heritage
Policy DM3.4 Housing Standards
Policy DM3.5 Private Outdoor Space
Policy DM6.1 Healthy development
Policy DM3.6 Play space
Policy DM6.2 New and improved public open space
Policy DM6.3 Protecting open space
Policy DM6.5 Landscaping, trees and biodiversity
Policy DM6.6 Flood prevention
Policy DM7.1 Sustainable Design and Construction
Policy DM8.1 Movement hierarchy
Policy DM8.2 Managing transport impacts
Policy DM8.4 Walking and Cycling

Policy DM8.5 Vehicle Parking
Policy DM8.6 Delivery and servicing for new developments
Policy DM9.3 Implementation

3. Designations

The site has the following designations under the London Plan 2021, Islington Core Strategy 2011, Development Management Policies 2013 and Site Allocations 2013:

- Core Strategy Key Areas (Kings Cross)

4. Supplementary Planning Guidance (SPG) / Document (SPD)

The following SPGs and/or SPDs are relevant:

London Plan – Accessible London (2016), Character and Context (2014), Public London Charter (2021), Housing (2016), Housing Design Standards (2022), Optimising site capacity: A design-led approach (2022), Play and Informal Recreation (2012), Urban Greening Factor (2021), Sustainable Transport, Walking and Cycling guidance (2021).

Local Plan – Environmental Design (2012), Inclusive Design (2014), Inclusive Landscape Design (2010), Urban Design Guide (2017), Streetbook (2012), Regenerating Kings Cross (2005).

5. Emerging Policies

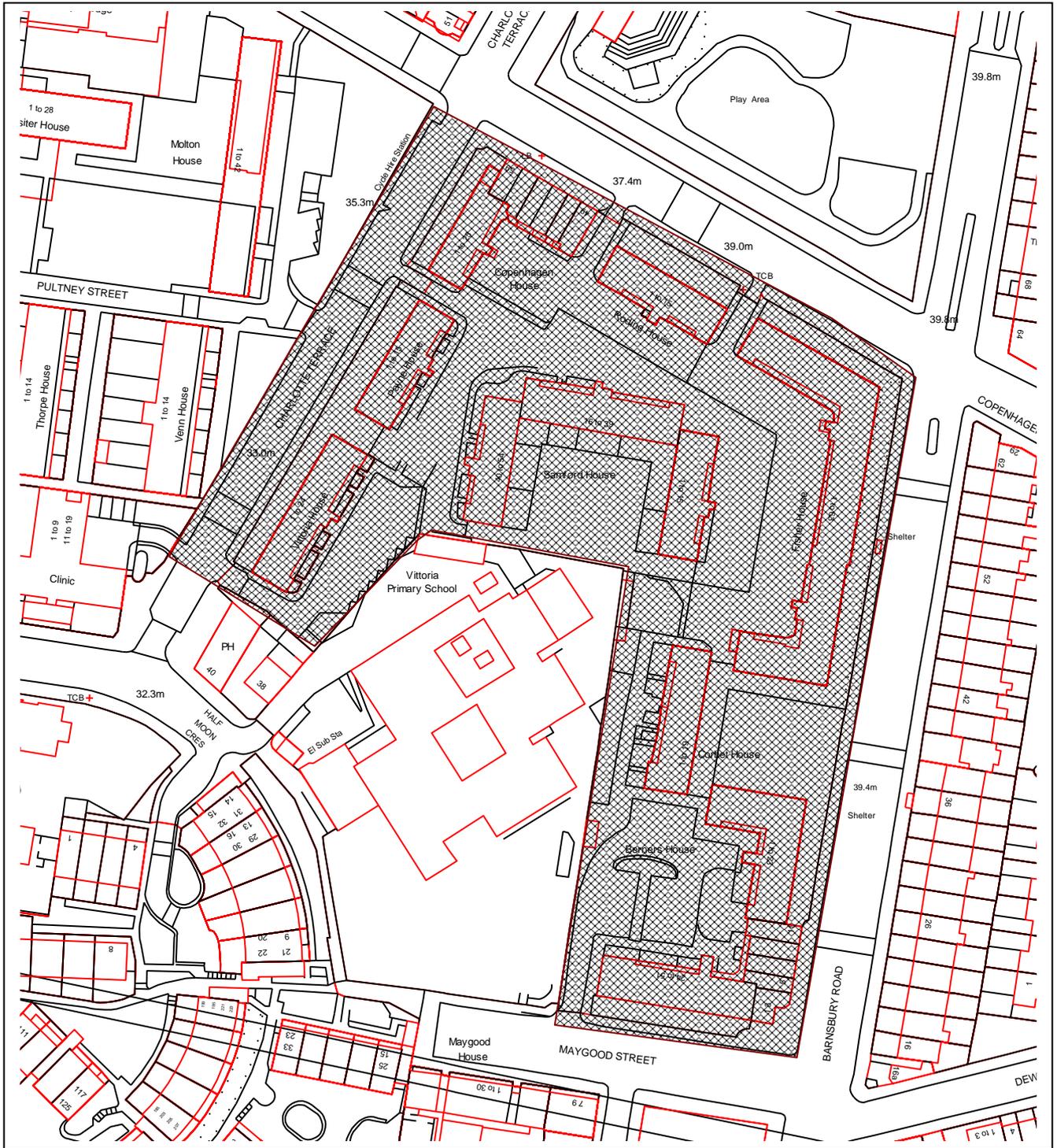
Draft Islington Local Plan (2019)

Emerging policies relevant to this application are set out below:

Policy PLAN1 Site appraisal, design principles and process
Policy SP2 Kings Cross and Pentonville Road
Policy H1 Thriving communities
Policy SC2 Play space
Policy G1 Green infrastructure
Policy G2 Protecting open space
Policy G4 Biodiversity, landscape design and trees
Policy G5 Green roofs and vertical greening
Policy S1 Delivering sustainable design
Policy S2 Sustainable design standards
Policy S4 Minimising greenhouse gas emissions
Policy S8 Flood risk management
Policy S9 Integrated water management and sustainable drainage
Policy T2 Sustainable transport choices
Policy T3 Car free development
Policy T4 Public realm
Policy T5 Delivery, servicing and construction
Policy DH1 Fostering innovation and conserving and enhancing the historic environment
Policy ST2 Waste

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Islington SE GIS Print Template



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P2022/2454/FUL

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**ADDENDUM TO PLANNING
COMMITTEE REPORT**

 Development Management Services
 Planning and Development Division
 Community Wealth Buildings

PLANNING COMMITTEE		AGENDA ITEM NO:
Date:	6 th December 2022	NON-EXEMPT

Application number	P2022/1221/FUL
Application type	Full Planning Application
Ward	Junction Ward
Listed building	Adjacent to Locally Listed 32 Junction Road
Conservation area	Adjacent to St John's Grove Conservation Area
Development Plan Context	Archway Town Centre Archway Key Area Site Allocations 2013 – ARCH1 Draft Site Allocations 2021 – ARCH1 Article 4 Direction – B1 to C3 (4 Vorley Road) Article 4 Direction – A1 to A2 Article 4 Direction - A1, B1, D1 to temporary 'flexible uses' London Underground Zone of Interest (Tunnels)
Licensing Implications	N/A
Site Address	Vorley Road Bus Stand & 4 Vorley Road, London N19 5JH
Proposal	Demolition of 620.9sqm community use part 1 / part 2 storey building; the construction of 72 new dwellings with associated private amenity space for affordable and private homes, provided in three blocks of 13 storeys, 8 storeys, and 2 to 6 storeys, improvements to the public realm, the provision of a Medical Centre (775sqm) and a library (766sqm), associated bicycle parking, mobility scooter stores, refuse stores and improvements to the public realm.

Case Officer	Brett McAllister
Applicant	London Borough of Islington - New Build
Agent	Sarah Eley - HTA Design

1. RECOMMENDATION

 The Committee is asked to resolve to **GRANT** planning permission:

1. For the reasons for approval;
2. Subject to the conditions set out in Appendix 1 (Recommendation B);
3. Subject to the prior completion of a Director's Letter securing the heads of terms as set out in Appendix 1 (Recommendation A);
4. Subject to any direction by the Mayor of London to refuse the application or for it to be called in for the determination by the Mayor of London.

2. REASON FOR DEFERRAL

- 2.1 Planning application P2022/1067/FUL was published on the Agenda for the Planning Committee on 8th November 2022. However, prior to the Planning Committee, it was identified that the application had not been advertised as a departure from the development plan, which is required under Part 3, Article 15 of the Town and Country Planning (Development Management Procedure)(England) Order 2015.
- 2.2 The previous Committee Report is appended as **Appendix 3**.

3. CONSULTATION

- 3.1 Further to the previous consultation, the application was re-consulted upon, noting that it represented a departure from the development plan. Letters were sent to occupants of 2,922 adjoining and nearby properties on 8th November 2022. A site notice and press advert were displayed on 10th November 2022. The public consultation of the application therefore expires on 1st December 2022. It is acknowledged that the expiry of the consultation period is after the date of publication for this addendum report. However, in any case, it is the Council's practice to continue to consider representations made up until the date of a decision. As such, should any representations be received after the publication date of this report, and up to the date of a decision, these will be given full consideration, reported to the Committee and where necessary an update provided regarding the assessment.
- 3.2 At the time of drafting the report, **2** additional representations had been received since the previous report was drafted. This results in a total of **26** objections and **2** letters of support. The issues raised are summarised below:
- The proposed development would place additional pressure on community infrastructure including nursery provision, parking, medical facilities and schools and would lead to congestion (*See paragraphs 11.2 to 11.34, 11.231 to 11.233 and 11.386 of Committee Report at Appendix 3*)
 - How many of the 72 dwellings will be affordable housing? and what is considered 'affordable'? (*See paragraphs 11.374 to 11.384 of Committee Report at Appendix 3*)

4. DEPARTURE FROM DEVELOPMENT PLAN

- 4.1 The application proposal represents a tall building, being over 30 metres in height. Islington Development Management Policy DM2.1 details that '*the only locations in Islington where tall buildings may be suitable are set out in the Finsbury Local Plan (Area Action Plan for Bunhill and Clerkenwell)*'. The application site is not located within the area covered by the Finsbury Local Plan and therefore represents a departure from the adopted development plan.
- 4.2 However, Draft Islington Local Plan policy DH3, as modified in the Main Modifications for Consultation (June 2022), details that buildings of more than 30 metres are only acceptable in-principle on sites identified as potentially suitable for tall building under this policy and must not exceed the maximum building heights for that specific tall building location, as set out in the site allocation. The application site forms Site Allocation ARCH1 in the Draft Islington Local Plan – Site Allocations (September 2019), where specific reference is made to the suitability of a building of up to 15 storeys (46 metres). Given the advanced stage of the draft plan and the conformity of the emerging policies with the Framework it is considered that policies can be afforded moderate to significant weight depending on the significance of objections to main modifications.
- 4.3 While the height of proposed Building B is 45.575 metres to the parapet, being just under the 46 metre maximum height stated in the draft Site Allocation and draft Local Plan, the proposed rooftop

plant enclosure takes the height of the building to total of 47.625 metres. As such, the proposal would also represent a departure from the draft Local Plan.

5. UPDATED ASSESSMENT

5.1 Since the previous Committee Agenda, the applicant has submitted an updated application form and Planning Statement to ensure that these include the height of the development. No material changes have been made to the application proposal since the previously published report. As stated above, the principal massing of Building B to the parapet would be 45.575 metres in accordance with the draft Site Allocation. The rooftop plant enclosure is comprised of two elements. The main centrally located pump room would exceed the 46m maximum height by 1.625 metres and the acoustic screening for the Air Source Heat Pumps, adjacent to the west, would exceed the maximum height by 0.475 metres (see Figure 1 below).

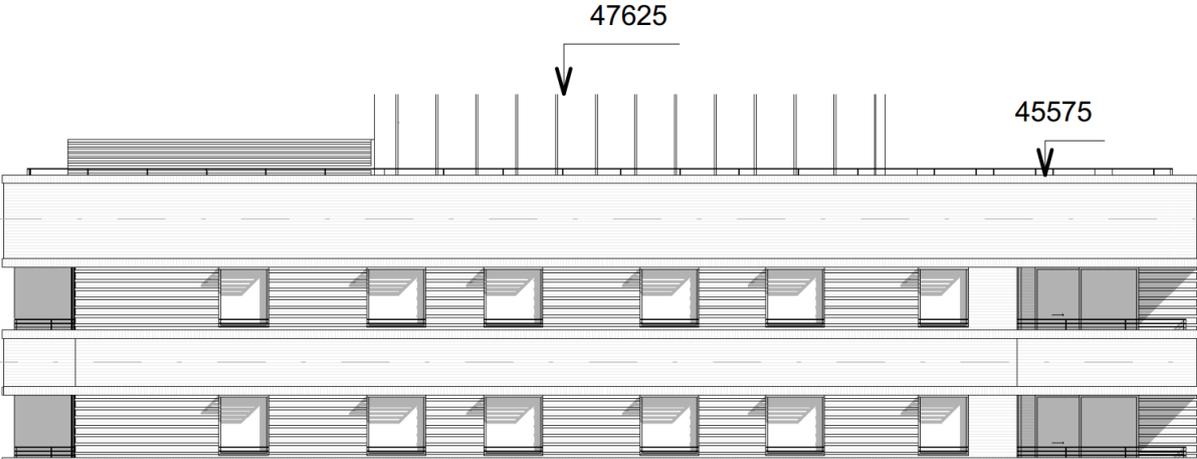


Figure 1 - Top of Building B – South Elevation

5.2 It can be seen from the roof plan below (Figure 2) that these elements would have a set back from the parapet on all sides, making it less visible, particularly from street level (see Figure 3 and 4 below). However, to further reduce its visibility, a condition is recommended relating to material finishes (11), requiring final details of the rooftop plant enclosure to be submitted to and approved in writing by the local planning authority. This will ensure that the roof plant screening has a lightweight and less solid appearance than is currently proposed.

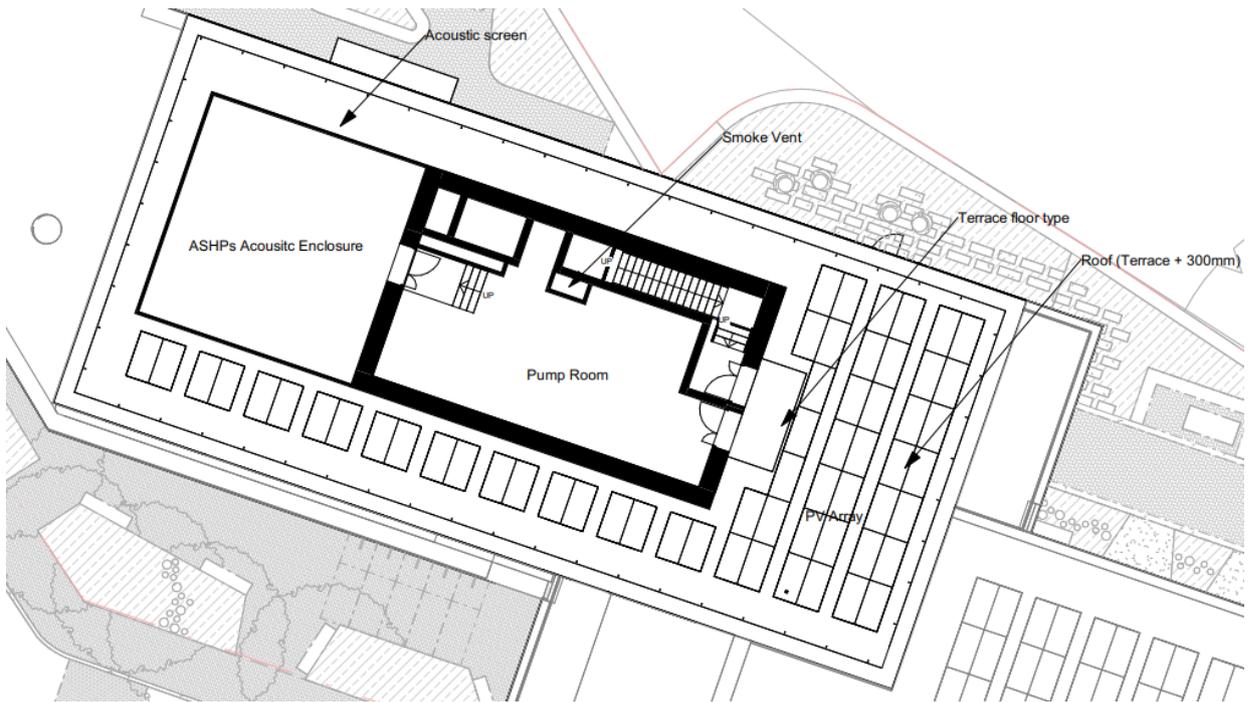


Figure 2 - Roof Plan – Building B



Figure 3 – View from Junction Road



Figure 4 – View from Junction Road (moving north)

5.3 While the additional projection above the stated site allocation heights would be visible in some longer views, particularly at higher ground levels to the northwest and west, given the limited extent of the plant area projection above the parapet, together with the setbacks, appropriate materiality that would be secure by condition and its setting, whereby it would be generally viewed in the context of the two higher buildings within the Archway Town Centre, the proposed limited projection of the plant above the site allocation height limit is considered, on balance, to be acceptable in this case.

6. PLANNING BALANCE AND REASON FOR APPROVAL

6.1 Following the deferral of the application prior to the 8th November 2022 Committee meeting officers have reconsulted on the application to note that the proposals represent a departure from the development plan.

6.2 The proposed new library, medical centre and housing would deliver on the draft Site Allocation for providing a residential-led development with community uses that includes a tall building forming part of an Archway cluster of tall buildings. The scale, form, materiality, elevation design and layout is well-considered and results in a scheme of high quality design that is sensitive to the local and wider context. The community uses would create a civic hub on Vorley Road that would support the vitality of Archway Town Centre. Although there would be instances of amenity and heritage impacts these are limited and must be weighed against the considerable public benefits, which in this case comprise the delivery of over 50% of onsite affordable housing, contribution of high-quality housing, the delivery of two new community uses and the delivery of an exemplary scheme regarding sustainability and energy efficiency.

6.3 For the reasons noted above and within the previous Committee report detailed at **Appendix 3**, it is considered that on balance, the harm created by the proposal, inclusive of the tall building exceeding the maximum stated height in the site allocation, which represents a departure from the development plan, is outweighed by the scheme benefits, notably the provision of new social rented housing, high quality new homes, new community facilities and the exemplar sustainability and energy efficiency measures.

7. CONCLUSION AND RECOMMENDATION

7.1 It is recommended that planning permission be granted subject to conditions and Director's Letter Heads of Terms as set out in **Appendix 1 - RECOMMENDATIONS** of this report.

APPENDIX 1 – RECOMMENDATIONS

RECOMMENDATION A

That the Committee resolve to GRANT planning permission subject to any direction by The Mayor to refuse the application or for it to be called in for determination by the Mayor of London. Therefore, following the Council's resolution to determine the application, the application shall then be referred to the Mayor of London in accordance with Article 5 of the Town and Country Planning (Mayor of London) Order 2008 – allowing him 14 days to decide whether to:

- A. allow the draft decision to proceed unchanged; or
- B. direct the Council under Article 6 to refuse the application; or
- C. issue a direction under Article 7 that he is to act as the Local Planning Authority for the purpose of determining the application.

And

That planning permission be granted for the reasons given in paragraph no. 6.1 to 6.3 and subject to the prior completion of a Directors' Letter between the Council and all persons with an interest in the land (including mortgagees) in order to secure the following planning obligations to the satisfaction of the Head of Law and Public Services and the Service Director, Planning and Development / Head of Service – Development Management or, in their absence, the Deputy Head of Service:

- On-site provision of affordable housing: 37 social rented units
- A financial contribution of £260,000 for Girdlestone Park play space upgrades.
- Construction Management Plan and Monitoring: in adherence to the Council's Code of Practice for Construction Sites, the provision of a Construction Management Plan, and a monitoring contribution of £17,400 (calculated at 72 residential units x £200, and 1561sqm of community floorspace – rounded to 1500sqm – calculated at 15 x £200)
- Compliance with the Code of Employment and Training.
- Compliance with the Code of Local Procurement.
- Compliance with the Code of Construction Practice, including a monitoring fee TBC and submission of site-specific response document to the Code of Construction Practice for approval of LBI Public Protection.
- The provision of 7 accessible parking bays or a contribution of £14,000 (£2,000 cost per bay) towards bays or other accessible transport initiatives.
- Highways Reinstatement: agreement from the developer to cover the cost of reinstating any highway damaged by the developer during the construction works
- Carbon Offset: A financial contribution of £57,542 towards offsetting any projected residual CO2 emissions of the development (63 tonnes and the standard offset rate of £920 per tonne)
- Green Performance Plan: to be submitted and approved prior to the proposed use being occupied
- Car free development- Removal of eligibility for residents' parking permits.
- Submission of draft full Travel Plan for Council approval prior to occupation, and of a full Travel Plan for Council approval 6 months from first occupation of the development or phase.

- Council's legal fees in preparing the Directors' agreement and officer's fees for the preparation, monitoring and implementation of the Directors agreement.
- All payments to the Council would be index-linked from the date of Committee and would be due upon implementation of the planning permission.

If the Committee resolve to grant, resolution will include provision to provide flexibility to officers to negotiate and finalise s106 on behalf of the Committee.

That, should the Directors' Letter not be completed within 13 weeks from the date when the application was made valid or within the agreed extension of time, the Service Director, Planning and Development / Head of Service – Development Management or, in their absence, the Deputy Head of Service may refuse the application on the grounds that the proposed development, in the absence of a Deed of Planning Obligation is not acceptable in planning terms.

ALTERNATIVELY, should this application be refused (including refusals on the direction of The Secretary of State or The Mayor) and appealed to the Secretary of State, the Service Director, Planning and Development / Head of Service – Development Management or, in their absence, the Deputy Head of Service be authorised to enter into a Directors' Letter to secure the heads of terms as set out in this report to Committee.

RECOMMENDATION B

That the grant of planning permission be subject to **conditions** to secure the following, and that there is delegated to each of the following: the Head of Development Management the Team Leader Major Applications and the Team Leader Planning Applications to make minor changes (additions removals or amendments) to the conditions:

1	Commencement
	<p>CONDITION: The development hereby permitted shall be begun not later than the expiration of three years from the date of this permission.</p> <p>REASON: To comply with the provisions of Section 91(1)(a) of the Town and Country Planning Act 1990 as amended by the Planning and Compulsory Purchase Act 2004 (Chapter 5)</p>
2	Approved plans and documents list
	<p>CONDITION: The development hereby approved shall be carried out in accordance with the following approved plans and documents:</p> <p><u>Plans:</u></p> <p><i>Existing:</i></p> <p>3468A-LB-XX-00-DP-A-1010-2 – Site Location Plan 3468A-LB-XX-00-DP-A-1020-1 – Existing Site Plan 3468A-LB-ZZ-XX-DE-A-1040-1 – Existing Site Elevations 3468A-LB-ZZ-XX-DE-A-1041-1 – Existing South Elevation 3468A-LB-ZZ-XX-DE-A-1042-1 – Existing North Elevation 3468A-LB-BB-XX-DS-A-1050-1 – Existing Sections</p> <p><i>Proposed:</i></p> <p>3468A-LB-ZZ-00-DP-A-1600-1 – Demolition Plan 3468A-LB-ZZ-G1-DP-A-1099-17 – Layout Basement 3468A-LB-ZZ-00-DP-A-1100-25 – Layout GF 3468A-LB-ZZ-01-DP-A-1101-18 - Layout Level 01 3468A-LB-ZZ-02-DP-A-1102-17 - Layout Level 02</p>

3468A-LB-ZZ-03-DP-A-1103-16 - Layout Level 03
 3468A-LB-ZZ-04-DP-A-1104-15 - Layout Level 04
 3468A-LB-ZZ-05-DP-A-1105-15 - Layout Level 05
 3468A-LB-ZZ-06-DP-A-1106-15 - Layout Level 06
 3468A-LB-ZZ-07-DP-A-1107-15 - Layout Level 07
 3468A-LB-ZZ-08-DP-A-1108-15 - Layout Level 08
 3468A-LB-ZZ-09-DP-A-1109-15 - Layout Level 09
 3468A-LB-ZZ-10-DP-A-1110-15 - Layout Level 10
 3468A-LB-ZZ-11-DP-A-1111-15 - Layout Level 11
 3468A-LB-ZZ-12-DP-A-1112-15 - Layout Level 12
 3468A-LB-ZZ-13-DP-A-1113-17 - Layout Level 13
 3468A-LB-ZZ-14-DP-A-1114-5 - Layout Roof Plan
 3468A-LB-ZZ-XX-DE-A-1300-15 – North Elevation
 3468A-LB-ZZ-XX-DE-A-1301-15 – South Elevation
 3468A-LB-ZZ-XX-DE-A-1302-15 – East/West Elevations
 3468A-LB-BA-XX-DS-A-1400-14 – Section A
 3468A-LB-BB-XX-DS-A-1401-14 – Section B
 3468A-LB-BC-XX-DS-A-1402-14 – Section C
 3468A-LB-BB-XX-DS-A-1404-11 – Site Sections
 3468A-LB-ZZ-XX-DE-A-1405-10 – Site Elevations
 3468A-LB-BA-XX-DS-A-1406-7 – Section A East Elevation
 3468A-LB-BB-XX-DS-A-1407-7 – Section B West Elevation
 3468A-LB-BC-XX-DS-A-1408-7 – Section C West Elevation
 3468A-LB-ZZ-XX-DS-A-1409-7 – Section C East Elevation
 3468A-LB-ZZ-ZZ-DS-A-1450-1 – Covered Entrance Section West
 3468A-LB-ZZ-ZZ-DS-A-1451-1 – Covered Entrance Section East
 3468A-LB-ZZ-ZZ-DE-A-3310-3 - A&B Gated Entrance
 3468A-LB-ZZ-ZZ-DE-A-350020-2 – Library Façade Bay Study
 3468A-LB-BC-ZZ-DE-A-350030-2 – Medical Centre bay Study
 3468A-LB-ZZ-ZZ-DD-A-3520-3 – Typical Window Detail
 3468A-LB-XX-ZZ-DD-A-3530-2 – Typical Parapet Details
 3468A-LB-XX-XX-DT-A-3540-1 – Louvre Types
 3468A-LB-XX-00-DD-A-3541-1 - Entrance gate & Bin Store Detail
 3468A-LB-ZZ-ZZ-DD-A-3800-3 – Balcony Detail
 3468-LB-ZZ-XX-SA-A-5001-7 – Tenure Plans
 3468A-LB-ZZ-XX-DR-L-6000 – Landscape Hardwork Plan
 3468A-LB-ZZ-XX-DR-L-6002 – Landscape Softwork Plan

Documents:

Application and CIL Forms, HTA Design, dated 04/04/2022
 Design & Access Statement, Levitt Bernstein, dated 29/03/22
 Planning Statement including Affordable Housing Statement and Health Impact Assessment, HTA Design, dated 04/04/2022
 Statement of Community Involvement; HTA Design, dated 04/04/2022
 Townscape Heritage and Visual Impact Assessment, KMHeritage, dated 03/2022
 Ecological Impact Assessment, P4106.2.2, agb Environmental P4106.2.2, dated 27/04/2022
 Biodiversity Net Gain Report, agb Environmental, P4106.3.3, dated 13/07/2022
 Daylight and Sunlight Report-June 2022, Waterslade, Rev 3, dated 28/06/2022
 Sustainable Design and Construction Statement, Etude, Rev 3, dated 03/22
 Green Performance Plan, Etude, Rev B, dated 09/21
 Circular Economy Statement, Etude, Rev D, dated 09/21
 Flood Risk Assessment and Drainage strategy, calfordseaden Rev. B, dated 23/08/2022
 Arboricultural Report: Arboricultural Impact Assessment and Arboricultural Method Statement, David Clarke, dated 03/2022
 Air Quality Assessment, Air Quality Consultants (4478A/1/F5)1, dated 09/2021

	<p>Noise and Vibration Impact Assessment, MaxFordham, Rev E 09, dated 06/2022 Healthy Street Transport Assessment (including CLP), Lime Transport, 20073 D2g, dated 23/10/2022 Fire Statement Form, Affinity Fire Engineering, Rev 12 – 07/09/2022 Vorley Road Structural Comments for Stage 3 Report, Calford Seaden, Rev B, dated 04/04/2022 Wind Microclimate Assessment; BRE, Issue 6, dated 16/03/2022 Draft Construction Management Plan, Calfordseaden, v3, dated 31/03/2022 BREEAM excellent tracker for Vorley Library, Ingleton Wood, dated 03/2022 Stage 2 BREEAM Options Appraisal Executive Summary, Ingleton Wood, dated 04/2022 Vorley Road Library BREEAM 2018, Ingleton Wood, dated 06/2021 Vorley Road Medical Centre BREEAM 2018, Ingleton Wood, dated 06/2021 Landscape UGF Calculation 3468A-LB-ZZ-XX-DR-L-6001, Levitt Bernstein 05/04/2022 Basement plant room layout, Ingleton Wood Rev.2, dated 04/2022 SK_16_External Services Layout, Ingleton Wood P02, dated 04/2022 UKPN Grid layout and connection details, dated 04/02/2021</p> <p>REASON: To comply with Section 70(1)(a) of the Town and Country Act 1990 as amended and the Reason for Grant and also for the avoidance of doubt and in the interest of proper planning.</p>
3	<p>Construction Management Plan and Construction Logistics Plan - (Details)</p>
	<p>CONDITION: A Construction Management Plan (CMP) and a Construction Logistics Plan (CLP) shall be submitted to, and approved in writing by, the Local Planning Authority as follows:</p> <p>a) Prior to demolition works: a Demolition Management and Logistics Plan</p> <p>b) Prior to construction works: a Construction Management and Logistics Plan</p> <p>The reports shall assess the impacts on surrounding streets, along with nearby residential amenity and other occupiers together with means of mitigating any identified impacts. The CMP must refer to the new LBI Code of Practice for Construction Sites.</p> <p>The development shall be carried out strictly in accordance with the approved documents during demolition and construction, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In the interests of residential amenity, highway safety, and the free flow of traffic on streets, and to mitigate the impacts of the development.</p>
4	<p>Air Quality and Dust Management Plan (Details)</p>
	<p>CONDITION: An Air Quality and Dust Management Plan (AQDMP) shall be submitted to, and approved in writing by, the Local Planning Authority as follows:</p> <p>a) Prior to demolition works and relating to the demolition</p> <p>b) Prior to construction works and relating to the construction</p> <p>The development shall be carried out strictly in accordance with the approved details and maintained as such thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To ensure adequate air quality</p>
5	<p>Tree Protection (Details)</p>

CONDITION: Prior to the commencement of works, a scheme for the protection of the retained trees, in accordance with BS 5837:2012, including a tree protection plan(s) (TPP) and an arboricultural method statement (AMS) shall be submitted to, and approved in writing by, the Local Planning Authority.

Specific issues to be dealt with in the TPP and AMS:

- a. Location and installation of services/ utilities/ drainage.
- b. Methods of demolition within the root protection area (RPA as defined in BS 5837: 2012) of the retained trees.
- c. Details of construction within the RPA or that may impact on the retained trees.
- d. A full specification for the installation of boundary treatment works.
- e. A full specification for the construction of any roads, parking areas and driveways, including details of the no-dig specification and extent of the areas of the roads, parking areas and driveways to be constructed using a no-dig specification. Details shall include relevant sections through them.
- f. Detailed levels and cross-sections to show that the raised levels of surfacing, where the installation of no-dig surfacing within Root Protection Areas is proposed, demonstrating that they can be accommodated where they meet with any adjacent building damp proof courses.
- g. A specification for protective fencing to safeguard trees during both demolition and construction phases and a plan indicating the alignment of the protective fencing.
- h. A specification for scaffolding and ground protection within tree protection zones.
- i. Tree protection during construction indicated on a TPP and construction and construction activities clearly identified as prohibited in this area.
- j. Details of site access, temporary parking, on site welfare facilities, loading, unloading and storage of equipment, materials, fuels and waste as well concrete mixing and use of fires
- k. Boundary treatments within the RPA
- l. Methodology and detailed assessment of root pruning
- m. Reporting of inspection and supervision
- n. Methods to improve the rooting environment for retained and proposed trees and landscaping
- o. Veteran and ancient tree protection and management

The development thereafter shall be implemented in strict accordance with the approved details, unless otherwise approved in writing by the Local Planning Authority.

REASON: Required prior to commencement of development to satisfy the Local Planning Authority that the trees to be retained will not be damaged during demolition or construction and to protect and enhance the appearance and character of the site and locality.

6 Land Contamination (Details)

CONDITION: Prior to the commencement of works, the following assessment in response to the NPPF and in accordance with Land Contamination Risk Management (LCRM) guidance (Environment Agency as updated 2021) and BS10175:2011+A2:2017 shall be submitted to and approved in writing by the Local Planning Authority:

- a) A land contamination investigation for all of the site excluding the existing footprint of 4 Vorley Road.

No above ground works shall commence until the following assessment in response to the NPPF and in accordance with Land Contamination Risk Management (LCRM) guidance (Environment Agency as updated 2021) and BS10175:2011+A2:2017 have been submitted to and approved in writing by the Local Planning Authority:

	<p>b) A land contamination investigation, following demolition, covering the area previously occupied by 4 Vorley Road.</p> <p>The investigations (a & b) shall be based upon and target the risks identified in the approved preliminary risk assessment and shall provide provisions for, where relevant, the sampling of soil, soil vapour, ground gas, surface and groundwater.</p> <p>Following the agreement to details relating to points a) and b); no above ground works shall commence until details of the following works have been submitted to, and approved in writing by, the Local Planning Authority:</p> <p>c) A remediation method statement of any necessary land contamination remediation works arising from the land contamination investigation.</p> <p>If, during development, contamination not previously identified is found to be present at the site, the Council is to be informed immediately and no further development (unless otherwise agreed in writing by the Council) shall be carried out until a report indicating the nature of the contamination and how it is to be dealt with is submitted to, and agreed in writing by, the Council.</p> <p>Following the agreement to details relating to point c); no above ground works shall commence until details of the following report has been submitted to, and approved in writing by, the Local Planning Authority:</p> <p>d) A verification report, that demonstrates the effectiveness of the remediation carried out.</p> <p>This report shall include details of the remediation works carried out; results of any verification sampling, testing or monitoring including the analysis of any imported soil; all waste management documentation showing the classification of waste, its treatment, movement and disposal; and the validation of gas membrane placement.</p> <p>Each part of this condition must be carried out in compliance with and by a competent person who conforms to Land Contamination Risk Management (LCRM) guidance (Environment Agency as updated 2021) or the current UK requirements for sampling and testing.</p> <p>REASON: To ensure the land is safe for development and minimise any potential impacts from land contamination.</p>
7	Piling Method Statement (Details)
	<p>CONDITION: No piling shall take place until a Piling Method Statement (detailing the depth and type of piling to be undertaken and the methodology by which such piling will be carried out, including measures to prevent and minimise the potential for damage to subsurface sewerage infrastructure, and the programme for the works) has been submitted to, and approved in writing by, the local planning authority in consultation with Thames Water.</p> <p>Any piling must be undertaken in accordance with the terms of the approved piling method statement.</p> <p>REASON: The proposed works will be close to underground sewerage and water utility infrastructure. Piling has the potential to significantly impact / cause failure to local underground sewerage utility infrastructure and local underground water utility infrastructure.</p>
8	Whole Life Carbon (Details)

CONDITION: An updated Whole Life Carbon Assessment shall be submitted to, and approved in writing by, the Local Planning Authority as follows:

a) Prior to demolition works and relating to the demolition phase; and

b) Prior to construction works

The updated assessment shall include/address:

- Further carbon reduction quantification through the detailed design stage material selection and specification;
- Completed GLA Whole Life-Cycle Carbon Assessment
- Details of how opportunities for retaining and refurbishing/re-purposing existing buildings, materials and other resources on site have been maximised to reduce the need for new materials;
- Details of life cycle of embodied carbon and finite resources relating to the enabling works stage and end of life approach;
- Details of the applicant’s Principals of Sustainable Procurement and details of specific measures being taken on the site for specification and sourcing of materials;
- Consideration of end of life de-construction;
- Cost premiums, supply chain limits and structural constraints for the proposal and Implications of Key Performance Indicators not being met; and
- Updated targets for Bill of Materials

The development shall be carried out strictly in accordance with the details so approved and no change therefrom unless otherwise specified in writing by the Local Planning Authority.

REASON: The revised and updated details and designs will ensure that the embodied carbon emissions associated with the proposed development, taking into account the materials quantities and loads, operational energy consumption of the built scheme, with total emissions estimated and compared to the GLA benchmarks are reduced to their lowest possible levels, having regard to GLA benchmarks in accordance with policy S4 of the London Plan.

9 Circular Economy (Details)

CONDITION: An updated Circular Economy Statement shall be submitted to, and approved in writing by, the Local Planning Authority as follows:

c) Prior to demolition works and relating to the demolition phase; and

d) Prior to construction works

The updated statement shall include outstanding information including the reporting of key metrics and commitments to London Plan policy targets.

The development shall be carried out strictly in accordance with the details so approved and no change therefrom unless otherwise specified in writing by the Local Planning Authority.

REASON: The revised and updated details and designs will ensure that the embodied carbon emissions associated with the proposed development, taking into account the materials quantities and loads, operational energy consumption of the built scheme, with total emissions estimated and compared to the GLA benchmarks are reduced to their lowest possible levels, having regard to GLA benchmarks in accordance with policy S4 of the London Plan.

10 Facing Materials (Details)

	<p>CONDITION: No above ground works shall commence until detailed drawings and samples of all facing materials are submitted to and approved in writing by the Local Planning Authority.</p> <p>The details and samples shall include:</p> <ol style="list-style-type: none"> a. Plan, elevation and section drawings, including jambs, head and sill, of all external windows and doors at a scale of 1:10; b. Samples and manufacturer's details at a scale of 1:10, of all main facing materials including brickwork in both primary base bricks, accent bricks and brick clad soffits and architectural pre-cast concrete balcony slabs, columns, copings, soffits, ground floor sills and entrance signage; c. Samples and manufacturer's details of all metalwork including PPC aluminium window system, sills, canopies, ventilation grilles and soffits and rainwater goods and bronze PPC steel balustrades, gates, shutters, soffits and gallery access railings; d. A full scale sample panel should be erected on-site to show a typical window detail and should be approved by the Council before the relevant parts of the work are commenced. This should demonstrate the exact facing brick blend and detail demonstrating the proposed colours, texture, face-bond and pointing and include a junction with a window opening; e. Details of the green roof system f. Green Procurement Plan; and g. Any other materials to be used. <p>The development shall be carried out strictly in accordance with the details and samples so approved, shall be maintained as such thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In the interest of securing sustainable development and to ensure that the resulting appearance and construction of the development is of a high standard</p>
11	Architectural Finish (Details)
	<p>CONDITION: No above ground works shall commence until detailed specifications including drawings (plan/section/elevation) at a minimum scale of 1:20 of the following details have been submitted to, and approved in writing by, the Local Planning Authority.</p> <ol style="list-style-type: none"> a) Projecting corduroy brick banding on Building B b) Plant enclosure on Building B c) Balconies including privacy screens, balustrading and soffits d) Brise soleil e) Parapets f) Rainwater pipes, gutters g) Windows including sills and lintels h) Doors including individual, communal and refuse store entrances. <p>The development shall be carried out strictly in accordance with the details and samples so approved, shall be maintained as such thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: High quality and well-considered detailing is fundamental to the success of the development and must be considered and approved by the Local Planning Authority prior to above ground works occurring</p>
12	Landscaping (Details)
	<p>LANDSCAPING: No above ground works shall commence until a detailed landscaping scheme has been submitted to, and approved in writing by, the Local Planning Authority.</p>

	<p>The landscaping scheme shall comprise the following details:</p> <ol style="list-style-type: none"> a. an updated Access Statement detailing routes through the landscape and the facilities it provides; b. a biodiversity statement detailing how the landscaping scheme maximises biodiversity; c. existing and proposed underground services and their relationship to both hard and soft landscaping; d. proposed trees: their location, species, size and section showing rooting area; to include trees planted below deck level with canopy to spread above podium level; e. soft planting; f. topographical survey: including earthworks, ground finishes, top soiling with both conserved and imported topsoil(s), levels, drainage and fall in drain types; g. enclosures and boundary treatment: including types, dimensions and treatments of walls, fences, screen walls, barriers, rails, retaining walls and hedges; h. hard landscaping: kerbs, edges, steps, ridge and flexible paving, including patterned coloured paving 'entrance carpets' to all buildings, unit paving, wet pour play safety surfaces and furniture including bike racks, seating and planters; i. wayfinding and signage; j. phasing of landscaping and planting; k. all play equipment and structures to include table tennis table, climbing features, all swings, fitness module, play sculptures, trampolines, slides and see saws; and l. any other landscaping feature(s) forming part of the scheme. <p>All landscaping in accordance with the approved scheme shall be completed / planted during the first planting season following practical completion of the relevant phase of the development hereby approved in accordance with the approved planting phase. The landscaping and tree planting shall have a two year maintenance / watering provision following planting and any existing tree shown to be retained or trees or shrubs to be planted as part of the approved landscaping scheme which are removed, die, become severely damaged or diseased within five years of completion of the development shall be replaced with the same species or an approved alternative to the satisfaction of the Local Planning Authority within the next planting season. The development shall be carried out strictly in accordance with the details so approved and shall be maintained as such thereafter.</p> <p>REASON: In the interest of biodiversity, sustainability, playspace and to ensure that a satisfactory standard of visual amenity is provided and maintained.</p>
13	Bird Boxes (Details)
	<p>CONDITION: No above ground works shall commence until details of bird boxes across all new buildings have been submitted and approved in writing by the Local Planning Authority prior to superstructure works commencing on site.</p> <p>The development shall be carried out strictly in accordance with the approved details and maintained as such thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To ensure the development provides the maximum possible provision towards creation of habitats and valuable areas for biodiversity.</p>
14	Sound Insulation (Details)
	<p>CONDITION: No above ground works shall commence until a scheme for sound insulation and noise control measures has been submitted to, and approved in writing by, the Local Planning Authority. The sound insulation and noise control measures shall achieve the following internal noise targets:</p>

	<ul style="list-style-type: none"> - Bedrooms (23.00-07.00 hrs) 30 dB LAeq,8 hour and 45 dB Lmax (fast) - Living Rooms (07.00-23.00 hrs) 35 dB LAeq, 16 hour - Dining rooms (07.00 –23.00 hrs) 40 dB LAeq, 16 hour <p>The sound insulation and noise control measures shall be implemented prior to the first occupation of the development hereby approved and carried out strictly in accordance with the details so approved and maintained as such permanently thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To ensure that an appropriate standard of accommodation is provided.</p>
15	Sound Insulation Strategy – Community Uses and Residential Uses (Details)
	<p>CONDITION: No above ground works shall commence until full particulars and details of a scheme for sound insulation between the proposed community uses (library and medical centre) and residential use of the building have been submitted to, and approved in writing by, the Local Planning Authority.</p> <p>The sound insulation and noise control measures shall be carried out strictly in accordance with the details so approved, shall be implemented prior to the first occupation of the development, and maintained as such permanently thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To ensure that an appropriate standard of accommodation is provided.</p>
16	Standby Generator (Details)
	<p>CONDITION: No above ground works shall commence until a written code for the management of noise from emergency plant and equipment is submitted to, and approved in writing by, the Local Planning Authority.</p> <p>The management code shall identify measures to reduce the impact of the noise and vibration on the community and shall include details of equipment testing. The code shall be fully implemented and operated at all times in strict accordance with the approved details and maintained permanently thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To ensure that the operation of the generator does not impact on residential amenity.</p>
17	Cycle Parking (Details)
	<p>CONDITION: Notwithstanding the plans hereby approved, details of 132 long-stay resident's cycle parking spaces, 7 residential visitor cycle parking spaces, 9 staff cycle parking spaces and 24 visitor cycle parking spaces (for the library and medical uses), inclusive of the provision of accessible parking spaces, shall be submitted to and approved in writing by the Local Planning Authority.</p> <p>The approved cycle storage shall be provided prior to the first occupation of the relevant part of the development hereby approved, shall be maintained as such thereafter and no change therefrom shall take place unless otherwise agreed in writing by the Local Planning Authority.</p> <p>REASON: To ensure adequate and suitable bicycle parking is available and easily accessible on site and to promote sustainable modes of transport.</p>
18	Secure by Design (Details)
	<p>CONDITION: No above ground works shall commence until details have been submitted to, and approved in writing, by the Local Planning Authority to demonstrate that the development can achieve 'Secured by Design' accreditation.</p>

	<p>The development shall be constructed and operated strictly in accordance with the approved details and maintained as such thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In the interest of creating safer, sustainable communities.</p>
19	<p>Accessible Housing (Compliance and Details)</p> <p>CONDITION: Notwithstanding the plans hereby approved, 65 x of the residential units shall be constructed to Category 2 of the National Standard for Housing Design as set out in the Approved Document M 2015 'Accessible and adaptable dwellings' M4 (2) and 7 x residential units shall be constructed to Category 3 of the National Standard for Housing Design as set out in the Approved Document M 2015 'Wheelchair user dwellings' M4 (3).</p> <p>No above ground works shall commence until Building Regulations Approved Plans and Decision Advice Notice, confirming that these requirements will be achieved, shall be submitted to and approved in writing by the Local Planning Authority. The Approved Plans to show furniture, key dimensions and manoeuvring allowances, as set out in the provisions of the Approved Document M (Volume 1).</p> <p>The development shall be carried out strictly in accordance with the approved details and permanently maintained as such thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To secure the provision of visitable, adaptable and wheelchair user accessible homes appropriate to meet diverse and changing needs.</p>
20	<p>Green/Blue Roof Details and Maintenance Plan (Details)</p> <p>CONDITION: Prior to the commencement of relevant works, details of the biodiversity green and blue roofs within the scheme as shown on plan C(0)1003 Rev. D within the revised Flood Risk and Drainage Strategy, dated 23/08/22 shall be submitted to, and approved in writing by, the Local Planning Authority. The biodiversity (green) roofs shall be:</p> <ul style="list-style-type: none"> A) biodiversity based with extensive substrate base (depth 80-150mm); and B) planted/seeded with an agreed mix of species within the first planting season following the practical completion of the building works (the seed mix shall be focused on wildflower planting, and shall contain no more than a maximum of 25% sedum). C) Details of the irrigation and maintenance regime for the proposed green roofs shall be submitted to and approved in writing by the local planning authority before any works thereby affected are begun. D) The biodiversity (green) roof shall not be used as an amenity or sitting out space of any kind whatsoever and shall only be used in the case of essential maintenance or repair, or escape in case of emergency. <p>The biodiversity roofs shall be carried out strictly in accordance with the details so approved and shall be maintained permanently as such thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To ensure the development provides the maximum possible provision towards creation of habitats and valuable areas for biodiversity.</p>
21	<p>Delivery and Servicing Plan (Details)</p>

	<p>CONDITION: Prior to the first occupation of the development hereby approved, a detailed Delivery and Servicing Plan (DSP) shall be submitted to, and approved in writing by, the Local Planning Authority.</p> <p>The development shall be constructed and operated strictly in accordance with the details so approved and shall be maintained as such thereafter unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In order to secure highway safety and free flow of traffic, local residential amenity and to mitigate the impacts of the development.</p>
22	Parking Design and Management Plan (Details)
	<p>CONDITION: Prior to the first occupation of the development hereby approved, a Parking Design and Management Plan shall be submitted to and approved by the Local Planning Authority.</p> <p>The plan must set out the parking arrangements across the site, including how drop-off points are properly controlled, how traffic will be suitably managed and how each wheelchair accessible unit can be provided with a designated accessible parking space in future should there be demand.</p> <p>The approved details shall be installed prior to the occupation of the development and shall be maintained as such thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In the interest of traffic safety and traffic management in accordance with Islington Core Strategy Policy CS10.</p>
23	Energy Statement (Details)
	<p>CONDITION: The energy efficiency measures/features and renewable energy technology, as detailed within the 'Energy Strategy' (ref no: Sustainable Design and Construction Statement prepared by Edute and dated March 2022) shall be installed and operational prior to the first occupation of the buildings hereby approved.</p> <p>Should there be any change to the energy features/ measures within the approved Energy Strategy, a revised Energy Strategy shall be submitted to and agreed in writing by the Local Planning Authority prior to the occupation of the development.</p> <p>REASON: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO2 emission reduction targets by energy efficient measures/features and renewable energy are met.</p>
24	BREEAM Excellent – Residential (Details)
	<p>CONDITION: Prior to the first occupation of the residential element of the development, evidence to show that the development will achieve four a four-star rating (as a minimum) under the BRE Home Quality Mark scheme shall be submitted to, and approved in writing, by the Local Planning Authority.</p> <p>Within 6 months of first occupation of the relevant building, certification confirming that the development achieves a four-star rating (as a minimum) under the BRE Home Quality Mark scheme. shall be submitted to, and approved in writing, by the Local Planning Authority.</p> <p>REASON: In the interest of energy efficiency and sustainability.</p>
25	BREEAM Excellent – Non-Residential (Details)

	<p>CONDITION: The library and medical centre element of the development shall achieve a BREEAM rating of no less than 'Excellent', unless otherwise agreed in writing by the Local Planning Authority.</p> <p>REASON: In the interest of addressing climate change and to secure sustainable development.</p>
26	Photovoltaic Panel Installation (Details)
	<p>CONDITION: Prior to first occupation of the development hereby approved, details of the proposed Solar Photovoltaic Panels shall be submitted to, and approved in writing by, the Local Planning Authority.</p> <p>This submission shall demonstrate how Solar PVs have been maximised on site, and details shall include but not be limited to: location; area of panels; how the PV output has been maximised and design (including section drawings showing the angle of panels in-situ, and elevation plans).</p> <p>The solar photovoltaic panels as approved shall be installed prior to the first occupation of each of the buildings and maintained as such permanently thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In the interests of securing sustainable development and to ensure that the resulting appearance and construction of the development is of a high standard of design.</p>
27	Lighting (Details)
	<p>CONDITION: Prior to the first occupation of the development hereby permitted details of any general / security lighting measures shall be submitted to, and approved in writing by, the Local Planning Authority.</p> <p>The details shall:</p> <ul style="list-style-type: none"> - include the location and full specification of: all lamps; light levels/spill lamps and support structures where appropriate and hours of operation; - demonstrate how the ecology of the site would not be adversely affected by the proposed lighting. <p>The general lighting and security measures shall be carried out strictly in accordance with the details so approved, shall be installed prior to occupation of the development and shall be permanently maintained as such thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To ensure that any resulting general or security lighting is appropriately located, designed to not adversely impact neighbouring residential amenity nor those with visual impairments, contributes towards safety and security, does not adversely impact biodiversity or ecology and is appropriate to the overall design of the building.</p>
28	External Signage (Details)
	<p>CONDITION: Prior to occupation of the development hereby approved, details of all external signage shall be submitted to, and approved in writing by, the Local Planning Authority.</p> <p>The agreed details shall be installed prior to the occupation of the development and shall be maintained as such permanently thereafter, unless otherwise approved in writing by the Local Planning Authority.</p>

	REASON: In the interests of visual amenity and to ensure that the entrance approach is both welcoming and inviting.
29	Inclusive Design (Details)
	<p>CONDITION: All inclusive design measures identified within the application submission shall be installed and operational prior to the first occupation of the development hereby approved.</p> <p>Further details on the following points shall be submitted and approved in writing by the Local Planning Authority prior to the completion of the development hereby approved:</p> <ul style="list-style-type: none"> - the accessibility of bike and bin stores; - Provision of accessible cycle parking spaces; - details on appropriate seating within the public realm, - audio loop systems, - medical garden accessibility - medical garden accessible play - confirmation that corridors and lobbies meet relevant standards within Islington's Inclusive Design SPD. <p>The inclusive design measures shall be retained as such in perpetuity unless otherwise approved in writing by the Local Planning Authority</p> <p>REASON: In order to facilitate and promote inclusive and sustainable communities.</p>
30	Obscure Glazing and Privacy Screens (Details)
	<p>CONDITION: Prior to the first occupation of Building B, details of obscured glazing and privacy screens to prevent potential overlooking from proposed Building B to the land to the north shall be submitted to, and approved in writing by, the Local Planning Authority.</p> <p>The obscure glazing and privacy screens shall be installed prior to the occupation of the relevant units and retained as such permanently thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In the interest of preventing undue overlooking between habitable rooms within the development itself, to protect the future amenity and privacy of residents.</p>
31	Management Plan – Library (Details)
	<p>Prior to the first occupation of the library, a management plan for any private hire of the space shall be submitted to, and approved in writing by, the Local Planning Authority.</p> <p>The development shall be carried out strictly in accordance with the approved plan, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In the interest of preventing undue overlooking between habitable rooms within the development itself, to protect the future amenity and privacy of residents.</p>
32	Management Plan – Medical Centre (Details)
	<p>Prior to the first occupation of the medical centre, a management plan for any private hire of the medical centre shall be submitted to, and approved in writing by, the Local Planning Authority.</p> <p>The development shall be carried out strictly in accordance with the approved plan, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In the interest of preventing undue overlooking between habitable rooms within the development itself, to protect the future amenity and privacy of residents.</p>

33	Whole Life Carbon Post-Construction Assessment Report (Details)
	<p>CONDITION: Within 3 months of practical completion of the development hereby approved, a whole life carbon post-construction assessment report shall be submitted to approved by the Local Planning Authority.</p> <p>REASON: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO2 emission reduction targets by energy efficient measures/features and renewable energy are met.</p>
34	Circular Economy Post-Construction Report (Details)
	<p>CONDITION: Within 3 months of practical completion of the development hereby approved, a post-construction circular economy report shall be submitted to the Local Planning Authority for approval in writing.</p> <p>REASON: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that circular economy principles have been incorporated into the design, construction and management of the approved development in accordance with London Plan Policy SI7.</p>
35	Fire Strategy (Compliance)
	<p>CONDITION: The details and measures set out in the Fire Strategy Report, by Affinity Fire Engineering – Rev. 12, dated 07.09.2022 shall be implemented in accordance with the approved document, unless otherwise agreed in writing by the Local Planning Authority.</p> <p>Should any subsequent change(s) be required to secure compliance with the submitted fire strategy, a revised fire strategy shall be submitted to and approved by the Local Planning Authority.</p> <p>The development shall be carried out in accordance with the Fire Strategy under this condition and shall be maintained as such thereafter.</p> <p>REASON: To ensure that the development incorporates the necessary fire safety measures in accordance with the Mayor's London Plan Policy D12.</p>
36	Basement Excavation (Compliance)
	<p>CONDITION: The development shall be constructed in accordance with the approved 'Structural Method Assessment – Revision P2 – Dated: May 2021 – prepared by: ads consultancy' unless otherwise agreed in writing.</p> <p>The certifying professional that endorsed the Structural Method Statement (or a suitably qualified Chartered Civil Engineer (MICE) or a Chartered Structural Engineer (MIStruct.E) with relevant experience shall be appointed to inspect, approve and monitor the critical elements of both permanent and temporary basement construction works throughout their duration to ensure compliance with Council's Basement Development SPD.</p> <p>REASON: To ensure that structural stability has been evaluated by a suitably qualified and experienced professional.</p>
37	Refuse and Recycling (Compliance)
	<p>CONDITION: Prior to the first occupation of the development hereby approved the dedicated refuse / recycling enclosure(s) shown on the approved plan - Layout Ground Floor – drawing number: 3468A-LB-ZZ-00-DP-A-1100-25 – Layout GF – dated: 11.10.2022 - shall be provided and shall be maintained as such permanently thereafter, unless otherwise approved in writing by the Local Planning Authority.</p>

	REASON: To secure the necessary physical waste enclosures to support the development and to ensure that responsible waste management practices are adhered to.
38	Lifts (Compliance)
	CONDITION: All lifts hereby approved shall be installed and operational prior to the first occupation of each of the buildings hereby approved. REASON: To ensure that inclusive and accessible routes are provided throughout the floorspace at all floors and also accessible routes through the site are provided to ensure no one is excluded from full use and enjoyment of the site.
39	Plumbing (No pipes to outside of building) (Compliance)
	CONDITION: No plumbing, down pipes, rainwater pipes or foul pipes shall be located to the external elevations of the development hereby approved, unless otherwise approved in writing by the Local Planning Authority as part of discharging this condition. REASON: The Local Planning Authority considers that such plumbing and pipes would potentially detract from the appearance of the building and undermine the current assessment of the application.
40	Biodiversity Mitigation (compliance)
	CONDITION: The proposal hereby approved shall implement the biodiversity enhancement measures as outlined in approved document Ecological Impact Assessment by AGB Environmental Limited – dated 27/04/2022. The biodiversity enhancement measures shall be installed prior to the occupation of the development and maintained as such permanently thereafter, unless otherwise approved in writing by the Local Planning Authority. REASON: In the interests of improving on-site biodiversity.
41	Plant Emissions (Compliance)
	CONDITION: On-site plant and machinery must comply with the London Non-Road Mobile Machinery (NRMM) Low Emission Zone standards (London Plan Policy SI 1 (D)). REASON: To ensure adequate air quality.
42	Fixed Plant (Compliance)
	CONDITION: The design and installation of new items of fixed plant shall be such that when operating the cumulative noise level LAeq Tr arising from the proposed plant, measured or predicted at 1m from the facade of the nearest noise sensitive premises, shall be a rating level of at least 5dB(A) below the background noise level LAF90 Tbg. The measurement and/or prediction of the noise should be carried out in accordance with the methodology contained within BS 4142: 2014+A1:2019. REASON: To ensure that an appropriate standard of accommodation is provided.
43	Hours of Operation – Medical Centre (Compliance)
	CONDITION: The medical centre shall be operational only between the following hours <ul style="list-style-type: none"> - 8am to 10pm (Monday to Saturday) - 11am to 5pm (Sundays) The restrictions shall be applied and permanently adhered to unless otherwise agreed with the Local Planning Authority.

	REASON: In the interests of protecting neighbouring residential amenity.
44	Hours of Operation – Library (Compliance)
	<p>CONDITION: The library shall be operational only between the following hours</p> <ul style="list-style-type: none"> - 8am to 10pm (Monday to Saturday) - 11am to 5pm (Sundays) <p>The restrictions shall be applied and permanently adhered to unless otherwise agreed with the Local Planning Authority.</p> <p>REASON: In the interests of protecting neighbouring residential amenity.</p>
45	Sustainable Urban Drainage (Compliance)
	<p>CONDITION: The details and measures set out in the Flood Risk Assessment and Drainage strategy by Calford Seaden Rev. B, dated 23/08/2022 shall be implemented in accordance with the approved document, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To ensure that sustainable management of water and minimise the potential for surface level flooding.</p>
46	Restriction of Use (Compliance)
	<p>CONDITION: Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended), or the provisions of any Order revoking and re-enacting that Order, no change of use of the 735 sqm (GIA) of approved E(e) floorspace and 826sqm (GIA) of approved F1(d) floorspace to any other use within Class E and Class F of the Schedule to the Town and Country Planning (Use Class) Order 1987 as amended 2005 (or the equivalent use within any amended/updated subsequent Order) or any other uses within any other use Class, unless otherwise agreed in writing by the Local Planning Authority.</p> <p>REASON: To safeguard the amenities of the adjoining residential units and the area generally and in the interest of securing the specific community uses.</p>
47	Restriction of PD Rights - Class E to residential (Compliance)
	<p>CONDITION: Notwithstanding the provisions of Schedule 2, Part 3, Class MA the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modifications), no change of use from Class E (commercial, business and service) to a use falling within Class C3 (dwellinghouses) shall take place without obtaining the express planning permission from the Local Planning Authority.</p> <p>REASON: For the avoidance of doubt and to ensure that the Local Planning Authority can restrict the use of the building to this specific use only, in order to protect the supply of office and commercial floorspace in this location.</p>

INFORMATIVES

List of Informatives:

1	<p>Working with the applicant</p> <p>In dealing with this application, Islington Council has implemented the requirements of the National Planning Policy Framework and of the Town and Country Planning (Development Management Procedure) (England) Order 2015 to work with the applicant in a positive and proactive manner. As with all applicants, we have made available detailed advice in the form of our statutory policies in the relevant constituent parts of the Local Plan and London Plan, Supplementary Planning documents, and all other Council guidance, as well as offering a full pre-application advice service, so as to ensure that applicant has been given every opportunity to submit an application which is likely to be considered favourably.</p>
2	<p>Director's Letter</p> <p>You are advised that this permission has been granted subject to a Director's Letter.</p>
3	<p>Superstructure</p> <p>DEFINITION OF 'SUPERSTRUCTURE' AND 'PRACTICAL COMPLETION'</p> <p>Some conditions attached to this permission have the time restrictions 'prior to superstructure works commencing on site' and/or 'following practical completion'. The council considers the definition of 'superstructure' as having its normal or dictionary meaning, which is: the part of a building above its foundations. The council considers the definition of 'practical completion' to be: when the work reaches a state of readiness for use or occupation even though there may be outstanding works/matters to be carried out.</p>
4	<p>Community Infrastructure Levy (CIL) (Granting Consent)</p> <p>INFORMATIVE: Under the terms of the Planning Act 2008 (as amended) and Community Infrastructure Levy Regulations 2010 (as amended), this development is liable to pay the Mayor of London's Community Infrastructure Levy (CIL). This will be calculated in accordance with the Mayor of London's CIL Charging Schedule 2012. One of the development parties must now assume liability to pay CIL by submitting an Assumption of Liability Notice to the Council at cil@islington.gov.uk. The Council will then issue a Liability Notice setting out the amount of CIL that is payable.</p> <p>Failure to submit a valid Assumption of Liability Notice and Commencement Notice prior to commencement of the development may result in surcharges being imposed. The above forms can be found on the planning portal at: www.planningportal.gov.uk/planning/applications/howtoapply/whattosubmit/cil</p> <p>Pre-Commencement Conditions: These conditions are identified with an 'asterix' * in front of the short description. These conditions are important from a CIL liability perspective as a scheme will not become CIL liable until all of these unidentified pre-commencement conditions have been discharged.</p>
5	<p>Car-Free Development</p> <p>INFORMATIVE: (Car-Free Development) All new developments are car free in accordance with Policy CS10 of the Islington Core Strategy 2011. This means that no parking provision will be allowed on site and occupiers will have no ability to obtain car parking permits, except for parking needed to meet the needs of disabled people.</p>
6	<p>Groundwater Risk Permit</p> <p>A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act</p>

	1991. We would expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 020 3577 9483 or by emailing trade.effluent@thameswater.co.uk . Application forms should be completed on line via www.thameswater.co.uk . Please refer to the Wholesale; Business customers; Groundwater discharges section.
7	Working Near Water Mains
	There are water mains crossing or close to your development. Thames Water do NOT permit the building over or construction within 3m of water mains. If you're planning significant works near our mains (within 3m) we'll need to check that your development doesn't reduce capacity, limit repair or maintenance activities during and after construction, or inhibit the services we provide in any other way. The applicant is advised to read our guide working near or diverting our pipes. https://www.thameswater.co.uk/developers/larger-scale-developments/planning-your-development/working-nearour-pipes
8	Water Pressure
	Thames Water will aim to provide customers with a minimum pressure of 10m head (approx. 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.
9	Management Code – Noise from emergency plant and equipment
	The Management code shall include measures to address the following matters: <ol style="list-style-type: none"> 1. The testing of equipment not to take place between the hours of 1800 and 0800 on any day, and not at any time on Sundays, Bank Holidays or after 1300 on a Saturday. 2. The duration of the testing to be commensurate with the test requirements and not to exceed one hour. 3. A list of potential residential receptors to be drawn up and those receptors to be given advance written notification of the time and date of the test. 4. The acoustic design and control of the fixed plant and equipment to meet a criterion of a rating level, measured or calculated at 1m from the façade of the nearest noise sensitive premises, of not more than 5dB(A) above the existing background noise level (L_{A90}). The rating level to be determined as per the guidance provided in BS4142:2014. 5. A report to be commissioned by the applicant, using an appropriately experienced & competent person, to assess the noise from the plant and machinery. The report is to be submitted to, and approved in writing by the Local Planning Authority, and any noise mitigation measures shall be installed before the commencement of the use hereby permitted and permanently retained thereafter.”

APPENDIX 2: RELEVANT POLICIES

This appendix lists all relevant development plan policies and guidance notes pertinent to the determination of this planning application.

1 National Guidance

The National Planning Policy Framework (July 2021) seeks to secure positive growth in a way that effectively balances economic, environmental and social progress for this and future generations. The NPPF is a material consideration and has been taken into account as part of the assessment of these proposals.

2. Development Plan

The Development Plan is comprised of the London Plan 2011, Islington Core Strategy 2011, Development Management Policies 2013, Finsbury Local Plan 2013 and Site Allocations 2013. The following policies of the Development Plan are considered relevant to this application:

A) The London Plan - Spatial Development Strategy for Greater London (March 2021)

- Policy GG1 Building strong and inclusive communities
- Policy GG2 Making the best use of land
- Policy GG3 Creating a healthy city
- Policy GG4 Delivering the homes Londoners need
- Policy GG5 Growing a good economy
- Policy GG6 Increasing efficiency and resilience
- Policy D1 London's form, character and capacity for growth
- Policy D2 Infrastructure requirements for sustainable densities
- Policy D3 Optimising site capacity through the design-led approach
- Policy D4 Delivering good design
- Policy D5 Inclusive design
- Policy D6 Housing quality and standards
- Policy D7 Accessible housing
- Policy D8 Public realm
- Policy D9 Tall Buildings
- Policy D11 Safety, security and resilience to emergency
- Policy D12 Fire safety
- Policy D13 Agent of Change
- Policy D14 Noise
- Policy H1 Increasing housing supply
- Policy H4 Delivering affordable housing
- Policy H5 Threshold approach to applications
- Policy H6 Affordable housing tenure
- Policy H10 Housing size mix
- Policy S4 Play and informal recreation
- Policy E1 Offices
- Policy E7 Industrial intensification, co-location and substitution
- Policy E11 Skills and opportunities for all
- Policy HC1 Heritage conservation and growth
- Policy HC3 Strategic and local views
- Policy G1 Green infrastructure
- Policy G4 Open space
- Policy G5 Urban greening
- Policy G6 Biodiversity and access to nature
- Policy G7 Trees and woodlands
- Policy SI1 Improving air quality

Policy SI2 Minimising greenhouse gas emissions
 Policy SI3 Energy infrastructure
 Policy SI4 Managing heat risk
 Policy SI5 Water infrastructure
 Policy SI7 Reducing waste and supporting the circular economy
 Policy SI12 Flood risk management
 Policy SI13 Sustainable drainage
 Policy T1 Strategic approach to transport
 Policy T2 Healthy streets
 Policy T3 Transport capacity, connectivity and safeguarding
 Policy T4 Assessing and mitigating transport impacts
 Policy T5 Cycling
 Policy T6 Car parking
 Policy T6.1 Residential parking
 Policy T7 Deliveries, servicing and construction
 Policy T8 Funding transport infrastructure through planning

B) Islington Core Strategy 2011

Spatial Strategy	Policy CS13 (Employment Spaces)
Policy CS2 (Finsbury Park)	Policy CS14 (Retail and Services)
Policy CS3 (Nag's Head and Upper Holloway Road)	Policy CS15 (Open Space and Green Infrastructure)
Policy CS4 (Highbury Corner and Holloway Road)	Policy CS16 (Play Space)
Policy CS5 (Angel and Upper Street)	Policy CS17 (Sports and Recreation Provision)
Policy CS6 (King's Cross)	Infrastructure and Implementation
Policy CS7 (Bunhill and Clerkenwell)	Policy CS18 (Delivery and Infrastructure)
Policy CS8 (Enhancing Islington's Character)	Policy CS19 (Health Impact Assessments)
Strategic Policies	Policy CS20 (Partnership Working)
Policy CS9 (Protecting and Enhancing Islington's Built and Historic Environment)	
Policy CS10 (Sustainable Design)	
Policy CS11 (Waste)	
Policy CS12 (Meeting the Housing Challenge)	

C) Development Management Policies June 2013

Design and Heritage

- DM2.1 Design
- DM2.2 Inclusive Design
- DM2.3 Heritage
- DM2.4 Protected Views

Housing

- DM3.1 Mix of housing sizes
- DM3.2 Existing housing
- DM3.4 Housing standards
- DM3.5 Private outdoor space
- DM3.7 Noise and vibration (residential uses)

Health and open space

- DM6.1 Healthy development
- DM6.2 New and improved public open space
- DM6.3 Protecting open space
- DM6.4 Sport and recreation
- DM6.5 Landscaping, trees and biodiversity
- DM6.6 Flood prevention

Energy and Environmental Standards

- DM7.1 Sustainable design and construction statements
- DM7.2 Energy efficiency and carbon reduction in minor schemes
- DM7.3 Decentralised energy networks
- DM7.4 Sustainable design standards
- DM7.5 Heating and cooling

Transport

- DM8.1 Movement hierarchy
- DM8.2 Managing transport impacts
- DM8.3 Public transport
- DM8.4 Walking and cycling
- DM8.5 Vehicle parking
- DM8.6 Delivery and servicing for new developments

Infrastructure

- DM9.1 Infrastructure
- DM9.2 Planning obligations
- DM9.3 Implementation

Emerging policy: draft Islington Local Plan 2019

1. The Regulation 19 draft of the Local Plan was approved at Full Council on 27 June 2019 for consultation and subsequent submission to the Secretary of State for Independent Examination. From 5 September 2019 to 18 October 2019, the Council consulted on the Regulation 19 draft of the new Local Plan. Submission took place on 12 February 2020 with the examination process now in progress. As part of the examination consultation on pre-hearing modifications took place between 19 March to and 9 May 2021. The Matters and Issues have now been published and hearings took place between 13 September to 5 October 2021.

In line with the NPPF, Local Planning Authorities may give weight to relevant policies in emerging plans according to:

- the stage of preparation of the emerging plan (the more advanced its preparation, the greater the weight that may be given);
- the extent to which there are unresolved objections to relevant policies (the less significant the unresolved objections, the greater the weight that may be given); and
- the degree of consistency of the relevant policies in the emerging plan to this Framework (the closer the policies in the emerging plan to the policies in the Framework, the greater the weight that may be given).

Emerging policies that are relevant to this application are set out below in Appendix 2. The emerging policies are considered to be consistent with the current policies.

Emerging Islington Local Plan (2019)

Policy PLAN1: Site appraisal, design principles and process
Policy SP7: Archway

Policy H1: Thriving communities
 Policy H2: New and existing conventional housing
 Policy H3: Genuinely affordable housing
 Policy H4: Delivering high quality housing
 Policy H5: Private outdoor space
 Policy SC1: Social and Community Infrastructure
 Policy SC2: Play Space
 Policy SC3: Health Impact Assessment
 Policy SC4: Promoting Social Value
 Policy B5: Jobs and training opportunities
 Policy R3: Islington's Town Centres
 Policy G1: Green Infrastructure
 Policy G4: Biodiversity, landscape design and trees
 Policy G5: Green roofs and vertical greening
 Policy S1: Delivering sustainable design
 Policy S2: Sustainable design and construction
 Policy S3: Sustainable design standards
 Policy S4: Minimising greenhouse gas emissions
 Policy S5: Energy infrastructure
 Policy S6: Managing heat risk
 Policy S7: Improving air quality
 Policy S8: Flood risk management
 Policy S9: Integrated water management and sustainable drainage
 Policy S10: Circular economy and adaptive design
 Policy T1: Enhancing the public realm and sustainable transport
 Policy T2: Sustainable travel choices
 Policy T3: Car-free development
 Policy T4: Public Realm
 Policy T5: Delivering, servicing and construction
 Policy DH1: Fostering innovation and conserving and enhancing the historic environment
 Policy DH2: Heritage assets
 Policy DH3: Building heights
 Policy DH4: Basement development
 Policy DH5: Agent of change, noise and vibration

Islington Draft Local Plan Site Allocation (September 2019)

ARCH1: Vorley Road/Archway Bus Station

5. Designations

The site has the following designations under the London Plan 2021, Islington Core Strategy 2011, Development Management Policies 2013, Finsbury Local Plan 2013 and Site Allocations 2013:

- Archway Town Centre
- Archway Key Area
- Site Allocations 2013 – ARCH1
- Draft Site Allocations 2021 – ARCH1
- Article 4 Direction – B1 to C3 (4 Vorley Road)
- Article 4 Direction – A1 to A2
- Article 4 Direction - A1, B1, D1 to temporary 'flexible uses'
- London Underground Zone of Interest (Tunnels)

6. Supplementary Planning Guidance (SPG) / Document (SPD)

The following SPGs and/or SPDs are relevant:

- | | |
|---------------------------------------|---|
| Islington Local Development Plan | London Plan |
| - Student Bursaries | - Accessible London: Achieving and Inclusive Environment |
| - Environmental Design | - Housing |
| - Small Sites Contribution | - Sustainable Design & Construction |
| - Accessible Housing in Islington | - Providing for Children and Young Peoples Play and Informal Recreation |
| - Conservation Area Design Guidelines | - Planning for Equality and Diversity in London |
| - Inclusive Landscape Design | |
| - Planning Obligations and S106 | |
| - Urban Design Guide | |

Register of Locally Listed Buildings and Locally Significant Shopfronts (April 2010)

APPENDIX 3 – PLANNING COMMITTEE REPORT



PLANNING COMMITTEE REPORT

Development Management Services
 Planning and Development Division
 Community Wealth Buildings

PLANNING COMMITTEE		AGENDA ITEM NO:
Date:	8 th November 2022	NON-EXEMPT

Application number	P2022/1221/FUL
Application type	Full Planning Application
Ward	Junction Ward
Listed building	Adjacent to Locally Listed 32 Junction Road
Conservation area	Adjacent to St John's Grove Conservation Area
Development Plan Context	Archway Town Centre Archway Key Area Site Allocations 2013 – ARCH1 Draft Site Allocations 2021 – ARCH1 Article 4 Direction – B1 to C3 (4 Vorley Road) Article 4 Direction – A1 to A2 Article 4 Direction - A1, B1, D1 to temporary 'flexible uses' London Underground Zone of Interest (Tunnels)
Licensing Implications	N/A
Site Address	Vorley Road Bus Stand & 4 Vorley Road, London N19 5JH
Proposal	Demolition of 620.9sqm community use part 1 / part 2 storey building; the construction of 72 new dwellings with associated private amenity space for affordable and private homes, provided in three blocks of 13 storeys, 8 storeys, and 2 to 6 storeys, improvements to the public realm, the provision of a Medical Centre (775sqm) and a library (766sqm), associated bicycle parking, mobility scooter stores, refuse stores and improvements to the public realm.

Case Officer	Brett McAllister
Applicant	London Borough of Islington - New Build
Agent	Sarah Eley - HTA Design

1. RECOMMENDATION

The Committee is asked to resolve to **GRANT** planning permission:

1. subject to the conditions set out in Appendix 1;
2. conditional upon the prior completion of a Directors' Agreement securing the heads of terms as set out in Appendix 1.

2. SITE PLAN (site outlined in red)

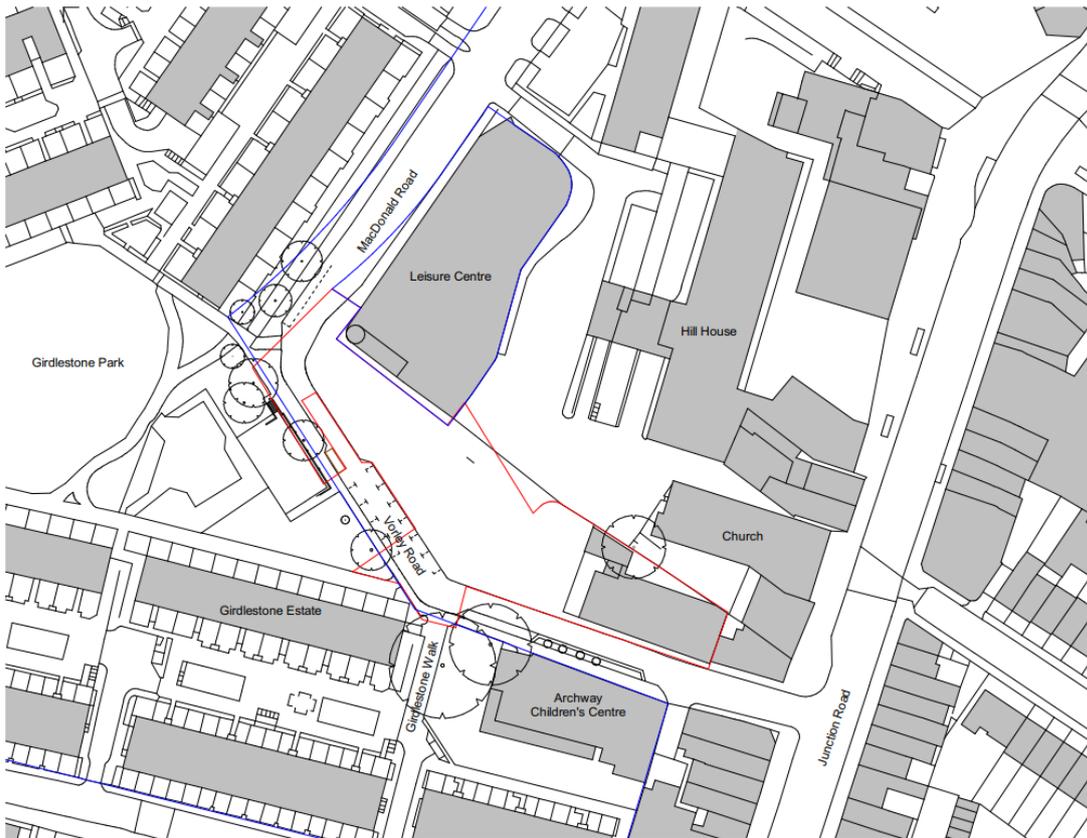


Figure 3 - Site Plan

3. PHOTOS OF SITE/STREET



Figure 4 - Aerial view from south



Figure 5 – Site looking east from midway point along Vorley Road



Figure 6 - Site looking west from midway point along Vorley Road



Figure 7 - Site looking east from western end of Vorley Road

4. SUMMARY

- 4.1 The application seeks planning permission for the demolition of 620.9sqm of vacant former adult integrated services community use part 1 / part 2 storey building and the construction of 72 new dwellings with associated private amenity space for affordable and private homes, provided in three blocks of 13 storeys, 8 storeys, and 2 to 6 storeys, improvements to the public realm, the provision of a Medical Centre (775sqm) and a library (766sqm), associated bicycle parking, mobility scooter stores, refuse stores and improvements to the public realm.
- 4.2 The 72 residential units would consist of 25 x one bedroom, 36 x two bedroom and 11 x three bedroom units. The proposal would deliver 37 social rented units (51% by unit and 58% by habitable room).
- 4.3 The site is currently occupied by a part one part two storey building which represents an under-development of the site. The proposal would deliver a library and medical centre on the lower floors and three residential buildings of 8 storeys, 13 storeys and part 6 part 2 storeys. This represents an optimisation of the site's capacity in this town centre location that is in accordance with the draft Local Plan Site Allocation for a residential-led development with social and community infrastructure uses.
- 4.4 The Site Allocation also sets out the site's capacity to include a landmark tall building forming part of an Archway cluster alongside Hamlyn House, Hill House and Archway Tower. The proposed development is considered to be acceptable in regard to its height, bulk, scale, massing, design and external finish, responding positively to its context. Some harm is identified to heritage assets but this is considered less than substantial and is outweighed by the benefits of the proposal.
- 4.5 Although there are reductions to daylight and sunlight receipt to neighbouring properties, these are considered acceptable on balance. Due consideration has been given to the proposal's impact on outlook, privacy, potential for noise and disturbance. Overall, neighbouring amenity is not considered to be unduly harmed by the development.

- 4.6 The proposal would be passivhaus certified and highly energy efficient resulting in a limited carbon offset figure of £57,542 for a scheme of this size.
- 4.7 The proposal would be a car-free development. No significant transport and parking impacts are posed by the scheme having regard to access, servicing, parking, trip generation, potential public transport impact, promotion of sustainable transport behaviour and potential impacts during the demolition and construction period.
- 4.8 All other matters relevant to planning are considered to be acceptable.
- 4.9 As such, the proposal is considered to be acceptable and is recommended for approval subject to conditions and a legal agreement.

5. SITE AND SURROUNDING

- 5.1 The application site is located on the north side of Vorley Road, just west of the junction with Junction Road and opposite Girdlestone Park.
- 5.2 The site spans nearly the entire southern side of the 'Archway Island' with only 2 Vorley Road/34 Junction Road to the east. To the west, the majority of the site comprises a vacant bus depot which is surrounded by hoardings and currently being used for storage of building materials. To the east is 4 Vorley Road a part-one part-two storey building that is the vacant Islington Adult Integrated Community Services Building. The site does not lie within a conservation area and is not listed.
- 5.3 The surrounding area provides a variety of building forms, styles, scales and ages. Immediately to the east of the site is 2 Vorley Road/34 Junction Road, a recently redeveloped part-three part-five storey block, on the corner of Junction Road, comprising a commercial unit at ground floor level (currently occupied by a Crisis charity shop) and residential on the upper floors. 32 Junction Road, a five-storey Edwardian mansion block to the north of this is locally listed.
- 5.4 Much of Junction Road is characterised by consistent three-storey Victorian terraced properties with commercial uses on the ground floor and residential/office above and forms part of St John's Grove Conservation Area.
- 5.5 Immediately to the north, moving from west to east there is the Archway leisure centre, a vacant backland site to the rear of the leisure centre currently being used for car parking and the storage of materials, another vacant site and then further to the east of this is a small car park and the rear of 30/28 Junction Road (a church).
- 5.6 Beyond these to the north is the rest of the Archway Island. This is comprised of a 1960s complex of three taller blocks – Hill House (15 storeys), Vantage Point (18 storeys) and Premier Inn (10 storeys) arranged in an orthogonal grid of volumes sliding past one another. Hill House and Vantage Point are connected by plinth of 3-4 stories that runs north-south. Between this and Junction Road is a plaza with shops. The complex is characterised by different levels and ramps, with the Archway Library under the Premier Inn (to be relocated to the application site under this proposal).
- 5.7 To the west of the site, on the other side of Vorley Road, is Girdlestone Park, a designated public open space with a number of trees that serves the Girdlestone estate that surrounds it to the north, west and south; a post-war housing estate of four-storey linear blocks. To the south of the site, on the other side of Vorley Road is the part two part single storey Archway children's centre.
- 5.8 In terms of the local policy context, the site is located within the Archway Key Area and within Archway Town Centre. 4 Vorley Road is within the primary shopping area in the draft Local Plan. The site is within the ARCH1 site allocation in the adopted Local Plan which includes the wider Archway island. The site is within ARCH1 site allocation in the draft Local Plan which solely comprises the application site and seeks residential-led development with social and community

infrastructure uses on the site and establishes the principle of a tall building of up to fifteen-storeys. The site has no listed buildings and is not within a Conservation Area although the site is adjacent to a St John's Grove Conservation Area to the east. Vorley Road is a designated clean air route.

6. PROPOSAL (IN DETAIL)

Demolition

- 6.1 The scheme proposed to demolish the existing part one- part two-storey 4 Vorley Road.

Residential

- 6.2 The scheme proposes the construction of 72 dwellings (Use Class C3), all of which would be flats, provided in the upper floors of three blocks which range in height from two storeys to thirteen storeys; the ground and much of the first floor of these blocks would form a plinth within which the non-residential development is located.
- 6.3 The massing of the blocks would be arranged with Building B, the tallest building of 13 storeys in the centre. This would be flanked by two lower buildings: 8-storey Building A to the west closest to MacDonald Road and 6-storey Building C to the east closest to Junction Road.
- 6.4 A total of 37 units would be social rent, and 35 units would be private sale.
- 6.5 There would be two entrances for the residents, a centrally located entrance to access Buildings A and B; and a separate entrance at the eastern end of the site to access Building C.
- 6.6 Below is an extract from the submitted Proposed Ground Floor Plan (drawing ref: 3468A-LB-ZZ-00-DP-A-110) which shows the proposed site layout: the footprint of the blocks would occupy the majority of the site.

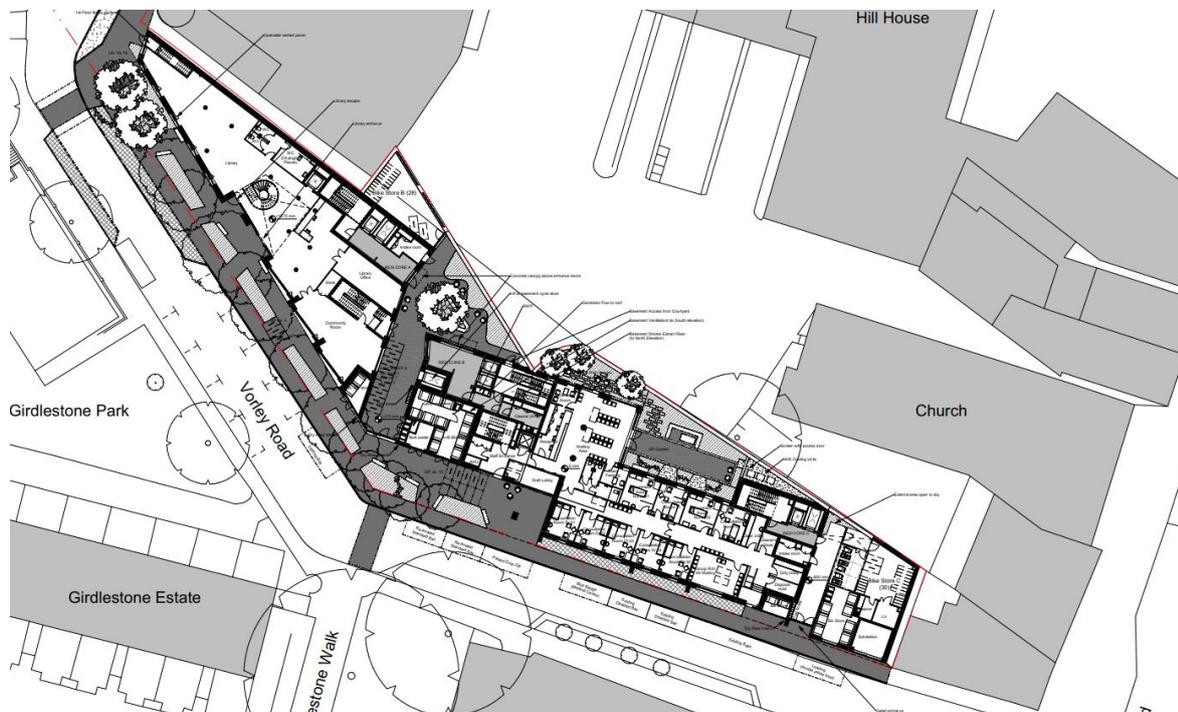


Figure 8 - Proposed Ground Floor Plan

Community Uses

- 6.7 Two community uses are proposed which would sit within the plinth: a Medical Centre (Use Class E) and a Library (Use Class F1), which would replace the existing Archway Library to the north of the site.

Medical Centre

- 6.8 The Medical Centre (Use Class E) would be located in the ground and first floor of Building B as well as the ground floor of Building C. The design and layout have been developed through discussions with a local practice which would relocate to the new premises, and NHS guidance notes.
- 6.9 Public access to the medical centre would be at ground floor from Vorley Road, at the junction of Building B and Building C. An ambulance drop-off bay and duty doctor's parking bay would be located along Vorley Road outside the public entrance. There would be a staff entrance point, located further east along Vorley Road.
- 6.10 To the rear of the new medical centre, a courtyard is proposed which will be greened and planted, providing amenity space for the staff and service users of the medical centre.

Library

- 6.11 Archway Library is currently located at sub-ground level of Hamlyn House to the north of the Archway Island. As the current space is leased from a private landlord, and it does not occupy a prominent location within Archway, discussions have been undertaken with Library Services to relocate the library to the Vorley Road site.
- 6.12 The proposed library (Use Class F1) would be located at ground and first floor of Building A. It would have a prominent street presence with a 50m façade facing Girdlestone Park, and an entrance at ground level on Vorley Road.

Landscaping, Play Space and Public Realm

- 6.13 Public realm upgrades are proposed along the front elevation of the blocks on Vorley Road. In addition to this it is proposed to fund a significant upgrade of the children's play area in Girdlestone Park which lies immediately opposite to the south west of the proposal site with a financial contribution of £260,000.
- 6.14 To the north of the proposed blocks are two landscaped courtyards; the eastern one is for the users of the residents of Buildings A and B; the western one is for the use by Medical Centre users.

Revisions

- 6.16 Over the course of the application a number of revisions to both plans and documents have been undertaken. These include:
- Updated Fire Strategy Report and associated plans to improve fire safety including a separate lift and stair to the basement and separation of residential staircores from areas of higher fire risk
 - Extension of corduroy brick banding to the north elevation of Block B
 - Additional CGIs and verified views
 - Updated Flood Risk and Sustainable Drainage to include blue roof
 - Biodiversity Net Gain Report
 - Updated Daylight and Sunlight Report to address latest BRE Guidance
 - Updated Circular Economy Statement in response to GLA comments

- Updated Noise and Vibration Impact Assessment to address noise from leisure centre

7. RELEVANT PLANNING HISTORY:

7.1 The applications relevant to the application site are as follows:

871400 – 4 Vorley Road - Construction of two-storey extension to Neighbourhood Office – Approved with conditions 05/10/1987.

P030644 – 2 Vorley Road – Change of use from offices on first to third floors to three residential units. – Approve with conditions 20/05/2003.

P102627 – 34 Junction Road – The conversion of the 1st, 2nd and 3rd floors from Taxicab Office to residential (Use Class C3) to provide 6 self-contained residential units with retail at ground floor. External alterations to building and the three-storey extension, an additional single storey extension to shopfront. – Approve with conditions 23/03/2011.

P2016/1360/EIA - Archway Gyratory (the site for the highways works covered a wide areas including Vorley Road - Request for a screening opinion under the town and country planning (Environmental Impact Assessment) regulations 2011 (as amended) as to whether the proposals constitute environmental impact assessment (EIA) development - Environmental statement not required 11/10/2016.

P2021/1615/EIA - Vorley Road/Archway Bus Station – Screening opinion for the proposed development in this application – Environmental Statement not required 12/10/2022.

Hill House

P2014/0332/PRA - Prior Approval application in relation to the following considerations arising from the change of use of the building of floors 1 to 4 and 6 to 12 to residential use (C3) use class creating 141 residential units: a) transport and highways impacts of the development b) contamination risks on the site; and c) flooding risks on the site - Prior approval required - approved 20/03/2014.

P2017/2795/PRA - Prior approval application in relation to the change of use of the ground to 4th and 6th to 11th floors (inclusive) from B1(a) office to C3 residential creating 150 residential units. Prior approval required - approved with conditions 11/09/2017

34 Junction Road

P102627 - The conversion of the 1st, 2nd and 3rd floors from Taxicab Office to residential (C3) to provide 6 self-contained residential units with retail at ground floor. External alterations to building and the three storey extension, an additional single storey extension to shopfront - Approve with conditions 23/03/2011

8. PRE-APPLICATION ADVICE

8.1 Pre-application advice was sought under the reference: **Q2016/0711/MJR**. Key points of response 23/01/20: -

Islington Pre-Applications

8.2 The pre-application was subject to ongoing discussions. Key points of response 17/06/2020: -

- The position, orientation and massing of the tower now corresponds appropriately with the Archway complex. Notwithstanding this townscape/views, heritage, overshadowing and micro-

climate assessments (particularly wind testing) need to be undertaken as a priority to inform the design of the tower.

- The proposed green lobby/link from Vorley Road between Building A and B to the Archway complex is supported.
- The proposed library and medical centre uses are strongly supported for the site creating a civic hub along Vorley Road. It is considered that these social and community town centre uses could justifiably replace the commercial element sought for the site.
- The residential provision, now exclusively on the upper floors, is strongly supported.
- Further work needs to be done to ensure that the amenity and safety of future and neighbouring residents would be of a high quality.
- An exploration of how to provide on-site child play space on this very tight site should be undertaken.
- Details of how delivery and servicing will be undertaken at the site needs to be provided as a priority.
- The scheme should be presented at DRP and at Members Pre-Application Forum as soon as possible.

GLA Pre-Application

8.3 Key Points from GLA Pre-Application: -

- The optimisation of this brownfield site by way of a residential-led mixed-use development including a 50% affordable housing offer is strongly supported in principle, subject to confirmation from the Council that the site is no longer required for its initial social infrastructure use.
- the draft site allocation specifically states that the site has the potential to accommodate a local landmark building of up to 15 storeys
- Approach to playspace may be acceptable given the proximity of the park to the site. The applicant should confirm details of the contribution towards the park improvements and the contribution must be secured via s106 agreement.
- The scheme does not bear any of the hallmarks of overdevelopment
- The alignment of the tallest element of the scheme with the two existing towers on the wider site, Archway Tower and Hill House, stepping down from Highgate Hill towards Girdlestone Park would create a clear visual hierarchy and foster a sense of cohesion within the wider site and positive relationship with the park across Vorley Road.
- The eight-storey element mediates the step change between the scale of the development and the lower rise residential west of the site.

Design Review Panels

8.4 The proposal has two full panel Design Reviews and a chair's review.

8.5 Key points from second Design Review Panel 09/02/2021:

- The proposed massing and layout are generally supported however relationship of the apartments in Block A to Girdlestone Park could be improved
- Defensiveness and relative lack of generosity resulting from the ratio of solid to void and rather austere materiality. These design choices should be reviewed from the perspective of whether they are sufficiently open and welcoming to express public buildings.
- Further design development is required to bring greater nuance and sophistication to the architectural expression and form of the buildings, both individually and as a collective composition of elements within the townscape.
- High embodied carbon of brick, the main facing material, was noted, and efforts encouraged to reduce the impact of this choice. All other finishes and materials must further contribute to minimising embodied carbon wherever possible, such as screeds and mortars.

- Replacing basement bike storage with provision at upper levels and in apartment lobby spaces facilitated by adequately sized lifts suggested.
- Suggested that the green or blue roof could extend to block B to help achieve the urban greening factor and SUDS targets and is compatible with the proposed PV and plant. Integrating PV on the roofs of blocks A and B was also encouraged

8.6 Key points from third Design Review Panel 13/10/2022 restricted to materiality of elevations:

- the two tones of brickwork (dark for lower building, light for taller) works well. The rationale for the lighter tones reducing impact to the long views and differentiates the taller built form from the historic fabric as an interesting commentary and works well.
- A 25mm projection of brick overhangs is considered enough to create a textured effect.
- There were strong concerns over the plant room. The appearance at the moment is too solid and appears part of the sculptural piece of the main body of the building but is not in terms of form and plan. It was not considered to work well as a crown or top to the building as proposed. A more ephemeral effect was desired, such as a transparent screen of fretted metal, to transition better into the sky.
- A suggestion was put forward for the approach of using rustication as a datum becoming less shadowed and phasing out as you ascend the building, such as larger gaps between the banding with no banding at the top.

8.7 The application has developed in response to the DRP's recommendations over the history of the scheme with further refinement to massing and elevations, provisions of CGIs, verified views provided, clarification on materiality, agreement to redesign the plant enclosure cladding on Block B.

9. CONSULTATION

Public Consultation

9.1 Letters were sent to occupants of 2,922 adjoining and nearby properties on 10th May 2022. A site notice and press advert were displayed on 12th May 2022. The public consultation of the application therefore expired on 5th June 2022, however, it is the Council's practice to continue to consider representations made up until the date of a decision.

9.2 At the time of the writing of this report 26 representations had been received from the public with regard to the application; **24** objecting and **2** in support. The issues raised are summarised below:

Objections

Principle of Development

- Strain on local services
- A masterplan for the District Centre and wider area is needed before any development can go ahead
- Proposals don't do much to address the increasing level of poverty and deprivation in London or provide homes and facilities for existing local families
- Economically risky to build tall building aimed at investors given the economic climate
- Will there be sufficient local services/social and community infrastructure to cater for the increase in population of this and other surrounding schemes - What will the relationship be between the proposed health centre and the Whittington Hospital less than 200 yards away? Will additional school places be required or are the flats intended solely for people without children?
- It should be a community centre instead
- No policy justification for tall building
(See paragraph 11.2 to 11.34)

Design

- Too tall, height would set a precedent for other tall buildings in Archway, building should be scaled back would tower over housing estates – lead to decline in the social fabric and lives of its inhabitants – should be more human scale - An eye sore to have 13 storey building in the middle of low rise estate 5.
- Islington Tall Building Study has no consideration of conservation areas and listed buildings in other boroughs such as setting of Highgate Conservation Area (Haringey and Camden), Dartmouth Park Conservation Area (Camden)
- No views of the development from Hampstead Heath, Highgate Hill, Highgate High Street, Waterlow Park and other parts of Camden and Haringey which might be adversely affected.
- Has the City of London, Dartmouth Park CAAC been made aware?
- Would block the view of Highgate coming up Holloway Road. This appears to be contrary to pages 75-76 of The Tall Building Study
- Would negatively influence views of and from Highgate.
- Adversely impact the Highgate Conservation Area
- Impact on nearby parts of Camden and Haringey, Hampstead Heath etc has been largely ignored.
- Harm listed buildings and conservation areas
(See paragraph 11.34 to 11.97)

Amenity

- Overshadowing
- Worsen wind conditions
- Loss of daylight/sunlight, impacts on Hill House
- Impact on wind – there should be no downdraught from new blocks on Navigator Square
- Increase overlooking/loss of privacy
- Reduce outlook – loom over nursery
- Insufficient amenity/play/communal space
- Please provide details of the amount of amenity space required for the residents of Girdlestone Estate under the London Plan compared with the proposed shared use of that space with the residents of the proposed buildings.
- Noise
- Wind report's focus is too narrow – doesn't consider nearby surrounding properties
- Not good for mental health
- Noise and disturbance from use
(See paragraphs 11.98 to 11.134 and paragraphs 11.145 to 11.217)

Housing/Viability

- Social and market housing could be more integrated
- Changed work and living patterns from the pandemic should be considered in the design
- Archway already too dense, shouldn't provide private units
- Poky flats are not suitable for young families
- Already lots of new flat developments in the area
- Given the changes to work practices is there working from home space built into the units?
- area does not need such a demand for 1-2 bedroom properties and the amount proposed does not reflect the current housing demand.
- High maintenance costs
- Already flats being built just off Chester Road
- Too many flats built in London
- Too many small flats in the area (1 and 2 bedroom) – low demand for these. Lots of unsold flats in recent development in the area including Hill House
- The council itself has identified that Islington need more low-density, family homes with gardens
- Will struggle to sell private units in the current economic climate
- Expensive maintenance costs for tall buildings
- How the building is cost effective against alternatives hasn't been provided
- Won't be able to sell the private units Council risks losing money and cutting services

- Loss of value for surrounding properties
- Should be a higher proportion of affordable units
(See paragraphs 11.135-11.144 and paragraphs 11.374-11.385)

Highways/Traffic

- Impact on parking, parking will be made harder on Vorley Road
- Overcrowding of Archway underground from new residents
- TfL and Islington Council should revise the arrangement of bus stops around Archway so that the problems caused by the separation of the 210, 143, 271 (and/or any replacement buses) are rectified.
- the redesign of the pavement and public realm around the Archway Tower and outside of the existing library is of paramount importance, which may include a part redesign of Archway underground station
- There is no new internal route through the Archway Centre complex to connect with Junction Road
- Delivery vans servicing flats in the proposed Vorley Road development will hinder buses and other traffic
- Is there adequate loading/drop off facilities for deliveries and taxis?
- The extension of the gyratory via Vorley Road should be reassessed. Ideally the 210 should be rerouted via the same bus stop on the northern side of Navigator Square as the 271 and 143.
- Congestion- including on public transport
- Parking problems
(See paragraphs 11.218 11.253)

Safety/Security

- Fire risk from high-density design
- Council should mitigate ASB in the surrounding area
- Increase in anti-social behaviour
- Relocation of current library will reduce activity and passive surveillance from this already threatening area in Archway. Vacant unit in that area will be unlettable.
(See paragraph 11.349-11.366)

Trees/Biodiversity/Drainage/Sustainability

- Will mature tree at back of leisure centre be removed/lose all its light?
- Loss of trees
- There should be far more planting and open areas of earth and other permeable surfaces which will contribute to lessening runoff, flood risk and climate change, both here and elsewhere around Archway
- Towers are not sustainable
- Ecology report overlooks the local presence of swifts - greater suitability of swift bricks to this site compared to general and sparrow nestboxes
- request that a significant number of integrated swift nestbox bricks - Biodiverse roofs, native planting and integrated bat boxes would further enhance biodiversity
(See paragraph 11.254 to 11.301 and paragraphs 11.311-11.337)

Construction

- Any building work should only be carried out by those in the Considerate Constructor's scheme and during working hours. Any breach should be enforced.
- Construction noise/dust/air pollution
(See paragraph 11.250 to 11.253)

Support

- Scheme delivers new housing
- Scheme delivers new affordable housing

(See paragraphs 11.135-11.144 and paragraphs 11.374-11.385)

External Consultees

9.3 GLA Stage 1:

Land Use: Principle of optimisation of the site within town centre for residential-led redevelopment including medical centre and library is strongly supported.

Housing: Level of affordable housing meets policy for publicly owned land. Tenure and unit mix supported.

Child Play Space: Proposed approach to child play space provision could be accepted given the immediate proximity to the park and the constraints of the site but appropriate financial contribution towards the park improvements must be robustly secured.

Tall Building Visual Impacts: Principle of tall building supported. The tallest element appears slightly austere in its character. In terms of Cores A and C greater contrast to Core B could be considered. Balconies and private terraces could also benefit from additional green infrastructure to soften the visual impact. Brise Soleil 1 feels institutional whereas the other two types result in a positive contribution to the façade composition.

Tall Building Functional Impacts: Ensure that any aviation or telecommunication impacts arising from the development are suitably addressed and that no significant detrimental effect on solar energy generation on adjoining buildings would result. Needs to be ensured that there will not be any building management issues from Core B being shared between private and social rented homes.

Tall Building Environmental Impacts: The proposal demonstrates consideration of potential impacts on the future site to the north which may come forward.

Layout & Public Realm: It is accepted that accommodating Library and Medical Centre compromised the ability to provide a new pedestrian route through the site. Additional activation around the entrances to Core A and B could be achieved, as bin and bulky waste stores sit either side.

Residential Quality: Reference should be made to Housing Design Standards LPG. Minimal building separation between dwellings of Core A and the Leisure Centre. The applicant must ensure that the operations of the adjacent Leisure Centre do not impact negatively on amenity of the proposed uses following the Agent of Change principles.

Fire Safety: The applicant should refer to the draft Fire Safety Guidance when formulating the Fire Statement.

Inclusive Design: 7 M4(3) wheelchair user units, all within social rent tenure, is accepted in this instance. Recommended to secure M4(2) and M4(3) requirements by condition. The library and medical centre would offer improved services within the community with a number of design measures to ensure excellent accessibility and an improvement from both of the current premises.

Digital Connectivity: Must ensure ducting space for full fibre connectivity infrastructure is provided to all end users within the development.

Heritage: There would be less than substantial harm resulting to the setting of the St John's Grove Conservation Area and a low level of harm to the significance of the locally listed building at 32 Junction Road, albeit at the lower end of this scale, which would need to be weighed against the public benefits of the proposal.

Transport: Public realm improvements welcomed. Suggested improvements to the surrounding highways have been recommended as part of the Active Travel Zone Assessment which should be secured by planning obligation or 278 agreement as appropriate. Operations parking for Medical Centre not supported. The applicant encouraged go over and above the cycle parking requirements also needs to set out further detail over access to the cycle parking.

Sustainable Development: Net Zero and passivhaus aspirations welcomed and strongly encouraged. Further details required for energy strategy. Excel version of Whole Life Carbon (WLC) assessment must be submitted. Outstanding matters identified in the GLA WLC Memo should be resolved. A condition should be secured requiring the applicant to submit a post-construction assessment to report on the development's actual WLC emissions. The Circular Economy (CE) Statement does not yet comply and should be updated. A condition should be secured requiring the applicant to submit a post-construction CE report.

Environmental Issues: The applicant has calculated the UGF of the proposed development as 0.55, which exceeds the target set by Policy G5 of the London Plan, demonstrating that urban greening has been embedded as a fundamental element of site and building design. The proposed planting includes eight trees, resulting in a net gain of five trees on site. A net biodiversity gain should be secured. No strategic concerns in relation to flood risk or drainage. The proposed development is air quality neutral and would not result in unacceptable risk of high levels of exposure to poor air quality. Conditions should be secured for 1. On-site plant and machinery must comply with the London Non-Road Mobile Machinery (NRMM) Low Emission Zone standards (London Plan Policy SI 1 (D)). And 2. Air Quality and Dust Management Plan (AQDMP)

9.4 **Historic England:**

Significance: Site adjacent to St John's Grove Conservation Area (CA), which is currently placed on the Heritage at Risk Register. The conservation area is characterised by low rise mid-19th century housing, with the Grade II* St John's Church designed by Charles Barry in 1828 as its crowning feature. Neighbouring tall buildings; Hill House, Vantage Point and Premier Inn, reaching up to 18 storeys in height are at odds with the prevailing low rise character of the area and considered to have a detrimental impact on local and wider London views. To the west of the site lies Parliament Hill which provides views of London's landmarks and tall building clusters and how they relate to each other.

9.5 **Impact: Block B (tallest element) would impact heritage assets located some distance from the site.**

Parliament Hill: The site is not situated within the protected view from Parliament Hill (LVMF 2B.1). The proposals would be visible from Parliament Hill appearing on the general skyline amongst the existing tall buildings within Archway, adding additional bulk and massing to this part of the vista. The scale of development and its relative closeness to Parliament Hill is considered to result in a prominent/dominant position in the wider view and affect townscape legibility that makes this viewpoint so important.

9.6 **St John's Conservation Area: Rendered images show the proposals would be visible in local views from St John's Grove Conservation Area appearing significantly taller than the prevailing streetscape and considered to erode the legibility of Archway as a 19th Century suburb to some extent.**

9.7 **Position: Historic England encourage the applicant to address these concerns, with the aim of negotiating a scheme that avoids significant impacts on this view. At the very least, the impacts on this view would need to be weighed in the planning balance in determining the scheme. Further work recommended to be undertaken to fully assess the impact of the proposals on the setting of the surrounding heritage assets and particularly on the Grade II* listed St John's Church and seek to mitigate those impacts wherever possible.**

9.8 Health and Safety Executive (HSE):

Initial concerns relating to there being a single staircase and lift in Block B serving dwellings which descended to the basement; this single staircase also being connected with areas of higher fire risk or other non-residential occupancies; a basement smoke vent directly below a window in the single staircase; and there being a basement smoke vent embedded within the building.

Following revisions to the plans and Fire Strategy Report HSE confirmed that they were satisfied with the information provided with the application.

9.9 Secure by Design - Metropolitan Police:

Range of security measures required: including compartmentalisation, secure lobbies, audio visual control access points, Gerder boxes, security rated door sets, use of self-closing and self-locking bins, spyholes for front doors, specification standards for communal doors and windows.

Condition to be attached requiring proposal to meet Secure by Design Certification.

9.10 Transport for London:

Scheme considered acceptable in terms of highways works, wayfinding, agent of change, blue-badge parking, car-free for residents, ambulance drop off bay, trip generation, cycle parking quantum and servicing.

The applicant should specify access to the cycle parking is London Cycling Design Guidance (LCDS) compliant.

Medical Centre Staff Parking: Following robust justification three staff car spaces within Girdlestone estate are deemed acceptable for duty doctor, duty nurse and duty manager roles.

A final Travel Plan requested to be secured as planning obligation. A final Construction Logistics Management Plan requested to be secured by condition.

9.11 Transport for London – London Underground/DLR Infrastructure Protection: No objections.

9.12 Thames Water: No objection subject to conditions relating to Piling Method Statement and informative regarding minimum water pressure.

9.13 London Borough of Camden: No comments received,

9.14 City of London: No comments received,

9.15 London Borough of Haringey: LBI Haringey's Design and transportation Officers raise no objections to this proposal. Officers request that an objection from the Highgate Conservation Area Advisory Committee is taken into account.

Internal Consultees

9.16 Affordable Workspace: No comments to make

9.17 Biodiversity Officer:

Agree with Ecological Impact Assessment (EIA) that the site has very low existing value for ecology. For any of the mature trees that are going to be felled then methodology to protect nesting birds and roosting bats must be followed (the approach for bat protection is outlined in the report).

Agree with the EIA reports recommendations for biodiversity enhancements regarding green roofing, general use bird boxes and planting a diverse range of native plant species including small trees and shrubs.

A lighting scheme which is sensitive to ecology should be put in place. Integrated swift bricks are preferred to swift boxes or sparrow terraces.

9.18 **Energy:**

Requested further evidence covering:

No objections following receipt of GLA emissions spreadsheet / tables / Unregulated emissions, review of active cooling within the non-residential elements, minor clarifications regarding heating strategies and amendments to Draft Green Performance Plan

Recommend a revised Energy Statement with appended feasibility studies is submitted.

9.19 **Environmental Health - Air Quality:**

The application achieves air quality neutral in terms of transport and building emissions. All the residential units will be provided with fresh air, via mechanical ventilation and heat recovery (MVHR) systems, which should be filtered/drawn from cleaner façade/height.

The scheme proposes Air Source Heat Pumps (ASHPs) and no combustion sources except for an emergency standby diesel generator.

It is essential that the flues associated with standby generator are situated at height and well away from air intakes, balconies, roof terraces and openable windows. Islington's Environmental Protection team will require the applicant to justify its purpose and complete a HMIP Technical Guidance TGND D1 "Guidelines on discharge stack heights for polluting emissions" calculation to ensure that the flue will not cause an amenity problem in adjacent areas.

It is requested construction air quality & dust impacts are covered in the Construction Management Plan (CMP) and/or Air Quality and Dust Management Plan (AQDMP).

9.20 **Environmental Health - Contamination:**

No objection subject to request condition securing submission of a land contamination investigation, remediation method statement of any necessary land contamination remediation works arising from the land contamination investigation and a verification report.

9.21 **Environmental Pollution – Noise:**

Leisure Centre: There is significant mechanical plant to serve the leisure centre which is close to the development site. The noise impact of this plant was included within a revised Noise Impact Assessment and was found to be acceptable. However, a condition is requested sound insulation and noise control measures.

Community Uses: Libraries were traditionally quiet spaces but are increasingly used for a wider range of uses which can be higher noise generating and the space includes a community room. A condition is requested ensuring that there is a higher standard of sound insulation between the ground floor community uses and the residential on the upper floors.

Plant: There is significant plant proposed for the basement, ground floor and roof areas, with ASHPs to be used for the residential units. A condition is requested to minimise the impacts of plant noise for proposed and existing receptors:

A condition and informative is requested for a written code for the management of noise from the standby generator.

9.22 **Highways and Traffic:**

Following tracking studies and amendments to the loading bay on Vorley Road moving this further west there are no objections.

There is the Girdlestone Nursery opposite the development and all vehicular movements and loading and unloading should take this into consideration. Including delivery times.

A financial contribution should be secured in the Director's Letter and section 278 agreement for public realm works, including a commuted sum if non-Islington standard materials are used such as permeable paving as proposed.

9.23 **Inclusive Design Officer:**

No objections following receipt of revised plans and subject condition to secure high quality wheelchair user homes and a condition securing the submission of details on appropriate seating and the provision of Sheffield cycle stands within the public realm, audio loop systems, medical garden accessibility, medical garden accessible play.

9.24 **Sustainability:**

Drainage: Proposed runoff rate of 2 l/s/ha and proposed drainage design are acceptable. The Etude Sustainable Design and Construction Statement states that blue roof storage is proposed which is supported. The Flood Risk Assessment and Drainage Strategy states, however, that the storage will be sub-surface tanks. Clarification on this was requested.

Urban Greening: UGF calculation – score of 0.56 is welcomed and exceeds policy requirement for 0.4 target. Proposed green roofs should be wildflower based with no more than a maximum of 25% sedum.

Biodiversity: The Sustainable Design and Construction Statement and Ecological Impact Assessment mention bird boxes for house sparrow and starling. There should also be an appropriate number of swift bricks as advised by an ecologist and in accordance with best practice guidance, including being placed in groups of at least three bricks.

Circular Economy: The commitments set out in the Circular Economy Statement are welcomed. This should be developed further in accordance with the GLA Circular Economy Statement Guidance.

9.25 **Trees Preservation Officer:**

No objections. The proximity of the trees along Vorley Road to the building line would necessitate regular pruning (repeated every few years) and concern was raised that the proximity could result in post-development pressure to excessively prune or remove trees to a detriment of public amenity and eco-system service provision.

9.26 **Urban Design & Conservation:**

Recommend approval. No objections raised in terms of scale and massing, landscaping, appearance, quality of accommodation, heritage impact. Concern is raised regarding bin stores being on both sides of Building C entrance.

Conditions are recommended to ensure high quality materials, cladding for roof plant, signage, location of bin stores.

10. RELEVANT POLICIES

- 10.1 Islington Council (Planning Committee), in determining the planning application has the main following statutory duties to perform:
- To have regard to the provisions of the development plan, so far as material to the application and to any other material considerations (Section 70 Town & Country Planning Act 1990);
 - To determine the application in accordance with the development plan unless other material considerations indicate otherwise (Section 38(6) of the Planning and Compulsory Purchase Act 2004) (Note: that the relevant Development Plan is the London Plan and Islington's Local Plan, including adopted Supplementary Planning Guidance.)
 - As the development is within or adjacent to a conservation area(s), the Council has a statutory duty in that special attention shall be paid to the desirability of preserving or enhancing the character or appearance of the area (s72(1)).
- 10.2 National Planning Policy Framework 2021 (NPPF): Paragraph 10 states: "at the heart of the NPPF is a presumption in favour of sustainable development.
- 10.3 The National Planning Policy Framework 2021 seeks to secure positive growth in a way that effectively balances economic, environmental and social progress for this and future generations. The NPPF is a material consideration and has been taken into account as part of the assessment of these proposals
- 10.4 Since March 2014 Planning Practice Guidance for England has been published online.
- 10.5 In considering the planning application account has to be taken of the statutory and policy framework, the documentation accompanying the application, and views of both statutory and non-statutory consultees.
- 10.6 The Human Rights Act 1998 incorporates the key articles of the European Convention on Human Rights into domestic law. These include:
- Article 1 of the First Protocol: Protection of property. Every natural or legal person is entitled to the peaceful enjoyment of his possessions. No one shall be deprived of his possessions except in the public interest and subject to the conditions provided for by law and by the general principles of international law.
 - Article 14: Prohibition of discrimination. The enjoyment of the rights and freedoms set forth in this Convention shall be secured without discrimination on any ground such as sex, race, colour, language, religion, political or other opinion, national or social origin, association with a national minority, property, birth, or other status.
- 10.7 Members of the Planning Committee must be aware of the rights contained in the Convention (particularly those set out above) when making any Planning decisions. However, most Convention rights are not absolute and set out circumstances when an interference with a person's rights is permitted. Any interference with any of the rights contained in the Convention must be sanctioned by law and be aimed at pursuing a legitimate aim and must go no further than is necessary and be proportionate.
- 10.8 The Equality Act 2010 provides protection from discrimination in respect of certain protected characteristics, namely: age, disability, gender reassignment, pregnancy and maternity, race, religion or beliefs and sex and sexual orientation. It places the Council under a legal duty to have due regard to the advancement of equality in the exercise of its powers including planning powers. The Committee must be mindful of this duty *inter alia* when determining all planning applications.

In particular, the Committee must pay due regard to the need to: (1) eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under the Act; (2) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it; and (3) foster good relations between persons who share a relevant protected characteristic and persons who do not share it.

10.9 In line with Section 72(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990, in assessing the proposals hereby under consideration, special regard has been given to the desirability of preserving the Conservation Area, its setting and any of its features of special architectural or historic interest.

10.10 In line with Section 66(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990, special regard has been given to the desirability of preserving the adjoining listed buildings, their setting and any of their features of special architectural or historic interest.

Development Plan

10.11 The Development Plan is comprised of the London Plan (March 2021), Islington Core Strategy 2011, Development Management Policies 2013, Finsbury Local Plan 2013 and Site Allocations 2013. The policies of the Development Plan are considered relevant to this application and are listed at Appendix 2 to this report.

Emerging policy: draft Islington Local Plan

10.12 The Regulation 19 draft of the Local Plan was approved at Full Council on 27 June 2019 for consultation, with consultation on the Regulation 19 draft taking place from 5 September 2019 to 18 October 2019. The Draft Local Plan was subsequently submitted to the Secretary of State for Independent Examination in February 2020. The Examination Hearings took place between 13 September and 1 October 2021, with consultation on Main Modifications running from 24 June to 30 October.

10.13 In line with the NPPF, Local Planning Authorities may give weight to relevant policies in emerging plans according to:

- the stage of preparation of the emerging plan (the more advanced its preparation, the greater the weight that may be given);
- the extent to which there are unresolved objections to relevant policies (the less significant the unresolved objections, the greater the weight that may be given); and
- the degree of consistency of the relevant policies in the emerging plan to this Framework (the closer the policies in the emerging plan to the policies in the Framework, the greater the weight that may be given).

10.14 Given the advance stage of the draft plan and the conformity of the emerging policies with the Framework it is considered that policies can be afforded moderate to significant weight depending on the significance of objections to main modifications.

10.15 Emerging policies that are relevant to this application are set out in Appendix 2. The emerging policies are considered to be consistent with the current policies.

Designations

10.16 The site has the following designations under the London Plan 2021, Islington Core Strategy 2011, Development Management Policies 2013, Finsbury Local Plan 2013 and Site Allocations 2013:

- Archway Town Centre (Adopted Plan)
- Archway Town Centre (4 Vorley Road - Emerging Plan)

- Archway Key Area
- Site Allocations 2013 – ARCH1
- Draft Site Allocations 2021 – ARCH1
- Article 4 Direction – B1 to C3 (4 Vorley Road)
- Article 4 Direction – A1 to A2
- Article 4 Direction - A1, B1, D1 to temporary 'flexible uses'
- London Underground Zone of Interest (Tunnels)

Supplementary Planning Guidance (SPG) / Document (SPD)

10.17 The SPGs and/or SPDs which are considered relevant are listed in Appendix 2.

11. ASSESSMENT

11.1 The main issues arising from this proposal relate to:

- Land Use
- Design, Conservation & Heritage Considerations
- Accessibility & Inclusive Design
- Quality of Accommodation
- Dwelling Mix
- Neighbouring Amenity
- Transport and Highways
- Energy & Sustainability
- Waste Management
- Biodiversity
- Trees
- Urban Greening Factor
- Air Quality
- Fire Safety
- Crime Prevention
- Flood Risk & Sustainable Drainage
- Land Contamination
- Affordable Housing & Viability
- Planning Obligations, CIL and Local Finance Considerations.

Land-Use

11.2 The key areas to assess are; the principle of the loss of the existing use: vacant bus depot and vacant community centre and the principle of the proposed uses: residential, medical centre and library. The principle of a tall building in this location is discussed in the Design & Conservation section.

Site Allocations

11.3 In the current Islington Local Plan: Site Allocations (2013), the application site forms part of a broad site allocation covering Archway Tower and Island site (ARCH1). In the Draft Islington Local Plan Site Allocations, the wider site allocation has been amended to include on this application site (ARCH1).

11.4 The existing, broader site allocation is for mixed-use development including residential. The key development considerations set out relating to this proposal are: -

- “the relocation of the library to a more prominent and accessible location, possibly integrated with other retail/community uses. A relocated library providing an improved facility would require circa 1,000sqm of floorspace.”
- enhanced pedestrian routes across the site
- active ground floor frontages to promote surveillance along Vorley Road
- “the Vorley Road bus stand will be considered as part of development proposals to contribute to the wider regeneration of the area. Proposals for this part of the site will involve consultation with Transport for London.”
- Consideration of St Johns Conservation Area

11.5 The Draft Local Plan Site Allocation now just encompasses the current application site at Vorley Road. Following the submission of the Islington Site Allocations to the Planning Inspectorate for Examination in February 2020, the text of the proposed modifications were published in March 2021. The allocation and justification of the Site Allocation ARCH1 was amended with the latest text is as follows:

“Residential-led development with social and community infrastructure uses. There may also be potential for an element of business floorspace including affordable workspace and space suitable for SMEs”

11.6 The key development considerations are: -

- “Archway Community Care Centre is located within the site at 4 Vorley Road. This use should be retained in line with policy SC1, or, where loss of the facility is proposed, justification must be provided in line with the policy.”
- “The Islington Tall Buildings Study suggests that the northern part of this site has the potential to accommodate a local landmark building of up to 15 storeys (46m), forming part of an Archway cluster of tall buildings alongside Hamlyn House, Hill House and Archway Tower.
- “The building should assist overlooking of and wayfinding from the adjacent open space, and mark a new entrance and internal route through the Archway Centre complex to connect with Junction Road.”

11.7 This more recent allocation, which has recently been reviewed and amended by the Planning Inspectorate, establishes the principle for residential-led development with social and community infrastructure uses on the site in addition to the principle of a fifteen-storey tall building.

Loss of Existing Uses

11.8 The site is comprised of a vacant community building (4 Vorley Road) and bus depot.

Loss of Community Building

11.9 The proposal would result in the demolition of the currently vacant Islington Adult Integrated Community Services Building (620.9sqm), a part one/part two storey building occupying the eastern end of the site.

11.10 Policy DM4.12 of the Development Management Policies (2013) states that “The council will not permit any loss or reduction in social infrastructure uses unless:

- i) a replacement facility is provided on site which would, in the council’s view, meet the need of the local population for the same use

- ii) the specific use is no longer required on site. In such circumstances, the applicant must provide evidence demonstrating:
 - a) that the proposal would not lead to a shortfall in provision for the specific use within the local catchment;
 - b) that there is either no demand for another suitable social infrastructure use on site, or that the site/premises is no longer appropriate for social infrastructure uses;
 - c) any replacement/relocated facilities for the specific use provide a level of accessibility and standard of provision at least equal to that of the existing facility.

11.11 Draft Local Plan Policy SC1 states that “the Council will not permit any loss of social and community infrastructure uses unless:

- i) a replacement facility is provided on-site. Any facility must be of at least equivalent quality, quantity and accessibility to that of the existing facility, and must be consistent with Part G. It must, in the Council’s view, ensure that the needs of the local population will continue to be met to at least the same level as the existing use; or
- ii) the existing use or another social and community infrastructure use is not required on site, demonstrated through:
 - a) provision of marketing and vacancy evidence to assess continued demand for the existing use and all other suitable social and community infrastructure uses that could be accommodated on site. Evidence must be provided for a period of at least 12 months, in line with Appendix 1; and
 - b) provision of a Community Needs Assessment detailing why the site cannot support social and community infrastructure uses and why the existing use is no longer required on-site as well as demonstrating that the needs of service users have been considered before making recommendations/submitting proposals involving replacement or relocation or consolidation of services; or
- iii) The proposal involves the loss/reduction/relocation of social and community infrastructure uses as part of a rationalisation of a recognised public sector body’s estates programme. The applicant will be required to provide a Community Needs Assessment demonstrating details of rationalisation, including that they have considered the needs of service users as part of any rationalisation strategy.”

11.12 As the Community Centre is proposed to be demolished, in accordance with Policy SC1 a Community Needs Assessment has been included within the Planning Statement.

11.13 This states that the site has been vacant since 2019 and was formerly occupied by the North Locality Community Team (social workers and support staff) and the Whittington Health Intermediate Care Team (a community rehabilitation service for adults with healthcare needs). These teams were relocated to Newington Barrow Way as part of an internal reorganisation to bring adult community services together under one roof, meaning that the service they provide has not been lost. The teams primarily provide their services in the community and therefore it is considered the change in location is unlikely to have had a detrimental effect on service users.

11.14 Both adopted policy DM4.12 part A and draft policy SC1 part D state that the council will not permit any loss of social and community infrastructure unless it can be demonstrated that the site is not suitable for the existing use or any alternative social and community infrastructure uses. In this case it is proposed that alternative social and community infrastructure uses will be accommodated on the site in the form of the relocated Archway Library (826sqm) and Archway Medical Centre (735sqm).

11.15 The demolition of the vacant Community Building is considered acceptable as the services formerly provided on the site have been relocated rather than lost and continue to serve the same user group to the same level. Furthermore, alternative social and community infrastructure will be provided at the site in accordance with the site allocation resulting in a significant uplift in community floorspace on the site of 940sqm.

Bus Depot

11.16 The former Archway bus depot (sui generis use class) that comprises the western part of the site has been vacant for a number years. It no longer serves any current or longer-term strategic need of TfL who raised no objection to its loss. As set out above the site has been part of or solely allocated site for a residential mixed-use redevelopment. The loss of the bus depot use is considered acceptable.

Proposed Community Use Floorspace

11.17 Part (C) of adopted Policy DM4.12 states that new social infrastructure and cultural facilities, including extensions to existing infrastructure must:

11.18 Be located in areas convenient for the communities they serve and accessible by a range of sustainable transport modes, including walking, cycling and public transport;

11.19 Provide buildings that are inclusive, accessible, flexible, sustainable and which provide design and space standards which meet the needs of intended occupants;

11.20 Be sited to maximise shared use of the facility, particularly for sports, recreational and community uses; and (iv) Complement existing uses and the character of the area, and avoid adverse impacts on the amenity of surrounding uses.

11.21 In considering the provision of a social or community infrastructure use, Draft Local Plan Policy SC1 repeats these requirements, noting in Part G at criteria (ii) that the building must also be sustainable; and a new criteria is introduced which requires that appropriate drop-off / pick-up facilities for disabled people.

11.22 The proposal includes the provision of a new library and a medical centre, to which existing facilities located in Archway will relocate. The application site is within Archway Town Centre and is considered to be an optimal location for social infrastructure uses. The proposed uses would sit between existing social and community facilities with Archway Leisure Centre adjacent to the north west, Girdlestone Park/playground/ball court opposite to the south west and Archway Children's Centre opposite at the eastern end Vorley Road, creating a civic focus to Vorley Road/MacDonald Road for the local community which would support the continued vitality of Archway Town Centre.

11.23 In accordance with Policy DM4.12 above the location of the new facilities would be convenient for the local community they would serve being highly accessible by public transport. The proposed public realm upgrades would also improve the pedestrian and cycling experience on Vorley Road, a designated 'clean air route', and improve links between all of the community uses.

Library

11.24 The existing Archway Library is located at 23 Highgate Hill. Due to the level changes in the Archway Island, the facility is located at lower ground level, and it is accessible by steps and ramp from Highgate Hill. The facility is accessed by double doors and located under a raised walkway which

reduces the amount of natural light in the building. These aspects reduce the visibility and sense of security of the existing library.

- 11.25 The proposed new library on Vorley Road would be located in Building A at ground and first floor, within the 'plinth' of the development. Islington Library Services have been involved in the pre-application stages directing the floorspace and layout of the new library. It has been designed to ensure high standards of inclusivity and accessibility. Islington's Inclusive Design officer was consulted on the application and further refinements to this have been incorporated within revised plans with other elements recommended to be secured by condition (29). The spaces provided would meet the needs of intended occupants. In terms of maximising shared use of the facilities, the library would include a community room which can be let for training courses, yoga classes and parent toddler groups etc. At first floor level there would be quieter reading areas, study zones, an IT suite, flexible meeting rooms and a soundproofed music practice room.

Medical Centre

- 11.26 The existing Archway Medical Centre is located at 652 Holloway Road, Archway, within a four storey terraced building which is accessible only by stairs leading up from the street. The proposed new medical centre has been designed in consultation with the practice managers and support staff. It would be located in a unit that occupies ground and first floor of Building B and ground floor of Building C within the plinth of the development. It has been designed to ensure high standards of inclusivity and accessibility. Islington's Inclusive Design officer was consulted on the application and further refinements to this have been incorporated within revised plans with other elements recommended to be secured by condition (29).
- 11.27 The proposal would be in accordance with the draft Site Allocation which seeks a 'residential-led development with social and community infrastructure uses.'
- 11.28 The principle of a proposed new library and medical centre is therefore supported. Libraries and medical centres formerly fell within the D1 use class however a library is now within the F1 use class and a medical centre is within the E use class. To ensure ongoing social and community infrastructure use of the site in accordance with adopted policy DM4.12 and emerging policy SC1, the library (F1 use sub-class F1(d)) and the medical centre use (E use sub-class E(e)) would be secured through a planning condition (46).

Proposed Residential Floorspace

- 11.29 The London Plan supports the building of more homes through Policy GG4, which promotes the delivery of genuinely affordable homes and the creation of mixed and inclusive communities, with good quality homes that meet high standards. Policy GG2 requires development proposals to make the best use of land by enabling development on brownfield land well-connected by public transport and by applying a design-led approach to determine the optimum development capacity of sites.
- 11.30 The London Plan also supports increasing housing supply and optimising housing potential through Policy H1, which states that the potential for housing delivery on all suitable and available brownfield sites should be optimised.
- 11.31 Islington's Core Strategy supports the provision of high quality, inclusive and affordable homes, seeking to meet and exceed the borough housing targets (set by the Mayor of London). Whilst this Policy CS12 refers to following and not exceeding the densities level set in the London Plan, these measures have since been deleted from the current London Plan.
- 11.32 The draft Local Plan Policy H1 states that high quality new homes are integral to achieving the aim of making the most efficient use of land and improving quality of life of residents. Furthermore, this policy states that Islington is committed to meeting and exceeding the boroughs minimum housing delivery target as set out in the London Plan.

- 11.33 The proposal would be in accordance with the draft site allocation which seeks a 'residential-led development with social and community infrastructure uses'. The principle of a residential use on this site is therefore supported.

Land Use Conclusion

- 11.34 The loss of the existing vacant community building and bus depot, and proposed replacement with a library, medical centre and residential use, inclusive of affordable housing support the delivery of the draft Site Allocation and would support Archway Town Centre. The proposed land uses are acceptable in principle.

Design, Conservation and Heritage Considerations

Urban Design

- 11.35 Quality of design lies at the heart of the planning system and is stressed at Chapter 12 of the NPPF (Achieving well-designed places). It states that the creation of high-quality buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities. The NPPF requires that developments are visually attractive as a result of good architecture, layout and appropriate and effective landscaping; and are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change. Further it states that permission should be refused for development of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions.
- 11.36 The London Plan, throughout Chapter 3 (Design) sets out its support for high quality architecture and public realm, which responds to local context by delivering buildings that are positioned and of a scale, appearance and form that response to the identify and character of the locality.
- 11.37 London Plan Policy D3 sets out the design-led approach that optimises the capacity of sites, including site allocations. Optimising site capacity means ensuring that development is of the most appropriate form and land use for the site. Higher density developments should generally be promoted in locations that are well connected to jobs, services, infrastructure and amenities by public transport, walking and cycling. Where these locations have existing areas of high-density buildings, expansion of the areas should be positively considered by Boroughs where appropriate.
- 11.38 London Plan Policy D3 also sets out criteria relating to appropriate form, layout, experience, quality and character which will be addressed in this section. Following this there will be further design scrutiny of the tall building within the scheme in accordance with London Plan Policy D9 and draft Local Plan Policy DH3 which both set stringent criteria for tall buildings.
- 11.39 Draft Local Plan Policy DH3 states that tall buildings must be high quality in accordance with policy PLAN1. The design of tall buildings must consider the individual and cumulative visual, functional, and environmental impacts, avoid negative impacts through good design, and mitigate any remaining negative impacts as far as possible.
- 11.40 At the local level, policy CS9 of Islington's Core Strategy (2011) sets out an aim for new buildings to be sympathetic in scale and appearance and to be complementary to local identity.
- 11.41 Development Management (2013) Policy DM2.1 states all forms of development are required to be of high quality, incorporate inclusive design principles and make a positive contribution to the local character and distinctiveness of an area, based upon an understanding and evaluation of its defining characteristics. Permission will be refused for development of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions.

- 11.42 Draft Local Plan Policy PLAN1 states that all forms of development are required to be of a high quality and make a positive contribution to local character, legibility and distinctiveness, based upon an up-to-date understanding and evaluation of the defining characteristics of an area. This policy outlines the high-quality design expectations which proposals will need to meet.
- 11.43 London Plan Policy D9 states that buildings should only be developed in locations that are identified as suitable in Development Plans and sets out a range of visual, functional, environmental and cumulative considerations to assess tall buildings.

Height and Massing

- 11.44 The site forms the southern boundary of the 'Archway Island', much of which is comprised of a 1960s complex of three orthogonally arranged tower blocks with four-storey plinth and a series of level changes, ramps and walkways at lower levels. The proposed tower element (Building B) would correspond logically to the existing Archway tower cluster in its orientation, scale and form. It would be a similar simple rectangular block layout to Hill House and Hamlyn House. Positioned parallel to Hill House and Archway Tower it would slot into the existing orthogonal arrangement. Moreover, as you move from Archway Tower (18 storeys) to Hill House (15 storeys) to the proposal (13 storeys), the towers would also incrementally reduce in width and step down in height. The proposed height would be a mediating step to the smallest of the existing towers Hamlyn House (10 storeys). As such the scale, massing and form of Building B respond positively to the Archway island cluster, reinforcing a coherent identity for this part of Archway Town Centre.

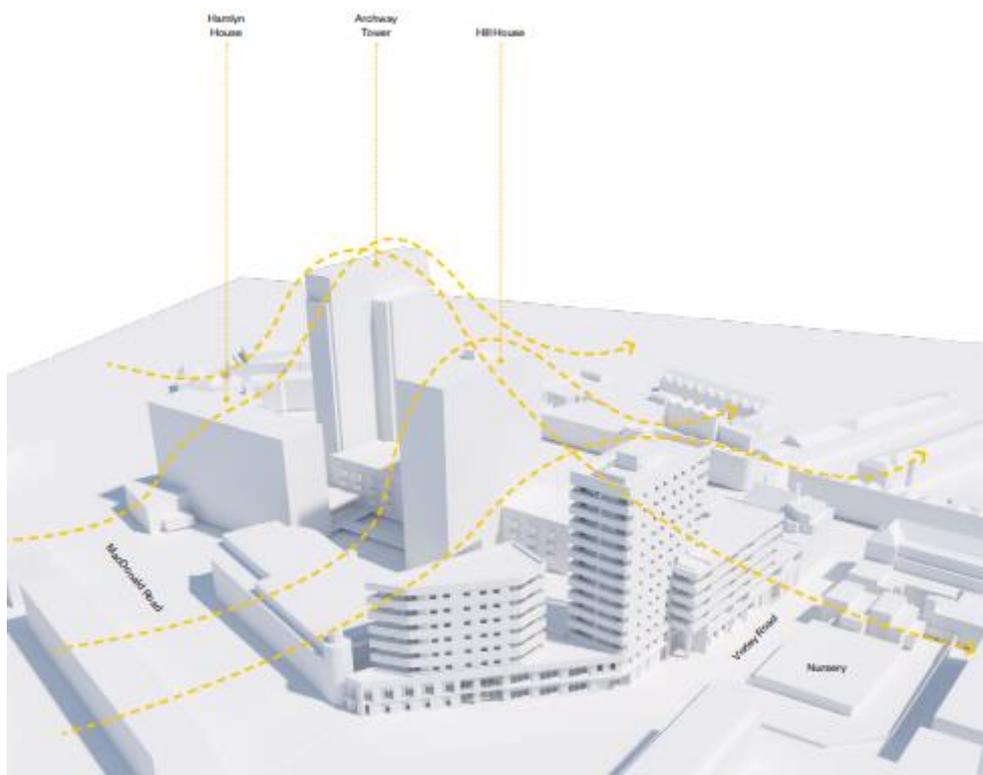


Figure 9 - Stepped massing is subservient to Archway Island cluster

- 11.45 The Girdlestone Estate is located to the south and west of the site, and is characterised by consistent four-storey blocks. Junction Road to the east is varied in its architecture and scale with a the two, three and four storey buildings. On the south side of Vorley Road is the Archway Children's Centre, a single storey building. The lower blocks either side of the tower element (Building A and C) would be eight and part six, part two storeys in height respectively. These are considered successful further steps down to the surrounding scale outside of Archway island. Building A is considered to have greater capacity for more height given its location opposite and

addressing the open space of Girdlestone Park, while the main six storeys and then two storeys of Block C where the site meets 2 Vorley Road/34 Junction Road is considered to respect the scale in this location, providing a transition down to the largely lower scale of Junction Road.

- 11.46 As an aspiring net-zero carbon scheme, the design has been guided by this goal. The shape of the buildings have been kept simple, which together with the proposed solid to void ratios and materiality would support the delivery of a high energy efficient development.
- 11.47 The form of each building is considered to define each building while clearly being a family of buildings. The form of Building A is guided by the tapering site to create a multi-angled façade, Building B and C both have a regular block form, albeit with Building B clearly reading as the tower element and Building C incorporating a more horizontal emphasis. The two-storey civic plinth would provide a unifying base and human-scale where the scheme meets the ground. The plinth is considered to enables the key functions of the building to be read, whilst also addressing the neighbouring community uses and park.
- 11.48 At the most recent DRP Chair's Review (13th October 2022), the panel raised concern regarding the overly solid detailing of the plant on Building B, which is the highest element of the proposal. A requirement to submit details of the plant enclosure on Block has been included in the architectural finish condition (11) with the aim of ensuring this element of the scheme is lighter and less prominent.

Elevational Design

Materials

- 11.49 The two lower buildings (A and C) would have dark brick elevations with matching dark window frames, while a light brick would be used for Building B. The plinth would incorporate exposed structural concrete elements to clearly break up and delineate the massing of the civic element from the residential buildings above.



Figure 10 – Use of dark and light bricks

- 11.50 The range of materials is considered durable with tones and textures that complement each as a family across the scheme. The range of materials doesn't vary widely for this reason as well as optimising energy performance.
- 11.51 The material palette responds appropriately to the surrounding context of Girdlestone Estate, Junction Road and the Archway Island. The use of brick as the main external material would

contrast with the commercial style glazing of neighbouring towers. The use of brick is considered to clearly state that the building is residential and mediates between this commercial aesthetic and the surrounding brickwork/concrete lintels of Girdlestone Estate to the south and west. The use of a dark brick for the lower elements corresponds to Girdlestone Estate and Junction Road. The use of a light brick for Building B distinguishes, emphasises and lightens the tower element. The use of board-marked concrete on the lower levels with a timber texture would help to soften the robust facade and ties in with the woodland setting of the park opposite. As stated above the overly solid appearance of the Building B plant enclosure is a concern and a lighter, more ephemeral cladding would be secured within condition (11).

- 11.52 As an aspiring net-zero carbon scheme the choice of materials and specification of components is of paramount importance in achieving that goal. The scale of the scheme limits the use of many sustainable material choices and brick does have a relatively high embodied carbon however it is excellent in terms of appearance and durability. The amount of solid balustrade has been reduced where possible to reduce the amount of materials required and render would be used underneath the balconies instead of metal soffits.

Brick Projections

- 11.53 The scheme proposes to use projecting brick detailing to add texture and depth to the elevations, which would provide further visual interest. The textural brick work would be at varying depths. A soldier course on the upper and lower side of the balconies and running across the width of the buildings would project 25mm. In between there would be rustication projecting 15mm of stretcher course bands in a corduroy effect in line with the windows. This brick detail does not require extra ties or fixing therefore limits compromise to the thermal performance of the building fabric. It also provides visual interest across all floors of the building. In response to Islington Urban Design and Conservation and Historic England's comments the brick banding was extended to the north elevation of Building B.

Fenestration and Balconies

- 11.54 The proposal would have an orderly grid arrangement for the windows across the development. Other than onto balconies, the windows are somewhat small and the overall level of glazing is modest although it varies across each of the elevations. The scheme balances overheating, energy performance and high quality design in terms of the extent of glazing which has been reduced during design development. Low energy buildings tend to require low levels of glazing, the southern aspect is sensitive to solar heat gain and in this case the northern aspect is sensitive in terms of overlooking to the adjacent site.
- 11.55 The southern, eastern and western elevations of the scheme are considered to have a good extent of windows that activate these principal elevations. The northern elevations of the scheme has a particularly high solid-to-void ratio in order to mitigate overlooking and the effects of heat loss in accordance with passivhaus principles. The windows on these elevations are limited to cross ventilation and daylight for some bedrooms as well as incidental light to some living areas. Lighter pre-cast concrete panels have been used within the soldier coursing band between windows to create a sense of larger openings and lighten areas of brickwork. The angled windows used on the north and east elevations are also considered to add visual interest to these elevations.



Figure 11 - North elevation Block B (left) and east elevation of Block A (right)

- 11.56 Different types of brise soleil are proposed across the south facing elevations to manage overheating and sunlight. Building A and the library would have vertical brise soleil to manage sunlight coming at a lower angle from the west. The louvres to the windows in building A are positioned above head height to ensure views out are not obscured. Building B would have horizontal brise soleil to manage sunlight from higher in the sky from the south. As well as serving these useful functions they are considered to add visual interest and aid in differentiating between the respective proposed buildings. They have been designed as an integral part of the window, fixed to the underside of the lintels. This ensures they are a robust, enduring elevational detail.
- 11.57 The inset balconies across the scheme appear as a carved feature into the main form, with the brick facing material and detailing running inside them. The balconies along the front of Building C also act as solar shading, allowing for generous amounts of glazing to the living areas opening out on to them.



Figure 12 - Windows, balconies and brise soleil (from left to right: Library, Building A, Building B, Balconies to Building B and Building C)

11.58 The varying design constraints and sustainability aspirations of the project are acknowledged, and the proposed materiality, brickwork detailing, fenestration, brise soleil, balconies are considered to present high-quality elevation design. The proposed materials palette are suitably robust and contextual. To ensure the quality and sustainability of the materials and architectural detailing of the proposal conditions (10 & 11) are recommended securing details and samples of the facing materials and architectural finish including a specific requirement relating to the brick projections and plant enclosure of Building B.

Layout

11.59 The draft site allocation ARCH1 seeks a 'a new entrance and internal route through the Archway Centre complex to connect with Junction Road.' The proposed ground floor layout does not provide a route through to the site to the north. This was considered during pre-application stage and ruled out following more detailed site investigations. The quality of a link was questioned given the convoluted arrangement and level changes of the Archway complex as well as how necessary this would be given there are relatively easy alternative routes around the Archway island. Furthermore, there isn't sufficient space on the application site to provide the required floorspace for the library, medical centre and a link. The provision of both these new community facilities was considered to outweigh the additional permeability of a link through the site.

11.60 The lower floors of the development would be a plinth comprising the library to the west and medical centre to the east. The entire south/south western elevation would address Vorley Road. It would comprise the active frontages and entrances of the two community facilities, entrances to the residential above, doors to bin stores and a substation. The proposal is considered to engage effectively with Vorley Road, clearly distinguishing between the public community and social uses and private entrances to the residential cores.

11.61 As stated previously, the proposal would create a continuous frontage of social and community infrastructure lending Vorley Road a civic focus that supports the vitality of Archway town centre. Moving from MacDonolad Road to Junction Road this would comprise a leisure centre, library,

park, playground, ball court, medical centre and children's centre, linked by enhanced public realm on Vorley Road which has a clean air walk designation.

- 11.62 The location of the library and medical centre at their respective ends of the site is considered to tie-in most effectively with the existing social and community facilities. The library would be located to the west of the site overlooking Girdlestone Park. As well as providing passive surveillance with large windows on the elevation which look out across the park, the library would benefit from views towards the mature trees. The medical centre has been located on the east of the site opposite the children's nursery. The similarly sensitive uses are considered to provide a more protected frontage to the busier end of Vorley Road towards Junction Road. The links to the existing and new uses is made easier and safer with the introduction of raised tables on Vorley Road as part of the public realm improvements.

Library

- 11.63 The new location for Archway library will be easily accessed at grade from the newly improved public realm along Vorley Road. Currently the library is situated below the Premier Inn (formerly Hamlyn House) via a series of ramps and steps. The library has been developed in consultation with Islington library services. Creating over 700sqm of library space would allow new facilities and services to be included in a more accessible and inclusive space. Accessed from the improved public realm there would be level access throughout and a platform lift.



Figure 13 – Library facade

- 11.64 The library's facade has been designed to encourage both views out and in with the chamfered reveals in the board-marked concrete and angling of the windows. The southerly aspect would be mediated with solar shading angled to the west to mitigate overheating. There would also be integrated opening panels to the side of each window for natural ventilation. The area of open-able panels has been developed in conjunction with the environmental and acoustic consultant to ensure a balance between comfort levels of the interior and noise from passing traffic. The chamfers to the bottom of each window in a smoother pre-cast concrete finish would be angled to avoid birds perching as well as an element of self-cleaning with rainfall.
- 11.65 On the ground floor a reception area would greet you from the main entrance. The reception area maintains good sightlines to the children's area on the far side and is also located to be in easy reach of the library staff office and new community room which can be let for training courses, yoga

classes and parent toddler groups etc. At first floor level the would be quieter reading areas, study zones, an IT suite, flexible meeting rooms and a soundproofed music practice room.

Medical Centre

- 11.66 The proposal has been designed in consultation with both the practice's designated healthcare planner and with several meetings with the practice managers and support staff. The entrance to the new facility would be located at the junction between Buildings B and C where a wider area of pedestrian public realm would be created and column and sheltered space is proposed. This is considered to create a high-quality entrance that aids in wayfinding with some cover from the elements.



Figure 14 - Entrance to Buildings A & B (left) and Medical Centre (right)

- 11.67 The new facility has step free access throughout with all patient-accessed areas located on the ground floor. There is a platform lift for staff use to the first-floor support areas. Level access is maintained out to the garden.
- 11.68 The new premises would expand on the services provided by the existing practice. Consultation and treatment rooms front Vorley Road with the main entrance located off the new generous area of public realm. The waiting room for patients has been located adjacent to the new medical garden area with generous glazing providing views and light. The first floor houses staff and support areas including meeting rooms and administrative offices.

Residential Cores

- 11.69 Both residential entrances end with a view to greenery. Buildings A and B would benefit from a green courtyard which the core of Building B would overlook.
- 11.70 The fact the scheme would only have one principle street-facing elevation means that all entrances, community use facades and servicing requirements must be located/undertaken along it. This constraint has resulted in bin stores being located on both side of both residential entrances and concern has been raised by Islington's Urban Design team in terms of creating a welcoming and active entrances. This is acknowledged however with no option to locate bin stores in less visible parts of the scheme or breaking up the library or medical centre facades this arrangement is considered optimal. High quality material treatment of the bin stores and entrances would be considered to mitigate this.

Landscaping

- 11.71 The proposal occupies the vast majority of the site. At the rear of the site there would be the green lobby and medical centre garden. To the front, public realm enhancements are proposed including the creation of two shared surface raised tables across Vorley Road. All proposed hard and soft landscaping outside of the site boundary, on highways land, has been designed to an adoptable standard and would be secured by S278 agreement.
- 11.72 The Vorley Road streetscape has been designed to create a leafy entrance for the new medical centre, library and residents' courtyards. It would have a series of low-level species rich planting incorporating SUDS features, with seating underneath the existing trees as not to obstruct sight lines and movement, with new trees providing year-round interest. The proposed bigger planters would also provide an informal play opportunity. The hard landscaping would use high quality robust materials and be secured by condition (12). The proposed lighting strategy would focus on safety and security while being wildlife friendly and would be secured by condition (27).
- 11.73 The proposal would create two shared surface raised tables across Vorley Road. One would convert an existing crossing at the western end of Vorley Road before it turns north and becomes MacDonald Road. The other would be a new crossing at the bend midway along Vorley Road that would be opposite the entrance to Building A and B. These would improve safe pedestrian links to and from the children's centre and Girdlestone park to the new development, aid in slowing down traffic and give it a much more pedestrian and cycle friendly emphasis. Tactile and contrast paving would be used to delineate the shared surface to support pedestrians with visual impairments.
- 11.74 The new streetscape is considered to reflect the design principles set out in the TFL 10 Healthy Streets Indicators.
- 11.75 The communal courtyard would be a small gated outdoor amenity space at the end of the entrance corridor to Building A and B accessible to all tenures within these two buildings. The courtyard would feature some loose tables and chairs, planting, 1.8m high gabion boundary walls and a central raised gabion planter with timber slate above for growing herbs and herbaceous planting.
- 11.76 The medical centre garden would be an outdoor amenity space for the patients, staff, and visitors. The courtyard is designed to be a flexible and tranquil space for patients. It would have a 2.5m wide central paved area with informal play through planting and a U-shaped raised seating area. The 1.8m high gabion wall would have climber growing over to provide visual amenity and wildlife value.

Conservation and Heritage

- 11.77 Section 66(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 ('the Act') requires decision makers to 'have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses' when determining applications which affect a listed building or its setting. Section 72(1) of the Act requires decision makers with respect to any buildings or other land in a Conservation Area to pay 'special attention... to the desirability of preserving or enhancing the character or appearance of that area'.
- 11.78 Chapter 16 of the National Planning Policy Framework 2021 (NPPF) deals with heritage assets. Paragraph 189 describes them as 'an irreplaceable resource' that 'should be conserved in a manner appropriate to their significance, so that they can be enjoyed for their contribution to the quality of life of existing and future generations' (paragraph 189).
- 11.79 Paragraph 200 of the NPPF states that 'any harm to, or loss of, the significance of a designated heritage asset (from its alteration or destruction, or from development within its setting), should require clear and convincing justification.'

- 11.80 Paragraph 201 of the NPPF states that ‘where a proposed development will lead to substantial harm to (or total loss of significance of) a designated heritage asset, local planning authorities should refuse consent, unless it can be demonstrated that the substantial harm or total loss is necessary to achieve substantial public benefits that outweigh that harm or loss’.
- 11.81 Paragraph 202 of the NPPF states that ‘where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use’.
- 11.82 Policy DM2.3 of the Development Management Policies states that ‘Islington’s historic environment is an irreplaceable resource and the council will ensure that the borough’s heritage assets are conserved and enhanced in a manner appropriate to their significance. Development that makes a positive contribution to Islington’s local character and distinctiveness will be encouraged.’
- 11.83 Policy DH1 (B) of the draft Local Plan states that ‘Islington is a diverse place of many different areas which give the borough its character. Development can be accommodated throughout the borough, but the scale of development is dependent on a number of considerations, including design and heritage.’
- 11.84 Policy DH1(C) of the draft Local Plan states that ‘development should protect or enhance the London View Management Framework views and Local Views (identified on the Policies Map).’
- 11.85 Policy DH1(F) of the draft Local Plan states ‘site potential for development and site density levels must be fully optimised, in order to make the best use of the scarce land resource in the borough.’

The Existing Site and Proposal

- 11.86 The site at present is unsightly and diminishes the experience of townscape locally especially when looking west along Vorley Road from the St John’s Grove Conservation Area towards Girdlestone Park.
- 11.87 The site’s location within Archway Town Centre and on the southern side of the Archway Island adjacent to an existing tall building cluster presents an opportunity for significant density of development which, as the Design section above sets out, the scheme is considered to optimise in a sensitive way. The scheme has evolved through careful assessment of the existing context, including examination of heritage assets, and within the constraints of high sustainability aspirations.

Impact on Heritage Significance

- 11.88 The proposal site has no heritage designations. There are no statutory or locally listed buildings on the site and it is not located in a Conservation Area. The site does not lie within a strategic or locally protected viewing corridor. The site is adjacent to 32 Junction Road, a locally listed building in St John’s Conservation Area which extends up Junction Road from larger expanses further south and east. Holborn Union Infirmary Conservation Area is north of the Archway island.
- 11.89 The applicant submitted a Townscape, Heritage and Visual Impact Assessment, by KMHeritage dated March 2022 and the full views assessment. It is considered that the visual effect as experienced by the pedestrian in the surrounding area would be to perceive the tallest part of the scheme rising above the surrounding roofscape with the lower parts only glimpsed, and the tower logically forming part of the Archway cluster in an ordered, stepped manner.
- 11.90 From the south, the tallest part of the development will be seen above the rooftops of the St John’s Grove Conservation Area and the locally listed buildings within that conservation area and on Junction Road, but only where longer views can be obtained. While the development will be seen as one moves north along Junction Road, the setting of locally listed houses on St John’s Grove

would be affected only where the development is seen through gaps between the locally listed buildings on the north side and across, for instance, the site of the parking garages next to St John's Grove, behind St John's Tavern.

- 11.91 In terms of impact, Historic England note that the proposal would be '*visible in local views from St John's Grove Conservation Area, appearing significantly taller than the prevailing streetscape, somewhat eroding the legibility of Archway as a 19th Century suburb.*' The GLA in their Stage 1 response concur stating that the development would impact the setting of the locally listed buildings at 32 Junction Road, 9 Junction Road and the former Red Lion Pub at 1 Junction Road. The reduction in height and massing towards the east of the site mitigates the visual impact of the proposal and assists with the integration of the proposal with the lower scale development to the east and south on Vorley Road and Junction Road. However, GLA officers consider that some harm would still result to the Conservation Area and the locally listed heritage asset at 32 Junction Road when viewed from the north-east where the proposal would distract from the lower-scale, 19th century terraced character of the buildings on Junction Road. The harm to the Conservation Area, however, is considered to be at the lower end of the 'less than substantial' in this instance.'
- 11.92 The fact that the scheme would be visible would not, by itself, mean it is harmful. As discussed above, the proposal design is considered to be of sufficiently high quality that it would be a significant improvement to the empty and underdeveloped site there presently. The significance of surrounding heritage assets considered does not inherently rely on there being no change occurring in their setting, nor is it necessarily harmed by the presence of new development in their setting that is highly visible. In terms of View 4 and 1 Junction Road the proposal is considered to complete the higher rise Archway island townscape in a well-considered way. However further south along Junction Road the increased height and massing would be a notable addition of the roofline and interfere somewhat from the lower scale of 32 Junction Road and this part of St John's Conservation Area, and some minor harm is considered to result.
- 11.93 The development would be seen along MacDonald Road from the Holborn Union Infirmary Conservation Area from the north. Views of the site from the north would be less expansive by virtue of the presence of Archway Tower, Hill House and Hamlyn House, seen in glimpsed views. The building's appearance would not be considered to harm the view from the Holborn Union Infirmary Conservation Area and could be considered an enhancement to the townscape by completing the southern end of Archway Island with a high-quality building of sympathetic scale, form and design. It would not be considered to harm the setting of the nearby listed or locally listed buildings. The effect on other heritage assets further afield would be considerably less by virtue of the lack of intervisibility or the degree of separation from the site and therefore no harm would be caused to heritage significance.
- 11.94 In consultation, English Heritage raised concerns regarding the impact of the proposal on views from Parliament Hill by adding additional bulk and massing that would be dominant in the wider view, affect townscape legibility and the ability to understand the urban composition and character of London.
- 11.95 As acknowledged in English Heritage's comments, the site does not fall within the designated viewing corridor from Parliament Hill. The proposal would be approximately 1.7km to the north east of the viewing point and a viewer would have to turn more than 90 degrees left away from the protected view (*LVMF 2B.1*) to see the building cluster within which the proposed new building would sit. In the wider panorama, the proposed new building would not be considered dominant, but rather subservient to the other towers in the Archway cluster. The proposed treatment of its elevations using light coloured materials for the tall element would allow it to recede into the skyline more successfully than its existing neighbours, helping to mitigate the cumulative massing effect.
- 11.96 It is therefore considered that the proposed development would not result in harm to a protected view. However, there would be less than substantial harm resulting to the setting of St John's Grove Conservation Area and the significance of the locally listed building at 32 Junction Road, albeit at

the lower end of this scale, which would need to be weighed against the public benefits of the proposal. The public benefits of much needed new housing, including a significant contribution of affordable housing, a library, a medical centre, public realm upgrades and improvements to Girdlestone Park are significant and are considered to outweigh the low level of harm to these heritage assets.

Design, Conservation and Heritage Conclusion

11.97 In accordance with Policy D9 of the London Plan tall buildings should only be developed in locations that are identified as suitable in Development Plans. The development would deliver on the draft Local Plan Site Allocation for a tall building of up to 15 storeys (46 metres). The proposal is considered to be of exemplary design responding appropriately to the varied context of the development in terms of scale, form, materiality and elevational design. The development would complete this southern end of the Archway Island tall building cluster in a sensitive way that does not compete with the existing tall buildings but sits comfortably alongside them. Harm to heritage assets would be considered limited and localised, being outweighed by the significant public benefits of the scheme.

Accessibility and Inclusive Design

11.98 London Plan Policy D7: Accessible housing requires that proposals provide suitable housing and genuine choice for London's diverse population, including disabled people, older people and families with young children, residential development must ensure that:

- 1) at least 10% of dwellings (which are created via works to which Part M volume 1 of the Building Regulations applies) meet Building Regulation requirement M4(3) 'wheelchair user dwellings'
- 2) all other dwellings (which are created via works to which Part M volume 1 of the Building Regulations applies) meet Building Regulation requirement M4(2) 'accessible and adaptable dwellings'.

11.99 Policy DM2.2 of the Islington Development Management Policies requires all development to demonstrate that they produce places and spaces that are convenient and enjoyable to use for everyone and bring together the design and management of development from the outset and over its lifetime.

11.100 To achieve this the proposal should be designed in accordance with Islington's Inclusive Design in Islington (2014) SPD.

11.101 The proposal would provide 7 wheelchair accessible units that meeting Building Regulations Part M4(3), which equates to 10% of the total number of units. All others would be accessible and adaptable units – M4(2) meeting the above policy and ensuring step-free access to all units and throughout the scheme.

11.102 Development Management Policy DM3.4 and London Plan Policy D7 requires wheelchair user M4(3) units to be distributed throughout the development to provide choice within a development. All of the proposed M4(3) units would be provided in Block C as it was agreed early on in the proposal's development with the Housing Options Team to provide all of the wheelchair user units within social rented tenures to meet an acute local need. It was also stated that larger homes were needed most, and of the two social rented buildings Building C provides the best opportunity to layout larger M4(3) homes given the width of the site in this location and the rectilinear massing this allows. Building C is also adjacent to the location of the existing accessible parking bays, currently ascribed to the Islington community services building but which would be used as Blue Badge car parking bays from the outset of this development. The tapered and angled form of Building A meant it would be difficult to provide the necessary larger wheelchair homes in any

efficient layout that optimises the number of units for the development. The location of the wheelchair user units within Building C is therefore considered acceptable.

11.103 Block C would also include a storage room and charging for three mobility scooters adjacent to the Block C entrance.

Medical Centre

11.104 The new facility has step free access throughout with all patient-accessed areas located on the ground floor. There is a platform lift for staff use to the first-floor support areas. Level access is maintained out to the garden to the rear which acts as an outdoor waiting room.

Library

11.105 The library would be accessed at grade from the improved public realm. It is proposed to provide a 'Changing Places' WC. The nearest facility of this type in the borough is at the Emirates Stadium, a new location here would help to make Archway and the surrounding social and community infrastructure more accessible. A platform lift would also been included to ensure both floors of the library are fully accessible for all users.

11.106 The Inclusive Design Officer commented on a range of detailed aspects of the scheme and found it acceptable following receipt of revised plans and subject to a condition (29) securing the submission of details on appropriate seating and the provision of Sheffield cycle stands within the public realm, audio loop systems, medical garden accessibility, medical garden accessible play. The requirement to provide the above stated level of M4(3) units and M4(2) units and the detailed design of M4(3) units would be secured by condition (19).

Quality of Residential Accommodation

11.107 London Plan Policy D6 requires housing development to be of high-quality design and provide adequately-sized rooms (in accordance with Table 3.1 in the London Plan) with comfortable and functional layouts, which are fit for purpose and meet the needs of Londoners without differentiating between tenures. The policy also requires qualitative aspects of a development to be addressed to ensure successful sustainable housing. Moreover, housing development should maximise the provision of dual aspect dwellings and normally avoid the provision of single aspect dwellings.

11.108 Finally, the design of development should provide sufficient daylight and sunlight to new and surrounding housing that is appropriate for its context, whilst avoiding overheating, minimising overshadowing and maximising the usability of outside amenity space.

11.109 Islington's Core Strategy policy CS12 advises that to help achieve a good quality of life for Islington residents, residential space and design standards will be significantly increased and enhanced from their current levels. Development Management Policy DM3.4 sets out the detail of these housing standards. Policy DM3.4 (part D (i)) states that 'new residential units are required to provide dual aspect accommodation unless exceptional circumstances can be demonstrated.

Private Internal Space

11.110 Standard 24 of the Mayoral Housing SPG, policy D6 of the London Plan (2019) and table 3.2 of the Islington Development Management Policies describe minimum standards of private internal space for new developments.

11.111 Accommodation Schedule Rev. N was submitted by the applicant. All proposed units provide a compliant level of internal floor area and built-in storage area. The floor areas and widths of the bedrooms are also compliant.

Private Amenity Space

- 11.112 Policy D6 (Housing quality and standards) states that in relation to private outside space: 'a minimum of 5sqm. of private outdoor space should be provided for 1-2 person dwellings and an extra 1sqm should be provided for each additional occupant, and it must achieve a minimum depth and width of 1.5m'.
- 11.113 This minimum private outside space requirement is also reflected in standards 26 & 27 of the London Housing SPG; Development Management Policies DPD Policy DM3.5; and Draft Local Plan Policy H5. There are some additional requirements for ground floor units in the local policies but these are not relevant to the scheme as none are proposed.
- 11.114 All residential units would have access to private amenity space in the form of balconies. Many of these would be generous, well exceeding the minimum standards. This would be particularly so for the two social rented blocks (A and C), as can be seen in the tables below.

Daylight & Sunlight within the Proposed Development

- 11.115 The submitted Daylight & Sunlight Report shows that the vast majority of rooms in the proposal would achieve their respective room Lux targets as set in the BRE Guidelines with a compliance rate of 94%. It is noted that whilst there are seven combined living/kitchen/dining rooms (LKDs) that would not achieve the 200 Lux target for a kitchen and therefore not counted as compliant, they would all achieve the living room target of 150 Lux over 50% of their area.
- 11.116 In addition to the seven LKDs, two bedrooms on levels two, three, four and five on the north side of Building B would also fail to reach their target level (100 Lux). These would have smaller windows that have been angled to prevent privacy issues and not prejudice development to the north. However, in these flats the main living space would receive good daylight levels.
- 11.117 In relation to sunlight, the Sunlight Exposure has been calculated for each living room. The updated BRE guidelines (2022) refer to BS EN 17037 and state that a 'space should receive a minimum of 1.5 hours of direct sunlight on a selected date between 1 February and 21 March with cloudless conditions.' All living rooms or LKDS would meet this target.
- 11.118 For new developments, the BRE guidance suggest that dwellings should be orientated so that at least one main window wall faces within 90 degrees of due south. This would be achieved by the proposal.
- 11.119 Each flat would comfortably exceed the BRE target level for sunlight.
- 11.120 For the above reasons the proposal would be considered to provide acceptable levels of daylight and sunlight.

Other Quality Considerations

Dual Aspect

- 11.121 London Plan Policy D6 states: 'that housing development should maximise the provision of dual aspect dwellings and normally avoid the provision of single aspect dwellings.'
- 11.122 Draft Local Plan Policy H4 states: 'that all new residential units should be dual aspect, unless provision of dual aspect is demonstrated to be impossible or unfavourable.'
- 11.123 All units would be at least dual aspect and some would be triple aspect, meeting the above policy.

Floor-to-ceiling heights

11.124 Development Management Policies DPD Policy DM3.4 and requires that in new housing development all habitable rooms are required to have a minimum floor to ceiling height of 2.6m (between finished floor level and finished ceiling level).

11.125 Floor-to-ceiling heights would be 2.6m, meeting the above policy.

Children's Play Space

11.126 London Plan Policy S4 states that 'development proposals for schemes that are likely to be used by children and young people should:

1) increase opportunities for play and informal recreation and enable children and young people to be independently mobile

2) for residential developments, incorporate good-quality, accessible play provision for all ages. At least 10 sqm of playspace should be provided per child.'

11.127 Development Management Policies DPD Policy DM3.6 requires all housing developments of more than 10 dwellings to make provision of play based on anticipated child yield. Adopted Islington policy sets a reduced standard of 5 sqm private / informal play space per child, including private outdoor space.

11.128 The draft Local Plan Policy SC2 requires at Part (C) states 'all major residential development must make appropriate on-site provision for free to-use publicly accessible play space, which is suitable for children and young people of all ages and abilities. Provision must be proportionate to the anticipated increase in child population as a result of development proposals. All proposed provision of new play space within development sites must be designed in partnership with Islington Council, in line with any relevant best practice standards.'

11.129 Paragraph 3.167 notes in the supporting text for Policy SC2 that 'the Council will take into consideration the London Plan benchmark for 10sqm of play space to be provided per child as a starting point but it is recognised that in Islington's context, delivery at this level may not always be feasible.'

11.130 Using the GLA population yield calculator the number of children that are expected to live at the development is 49. Therefore, 490sqm of play space is required to be delivered by the development. This is set out in the table below, with the breakdown in age groups:

Children's Age Range	Anticipated no. of children
0-4	22
5-11	16
12-16	7
16-17	4
Total	49

11.131 To optimise the amount of development on what is a constrained site it is proposed to provide a financial contribution of £260,000, secured by planning obligation, to upgrade the nearby playground in Girdlestone Park.

Although not 'on-site' provision as specified by policy, the playground is directly opposite Vorley Road on the western end of the site. The proposed public realm improvements including raised tables across Vorley Road would enhance safe pedestrian movement to and from Girdlestone Park. The existing playground (755 sqm) is dated and of relatively low quality. The financial contribution has been agreed by the Islington Park's Department who noted that: 'The current play area offers low play value, and the focus of the financial contribution should be on a significant increase in play value of playground. This will provide significant benefit to the wider community as well as the new families that will move into the development.'

11.132 The GLA stated that proposed approach to children's play space provision could be accepted given the immediate proximity to the park and the constraints of the site but appropriate financial contribution towards the park improvements must be robustly secured.

11.133 It is considered that the proposed financial contribution, secured in the Directors' Letter, to deliver a high-quality upgrade of the 755 sqm playground would meet the development's requirement for children's play space, while also providing an improvement for existing residents of Girdlestone Estate. Opportunities for incidental play on the southern boundary of the site would also be provided through the proposed planting beds and informal seating.

11.134 The Design and Access Statement submitted with the application provides details on an indicative layout for the playground to give an idea of what could be delivered on the site. This indicative proposal is subject to further design detail and public consultation which would be undertaken by Islington Parks Department.

Dwelling Mix

11.135 The NPPF speaks of the importance of ensuring that the size and type of new housing meets local need. London Plan Policy H10 states that schemes should generally consist of a range of unit sizes, having regard to local evidence of need, the requirement to deliver mixed and inclusive neighbourhoods, the need to deliver a range of unit types at different price points across London, the mix of uses in the scheme, the range of tenures in the scheme, the nature and location of the site, the aim to optimise housing potential on sites, the ability of new development to reduce pressure on conversion, sub-division and amalgamation of existing stock and the need for additional family housing and the role of one and two bed units in freeing up existing family housing.

11.136 Islington Core Strategy Policy CS 12 Meeting the housing challenge Islington will meet its housing challenge, to provide more high quality, inclusive and affordable homes by:

11.137 Requiring a range of unit sizes within each housing proposal to meet needs in the borough, including maximising the proportion of family accommodation in both affordable and market housing, and resisting the loss of existing units that are appropriate for the accommodation of families.

11.138 Islington's Local Plan: Development Management Policies DPD - Policy DM3.1 states that:

A. All sites should provide a good mix of housing sizes.

B. To ensure the range of housing sizes needed in the borough is provided, the housing mix required on all residential developments will be based on Islington's Local Housing Needs Assessment, and the requirements of any updated housing needs surveys and/or assessments prepared by or on behalf of the council.

Tenure	1-bed	2-bed	3-bed	4-bed or more	Total
Market	10%	75%	15%	0%	100%
Intermediate	65%	35%	0%	0%	100%
Social Rented	0%	20%	30%	50%	100%

Table 1 - Housing size mix required for each housing tenure (adopted Local Plan)

Tenure	Studio/bedsit	1-bed	2-bed	3-bed	4-bed
Social rented	None	Low/Medium	High	Medium	Low
Intermediate rent – at London Living Rent levels (or rents akin to social rent)	None	Medium	High	Medium	Low
Intermediate rent – other Discounted Market Rent products	None	High	Medium	None	None
Intermediate – shared ownership	None	High	Medium	None	None
Market	None	Low	High	Medium	Low

Table 2 - Housing size mix priorities for each housing tenure (draft Local Plan)

11.139 The draft Local Plan recognises that it may be necessary for affordable housing schemes to require a different affordable housing mix in order to address short term changes in need / demand as a result of specific policy interventions. Policy H2 states that all development proposals for residential dwellings must provide a good mix of unit sizes which contribute to the meeting of the Council's housing size mix priorities. For social rented housing, the highest priority is for 2 bedroom homes, followed by 3 bedroom and then 1 bedroom homes.

11.140 Development Management Policies document paragraph 3.14 states that 'the mix of dwelling sizes that is appropriate to specific developments should be considered in relation to the character of the development, the site and the area'. Furthermore, the Mayor's Housing SPG states that 'boroughs should consider applying local policies on unit size mix flexibly in town centre and edge of centre sites where there is good accessibility, recognising the particular suitability of these locations for 1 and 2 bedroom units. Boroughs may also wish to take into account the potential net benefits which can arise as a result of the provision of smaller units in relation to the existing stock of family sized homes, including the extent to which the provision of smaller accessible and adaptable units may encourage downsizing, freeing up larger homes for occupation by families.'

11.141 Table 3 below sets out the proposed housing mix:

	1 bed	2 bed	3 bed	total
Social rent				
<i>Number</i>	7	19	11	37
<i>Percentage</i>	19%	51%	30%	100%
Private				
<i>Number</i>	18	17		35
<i>Percentage</i>	51%	49%		100%
Total				
<i>Number</i>	25	36	11	72
<i>Percentage</i>	35%	50%	15%	100%

Table 3 - Proposed Housing Mix (tenure and size)

11.142 It is proposed that of the total 72 new homes, 35% will be one-bedroom homes, the majority of which will be for private sale. Of the total number of new homes, two-bedroom homes are the predominant type: a total of 50% are two bedroom, with an almost equal split between affordable, and private sale. One and two-bedroom homes represent the highest proportion of unit types, which are identified as most in need in the locality and are suited to the Town Centre location.

11.143 The smallest proportion of homes (15%) are three bed units, which are all social rent: 10No. 3B5P units, and one wheelchair accessible 3B6P unit are provided.

11.144 The provision of 37 affordable homes would be a valuable contribution to local and strategic housing objectives. The proposed housing mix is considered appropriate given the location of the proposal site, and would meet local demand and need.

Neighbouring Amenity

11.145 Chapter 12 of the NPPF details that new development should always seek to secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings. With specific regard to daylight and sunlight, the NPPF states that ‘when considering applications for housing, authorities should take a flexible approach in applying policies or guidance relating to daylight and sunlight, where they would otherwise inhibit making efficient use of a site (as long as the resulting scheme would provide acceptable living standards)’.

11.146 London Plan Policy D1 states that development design should deliver appropriate outlook, privacy and amenity. London Plan Policy D6 states that the design of development should provide sufficient daylight and sunlight for new and surrounding housing that is appropriate for its context, whilst avoiding overheating, minimising overshadowing and maximising the usability of outdoor space. London Plan Policy D13 seeks to reduce, manage and mitigate noise to improve health and quality of life.

Daylight and Sunlight: Assessment Guidelines

11.147 In general, for assessing the sunlight and daylight impact of new development on existing buildings, Building Research Establishment (BRE) criteria is adopted. In accordance with both local and national policies, consideration has to be given to the context of the site, the more efficient and effective use of valuable urban land and the degree of material impact on neighbours.

11.148 The starting point must be an assessment against the BRE guidelines and from there a real understanding of impacts can be understood. Knowing very clearly what the actual impacts are in the first instance is consistent with the judgement made in 'Rainbird vs Tower Hamlets [2018]'.

11.149 Once the transgressions against the BRE guidelines are highlighted, consideration of other matters can take place.

11.150 The 'Effective Use of Land' section in the Government's Planning Practice Guidance (PPG), confirms that consideration is to be given as to whether a proposed development would have an unreasonable impact on the daylight and sunlight levels enjoyed by neighbouring occupiers, setting out that all development should maintain acceptable living standards, although what will be appropriate will depend to some extent on the context. The Guidance cites city centre locations where tall modern buildings predominate is an area where lower daylight levels at some windows may be appropriate if new development is to be in keeping with the general form of its surroundings and optimise the use of land.

11.151 Whilst BRE guidelines are predominantly intended to assess the impact on residential properties, paragraph 2.2.2 (of the BRE guidelines) confirms that they may also be applied to existing non-domestic buildings where occupants have a reasonable expectation of daylight such as schools and some offices.

Daylight Guidance

11.152 The BRE Guidelines stipulate that... the diffuse daylighting of the existing building may be adversely affected if either:

- *the VSC [Vertical Sky Component] measured at the centre of an existing main window is less than 27%, and less than 0.8 times its former value.*
- *the area of the working plane in a room which can receive direct skylight is reduced to less than 0.8 times its former value." (Daylight Distribution)."*

11.153 At paragraph 2.2.7 of the BRE Guidelines it states:

"If this VSC is greater than 27% then enough skylight should still be reaching the window of the existing building. This value of VSC typically supplies enough daylight to a standard room when combined with a window of normal dimensions, with glass area around 10% or more of the floor area. Any reduction below this level should be kept to a minimum. If the VSC, with the new development in place, is both less than 27% and less than 0.80 times its former value, occupants of the existing building will notice the reduction in the amount of skylight. The area lit by the window is likely to appear gloomier, and electric lighting will be needed more of the time."

11.154 The BRE Guidelines state (paragraph 2.1.4) that the maximum VSC value is almost 40% for a completely unobstructed vertical wall.

11.155 At paragraph 2.2.10 of the BRE Guidelines state:

11.156 "Where room layouts are known (for example if they are available on the local authority's planning portal), the impact on the daylighting distribution in the existing building should be found by plotting the no sky line in each of the main rooms. For houses this would include living rooms, dining rooms, and kitchens; bedrooms should also be analysed although they are less important. In non-domestic buildings each main room where daylight is expected should be investigated. The no sky line divides points on the working plane which can and cannot see the sky."

11.157 Paragraph 2.2.13 considers the impact of balconies on existing windows:

“Existing windows with balconies above them typically receive less daylight. Because the balcony cuts out light from the top part of the sky, even a modest obstruction opposite may result in a large relative impact on the VSC, and on the area receiving direct skylight. One way to demonstrate this would be to carry out an additional calculation of the VSC and area receiving direct skylight, for both the existing and proposed situations, without the balcony in place. For example, if the proposed VSC with the balcony was under 0.80 times the existing value with the balcony, but the same ratio for the values without the balcony was well over 0.8, this would show that the presence of the balcony, rather than the size of the new obstruction, was the main factor in the relative loss of light.”

Sunlight Guidance

11.158 The BRE Guidelines state in relation to sunlight at paragraph 3.2.13:

“If a living room of an existing dwelling has a main window facing within 90° of due south, and any part of a new development subtends an angle of more than 25° to the horizontal measured from the centre of the window in a vertical section perpendicular to the window, then the sunlighting of the existing dwelling may be adversely affected. This will be the case if the centre of the window:

- *receives less than 25% of annual probable sunlight hours and less than 0.80 times its former annual value; or less than 5% of annual probable sunlight hours between 21 September and 21 March and less than 0.80 times its former value during that period;*
- *and also has a reduction in sunlight received over the whole year greater than 4% of annual probable sunlight hours.*

The BRE Guidelines consider orientation at paragraph 3.1.6:

“A south-facing window will, in general, receive most sunlight, while a north-facing one will only receive it on a handful of occasions (early morning and late evening in summer). East- and west-facing windows will receive sunlight only at certain times of the day. A dwelling with no main window wall within 90° of due south is likely to be perceived as insufficiently sunlit.

The BRE Guidelines go on to state at paragraph 3.2.3:

11.159 “... it is suggested that all main living rooms of dwellings, and conservatories, should be checked if they have a window facing within 90° of due south. Kitchens and bedrooms are less important, although care should be taken not to block too much sun.”.

Overshadowing Guidance

11.160 The BRE Guidelines state at paragraph 3.3.1:

“Good site layout planning for daylight and sunlight should not limit itself to providing good natural lighting inside buildings. Sunlight in the spaces between and around buildings has an important impact on the overall appearance and ambience of a development.”

11.161 The open spaces where the availability of sunlight should be checked is listed at paragraph 3.3.3 including gardens, parks and playgrounds.

11.162 The test for adequate sunlight of these spaces is set out at paragraph 3.3.17:

“It is recommended that for it to appear adequately sunlit throughout the year, at least half of a garden or amenity area should receive at least two hours of sunlight on 21 March. If as a result of new development an existing garden or amenity area does not meet the above, and the area that can receive two hours of sun on 21 March is less than 0.80 times its former value, then the loss of

sunlight is likely to be noticeable. If a detailed calculation cannot be carried out, it is recommended that the centre of the area should receive at least two hours of sunlight on 21 March.”

BRE Guidance Flexibility

11.163 Where the above guidelines are exceeded then daylight, sunlight or overshadowing will be adversely affected. However, light is just one among many important aspects of a successful development and must be weighed in the planning balance. Paragraph 1.6 states:

“Although it gives numerical guidelines, these should be interpreted flexibly since natural lighting is only one of many factors in site layout design. In special circumstances the developer or planning authority may wish to use different target values. For example, in a historic city centre, or in an area with modern high-rise buildings, a higher degree of obstruction may be unavoidable if new developments are to match the height and proportions of existing buildings.”

Daylight and Sunlight: Assessment

11.164 The applicant submitted a daylight and sunlight assessment prepared by Waterslade, dated: June 2022. This report updated the submission of a March 2022 version to considers the impacts of the proposed development on the residential neighbours against the latest BRE Guidelines released in June 2022.

11.165 The report considers the impact on all the surrounding residential windows facing the application site. The following properties were assessed:

- 71-106 Girdlestone Walk
- 25-64 Annesley Walk
- Hill House, 17 Highgate Hill
- 24 Junction Road
- 26 Junction Road
- 32 Junction Road
- 34 Junction Road
- Archways Children Early Years Centre
- 1-30 Girdlestone Walk
- 31-70 Girdlestone Walk

11.166 The axonometric view below shows the proposed buildings in situ and their relationship with existing built-form and consequently existing dwellings in the surrounding area.

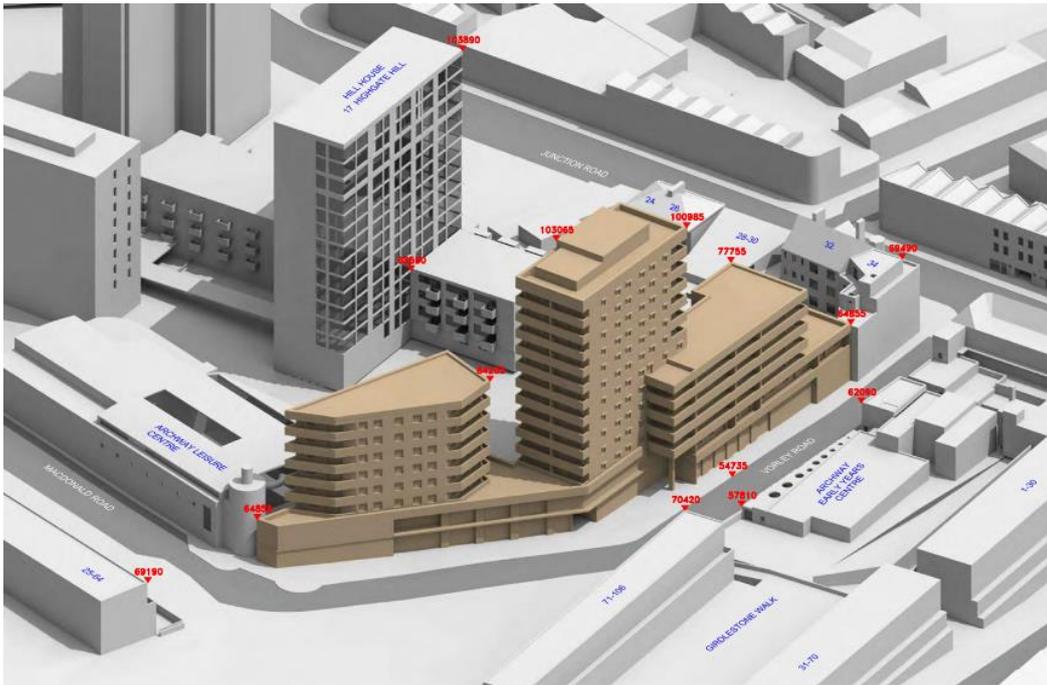


Figure 15: Axonometric view of proposed development and surrounding properties

Daylight

71-106 Girdlestone Walk

11.167 This residential block is located to the south-west of the site. Thirty-five windows across the northern elevation, highlighted in Figure 14 below, would see noticeable daylight reductions. The figures are presented in Table 4 below.

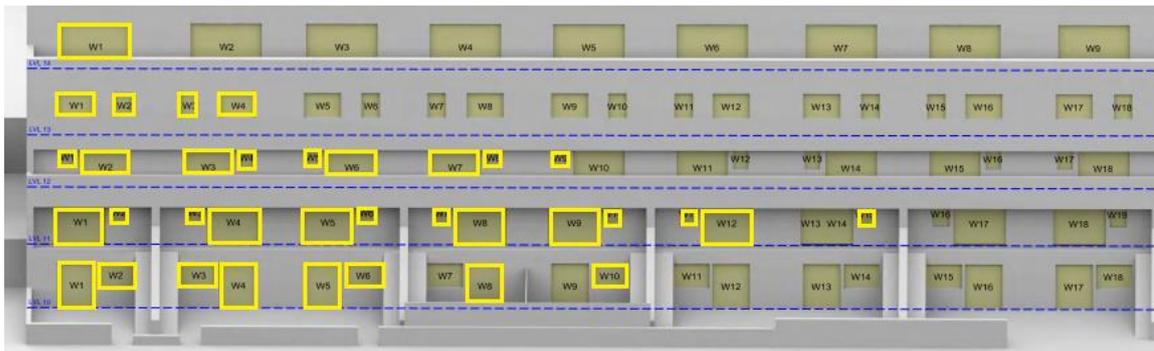


Figure 16 - 71-106 Girdlestone Walk – Windows affected by daylight transgressions

Room/ Window	Room Use	Vertical Sky Component			No Skyline (Daylight Distribution)		
		Existing (%)	Proposed (%)	Reduction (%)	Previous (sqm)	Proposed (sqm)	Reduction (%)
71-106 Girdlestone Walk							
Ground							
GND R1/10 W1/10	Bed	32.0	19.4	39%	116.5	110.9	5%
GND R2/10 W2/10	Bed	29.7	17.6	41%	108.6	93.7	14%
GND R3/10 W3/10	Bed	30.2	21.6	28%	111.3	103.4	7%
GND R4/10 W4/10	Bed	32.2	22.3	31%	112.9	103.0	7%
GND R5/10 W5/10	Bed	32.3	22.5	30%	113.7	105.7	10%
GND R6/10 W6/10	Bed	30.1	20.7	31%	109.5	99.0	7%
GND R8/10 W8/10	Bed	29.5	22.4	24%	113.2	84.7	25%
GND R10/10 W10/10	Bed	30.5	23.7	22%	109.1	90.1	17%
First							
1 ST R1/11 W1/11	Living	20.7	11.0	47%	164.8	158.7	4%
1 ST R2/11 W2/11	Bed	11.6	5.3	55%	56.1	33.9	40%
1 ST R3/11 W4/11	Living	21.3	12.7	40%	163.2	151.8	7%
1 ST R4/11 W3/11	Bed	11.7	6.0	49%	64.4	50.2	22%
1 ST R5/11 W5/11	Living	21.4	13.5	37%	163.2	161.2	1%
1 ST R6/11 W8/11	Bed	21.7	15.0	31%	163.2	118.0	28%
1 ST R7/11 W7/11	Living	12.0	7.2	40%	64.0	57.6	10%
1 ST R8/11 W6/11	Bed	11.9	6.4	46%	59.9	35.8	40%
1 ST R9/11 W9/11	Living	21.8	15.8	28%	163.2	161.8	1%
1 ST R10/11 W10/11	Bed	12.3	8.0	35%	61.3	34.3	44%
1 ST R11/11 W11/11	Living	12.3	9.0	27%	64.1	64.1	0%
1 ST R12/11 W12/11	Living	21.9	17.3	21%	163.2	142.9	12%
1 ST R14/11 W15/11	Bed	12.7	9.9	22%	63.5	45.7	28%
Second							
2 nd R1/12 W1/12	Kitchen	22.6	12.3	45%	50.6	40.7	20%
2 nd R2/12 W2/12	Living	29.9	18.7	38%	157.2	143.5	9%

2 nd R3/12 W3/12	Living	30.1	20.0	34%	159.2	159.1	0%
2 nd R4/12 W4/12	Kitchen	22.9	14.4	37%	46.9	28.9	38%
2 nd R5/12 W5/12	Kitchen	23.0	15.1	34%	49.1	42.0	14%
2 nd R6/12 W6/12	Living	30.3	21.6	29%	159.7	146.3	5%
2 nd R7/12 W7/12	Living	30.4	22.8	25%	157.9	146.3	7%
2 nd R8/12 W8/12	Kitchen	23.2	16.8	28%	49.1	27.7	44%
2 nd R9/12 W9/12	Kitchen	23.4	17.4	25%	49.9	49.4	1%
2 nd R12/12 W12/12	Kitchen	23.4	19.1	18%	51.4	36.9	28%
Third							
3 rd R1/13 W1/13	Bed	35.8	24.3	32%	122.1	118.6	3%
3 rd R2/13 W2/13	Bed	35.8	25.1	30%	71.1	55.2	22%
3 rd R3/13 W2/13	Bed	35.8	25.9	28%	71.4	62.6	12%
3 rd R4/13 W2/13	Bed	35.9	26.6	26%	120.1	115.4	4%
Fourth							
4 th R1/14 W1/12	Living	36.1	26.1	28%	200.3	197.7	1%

Table 4 - 71-109 Girdlestone Walk - VSC & DD Transgressions

Ground Floor

11.168 Eight ground floor windows would see noticeable VCS reductions. However, bar one room, the corresponding daylight distribution of the rooms that they serve would not result in noticeable losses of daylight. The one room experiencing VSC and NSL transgression would experience a noticeable loss with 25% reduction to NSL, which is marginally over the 20% threshold set out in the BRE.

First Floor

11.169 Thirteen first floor windows would see transgressions against the BRE Guidelines for VSC reductions. As shown in Figure 8, the windows at first floor level are recessed under balconies and as a result have lower existing VSC values of 11.6-21.9%, which would be reduced to 5.3-17.3% in the proposed situation. Six would experience transgressions against the BRE Guidelines for NSL in the rooms that they serve (22-44%).

11.170 BRE guidelines state that because balconies or overhangs cut out light from the top part of the sky, even a modest obstruction opposite may result in a large relative impact on the VSC, and on the area receiving direct skylight. One way to demonstrate this is to carry out an additional calculation of the VSC and area receiving direct skylight, for both the existing and proposed situations, without the balcony in place. This was calculated and improved the number of VSC transgressions to eight. Of these eight the proportional reductions would be significantly less and the retained VSCs would be considered marginal transgressions (21.7-26.7%).

Second Floor

11.171 Nine second floor windows would see transgressions over and above the BRE Guidelines for VSC. Three of these windows would also transgress guidelines for NSL levels in the room that they serve, while one window would meet the VSC target but have reductions to NSL in excess of the BRE Guidelines. Again, these are recessed under balconies overhanging of the third floor. Calculated without the

overhang the VSC transgressions would be improved to six and the retained VSC would be much improved bringing all up to marginal transgressions (22.9-26.4%).

Third and Fourth Floor

11.172 Four third floor windows and one fourth floor windows would see VSC transgressions and one of these would experience NSL losses in excess of BRE Guidelines in the room that they serve. Despite the noticeable proportional reductions, the retained VSCs would only be marginally below the 27% target (24.3-26.6%).

11.173 As demonstrated above, the proposal would result in losses to daylight in excess of the BRE Guidelines to the properties at 71-106 Girdlestone Walk. However, for the reasons set out above, these transgressions are in the most part marginally below the BRE Guidelines and/or when the existing overhangs are omitted are vastly improved. It is also important to note that any reasonable residential led development of this currently open site, in a town centre location and with excellent transport links would be likely to result in some reductions to daylight and sunlight to these properties.

Hill House, 17 Highgate Hill

11.174 Hill House is a residential tower, converted from office use under permitted development rights, located to the north of the site. The flats here are single aspect with some instances of projecting balconies above windows. The figures are presented in Table 5 below.

Room/ Window	Room Use	Vertical Sky Component			No Skyline (Daylight Distribution)		
		Existing (%)	Proposed (%)	Reduction (%)	Previous (sqm)	Proposed (sqm)	Reduction (%)
Hill House, 17 Highgate Hill							
First	<i>*NS – Not Stated</i>						
1 st R1/21 W1/21	LKD	30.4	20.2	33%	359.6	358.5	0%
1 st R1/21 W2/21	LKD	26.1	17.1	35%	359.6	358.5	0%
1 st R2/21 W3/21	Bed	21.1	12.9	39%	112.3	112.3	0%
1 st R3/21 W4/21	Bed	15.8	9.1	42%	135.2	131.7	3%
1 st R4/21 W5/21	NS*	16.0	12.7	21%	235.6	235.5	0%
1 st R4/21 W6/21	NS	16.1	11.0	32%	235.6	235.5	0%
1 st R5/21 W8/21	NS	18.9	12.6	33%	237.9	237.9	0%
1 st R6/21 W10/21	NS	21.6	13.5	38%	287.9	287.9	0%
Second							
2 nd R1/22 W3/22	NS	37.1	25.5	31%	287.9	287.9	0%
2 nd R1/22 W4/22	NS	36.9	25.6	30%	287.9	287.9	0%
2 nd R2/22	NS	36.6	25.6	31%	140.7	140.7	0%

W5/22							
2 nd R3/22 W6/22	NS	35.3	25.0	29%	271.5	267.5	1%
2 nd R4/22 W7/22	NS	33.8	24.1	29%	115.6	107.8	7%
2 nd R5/22 W8/22	NS	30.0	21.3	29%	329.1	329.1	0%
2 nd R5/22 W9/22	NS	24.4	16.5	32%	329.1	329.1	0%
2 nd R6/22 W10/22	NS	18.2	11.8	35%	199.5	166.8	16%
2 nd R7/22 W12/22	NS	14.8	9.8	34%	235.6	235.6	0%
2 nd R8/22 W14/22	NS	17.4	11.3	35%	237.0	237.0	0%
2 nd R9/22 W16/22	NS	19.6	12.1	38%	237.9	237.8	0%
Third							
3 rd R5/23 W9/23	NS	31.4	23.6	25%	329.5	329.5	0%
3 rd R6/23 W10/22	NS	23.5	16.3	31%	195.5	167.6	16%
3 rd R7/23 W11/22	NS	22.0	17.5	21%	236.2	236.2	0%
3 rd R7/23 W12/22	NS	24.0	19.0	21%	236.2	236.2	0%
3 rd R8/23 W13/22	NS	26.8	21.3	21%	237.9	237.9	0%
3 rd R8/23 W14/22	NS	28.3	22.2	21%	237.9	237.9	0%
3 rd R9/23 W15/22	NS	30.6	23.8	22%	238.5	238.5	0%
3 rd R9/23 W16/22	NS	31.6	24.1	24%	238.5	238.5	0%

Table 5 – Hill House – VSC and DD Transgressions

11.175 The lowest floor would generally see the highest relative VSC reductions. Eight first floor windows would see VSC reductions between 21-42%. Eleven second floor windows would see VSC reductions between 29-38%. Eight third floor windows would see VSC reductions of 21-31%, which are considered marginal transgressions. However, none would see losses to NSL in excess of the BRE Guidelines for the rooms that they serve.

11.176 The lowest floor would generally see the lowest retained VSCs and factors such as balconies and the windows at the corner where the tower meets the plinth are particularly important in determining the retained VSC figures. These features of the building design already have the effect of restricting the existing VSC levels. Of the 27 transgressions on this residential block, 14 windows would retain VSC levels between 19.0% and 25.6%, which although transgressions are on the more marginal end. Thirteen windows, however, would see retained VSC levels of 9.1-17.5% which represent more significant transgressions. Three of these windows would be in this lower range owing to balconies overhead and most would be on the lowest floor and/or positioned towards the corner.

11.177 Again, it is important to note that any reasonable development of this currently low level, partly empty site, in a town centre location would have impacts to the lower floors and corner of this building. The design of this conversion of a former commercial building is also questioned in which habitable rooms of single aspect flats have been placed in constrained locations such as this internal corner, although it is noted that those flats on the tower elevation would have floor-to-ceiling windows giving an open aspect. Although significant they are considered relatively isolated and unavoidable impacts given the appropriate scale of development within this dense, urban location.

The remaining windows across the wider façades of Hill House would retain excellent levels of daylight.

32 & 34 Junction Road

11.178 32 & 34 Junction Road are five storey buildings fronting Junction Road, backing onto the east of the site. They have commercial uses on the lower floor/floors and residential above. Nine windows across the residential upper floors of the buildings, highlighted in Figure 15 below, would see noticeable VSC reductions. The figures are presented in Table 6 below.

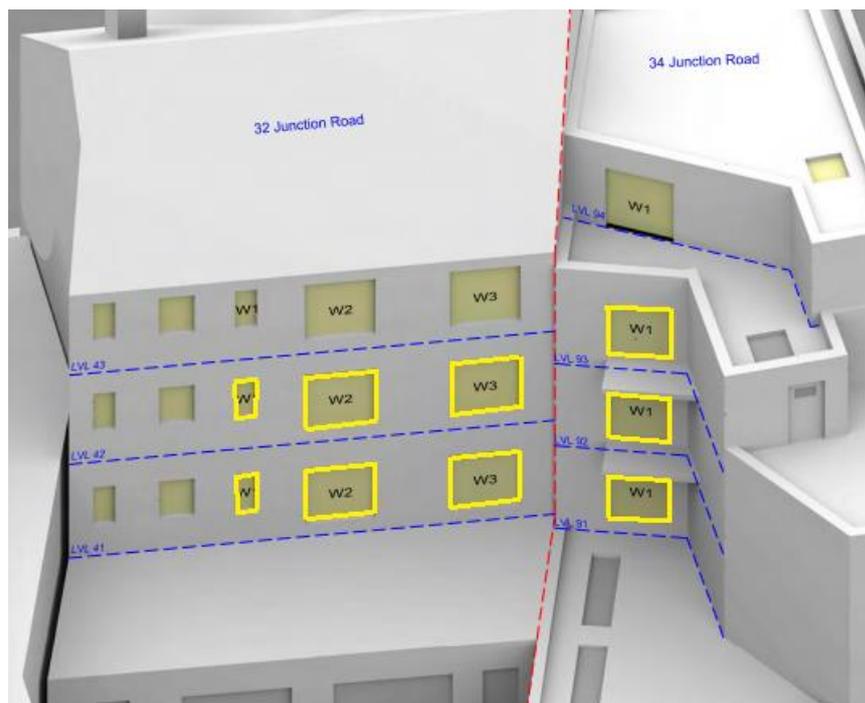


Figure 17 - 32 & 34 Junction Road – Windows affected by daylight transgressions

Room/ Window	Room Use	Vertical Sky Component			No Skyline (Daylight Distribution)		
		Existing (%)	Proposed (%)	Reduction (%)	Previous (sqm)	Proposed (sqm)	Reduction (%)
32 Junction Road							
First	<i>*NS – Not Stated</i>						
1 st R1/41 W3/41	NS*	28.3	20.1	29%	141.9	129.6	9%
1 st R2/41 W1/41	NS	31.8	22.8	28%	173.0	170.4	2%
1 st R2/41 W1241	NS	31.1	22.1	29%	173.0	170.4	2%
Second							
2 nd R1/42 W3/42	NS	32.2	24.4	24%	144.0	142.2	1%
2 nd R1/42 W1/42	NS	34.0	26.4	22%	170.6	170.6	0%
2 nd R1/42 W1/42	NS	33.7	25.9	24%	170.6	170.6	0%

34 Junction Road							
First							
R1/91 W1/91	Bed	18.2	10.4	43%	89.0	68.4	23%
Second							
R1/92 W1/92	Bed	21.9	13.0	41%	125.9	101.1	20%
Third							
R1/93 W1/93	Bed	29.1	21.9	25%	122.6	122.6	0%

Table 6 – 32 & 34 Junction Road - VSC and DD Transgressions

- 11.179 In terms of 32 Junction Road, six windows across the second and third floors would see reductions in VSC between 23 and 29%. However, none of the rooms these windows serve would see reductions in NSL below BRE Guidelines. The retained VSCs, although transgressions would also be considered more marginal (20.1-26.4%).
- 11.180 Three windows on the first, second and third floors of 34 Junction Road would have reductions to VSC beyond the BRE guidelines. The window on the third floor would see a more marginal VSC reduction of 25% but the lower two windows would see significant reductions (second floor, 41% and first floor, 43%). Of these, the first floor window would see a reduction in NSL to the room marginally below BRE Guidelines (at 23%).
- 11.181 Again, the impact of the design of the units are an important factor in both the relative reductions and retained VSC figures. All three of these windows would be adjacent to a large protruding element of the building and the two lower windows that would be significantly impacted are positioned below balconies. However, discounting the effect of the balcony the windows would still experience transgressions against BRE guidelines to daylighting.
- 11.182 The daylight and sunlight impact to these properties has been carefully considered during the design development with the massing cut back to the east at two storeys to minimise the amenity impact whilst optimising the development potential of the site. Although a negative impact of the scheme, these isolated impacts would be considered difficult to avoid given the design of the building and a reasonable expectation of the site's development potential. It should also be noted that the layouts for these properties show that the principal habitable living spaces in these units are on the opposite side of the building and this elevation would be unaffected by the proposed development.

1-30 Girdlestone Walk

11.183 1-30 Girdlestone Walk is located to the south of the site, south of Archways Children Centre. The figures are presented in Table 7 below.

Room/ Window	Room Use	Vertical Sky Component			No Skyline (Daylight Distribution)		
		Existing (%)	Proposed (%)	Reduction (%)	Previous (sqm)	Proposed (sqm)	Reduction (%)

1-30 Girdlestone Walk							
Ground							
GND R10/60 W10/60	Bed	25.2	20.9	17%	109.9	82.4	25%
GND R18/60 W18/60	Bed	25.8	20.3	21%	109.9	93.4	15%

Table 7 – 1-30 Girdlestone Walk – VSC and DD Transgressions

11.184 Two ground floor bedroom windows would experience reductions in VSC marginally in excess of the BRE Guidelines (21% and 25%). These impacts are considered minor, and isolated. All other windows in this building would retain good levels of daylight.

Archway Children’s Centre

11.185 Archway Children’s Centre is positioned immediately south of the site on the opposite side of Vorley Road. All twenty one windows on the north elevation, highlighted in Figure 17 below, would see transgressions in VSC against BRE Guidelines. The figures are presented in Table 8 below.

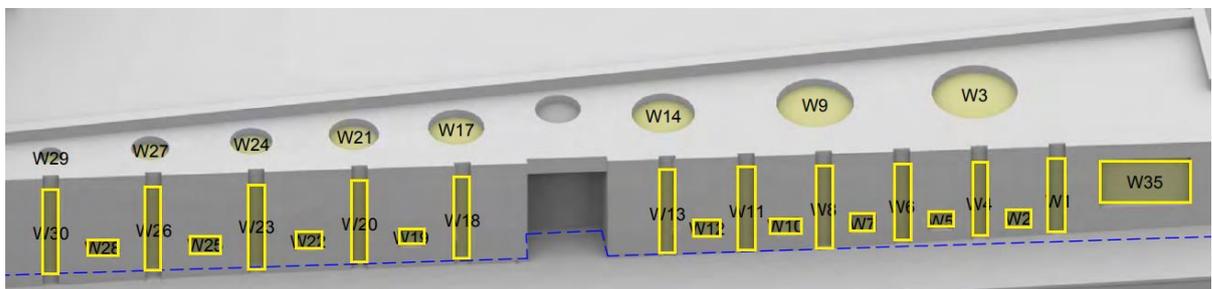


Figure 18 - Archway children's centre - Windows affected by daylight transgressions

Room/ Window	Room Use	Vertical Sky Component			No Skyline (Daylight Distribution)		
		Existing (%)	Proposed (%)	Reduction (%)	Previous (sqm)	Proposed (sqm)	Reduction (%)
Archway Children's Centre							
Ground							
GND R1/80 W1/80	Child room	30.7	8.8	71%	517.8	517.8	0%
GND R1/80 W2/80		30.5	8.5	72%	517.8	517.8	0%
GND R1/80 W4/80		30.7	8.6	72%	517.8	517.8	0%
GND R1/80 W5/80		30.3	8.4	72%	517.8	517.8	0%
GND R1/80 W6/80		30.6	8.5	72%	517.8	517.8	0%
GND R1/80 W7/80		30.2	8.3	72%	517.8	517.8	0%
GND R1/80 W8/80		30.5	8.5	72%	517.8	517.8	0%
GND R1/80 W10/80		30.2	8.3	72%	517.8	517.8	0%
GND R1/80 W11/80		30.5	8.6	72%	517.8	517.8	0%
GND R1/80 W12/80		30.1	8.4	72%	517.8	517.8	0%
GND R1/80 W13/80		30.4	8.6	72%	517.8	517.8	0%
GND R3/80 W18/80	Child room	30.0	9.0	70%	234.9	234.9	0%
GND R3/80 W19/80		29.5	8.9	70%	234.9	234.9	0%
GND R3/80 W20/80		29.8	9.4	69%	234.9	234.9	0%
GND R3/80 W22/80		29.1	9.3	68%	234.9	234.9	0%
GND R3/80 W23/80		29.4	9.8	67%	234.9	234.9	0%
GND R3/80 W25/80		28.7	9.9	65%	234.9	234.9	0%
GND R3/80 W26/80		29.2	10.5	64%	234.9	234.9	0%
GND R3/80 W28/80		28.5	10.6	63%	234.9	234.9	0%
GND R3/80 W30/80		29.9	11.3	61%	234.9	234.9	0%
GND R5/80 W34/80	Parent room	7.9	2.1	73%	571.9	133.7	77%

Table 8 – Archway Children's Centre – VSC and DD Transgressions

11.186 The 21 windows on the north elevation would see VSC reductions of between 61-63%. However, all bar a parent room would not see noticeable reductions to NSL although this room would see a significant reduction (77%). The retained VSCs would also be very low (8.8-11.3%).

11.187 There are several domed rooflights which will continue to provide ample daylight into the children’s room and the impact in relation to NSL is negligible. The parent’s room is not served by a rooflight hence the large NSL reduction. The daylight/sunlight impact on a use such as a nursery is given less significant in the BRE guidelines although it is still a consideration. It can be seen that it would still receive good daylight and retains external spaces which would not be unduly overshadowed, as covered below.

Sunlight Impacts

Hill House

11.188 In relation to sunlight, whilst there would be nine transgressions to Hill House, six of these would be marginal (20-25%). There would be more significant impacts to three windows located in the internal corner which lose almost all winter sunlight, however, the retained annual APSH values of between 21% and 37% are considered acceptable. The majority of windows would retain very good APSH values, over double the target BRE guidelines.

25-64 Annesley Walk

11.189 25-64 Annesley Walk is located to the north-west of the site. There would be minimal reductions in sunlight to this block with all windows retaining very good APSH values of over double the target BRE guidelines.

Overshadowing

11.190 Neighbouring amenity areas including Girdlestone Park, Annesley Walk, 26 & 24 Junction Road, and Archway Children’s Centre were tested. It can be seen from figure 18 that the overshadowing impact to all neighbouring amenity areas would be minimal and fully compliant with the BRE guidelines with all amenity areas retaining well over half their area receiving at least two hours of sunlight on 21 March.



Figure 19 – Two Hour Overshadowing Assessment - 21st March

Daylight & Sunlight to Neighbouring Properties Conclusion

11.191 While the impacts of the proposal on daylight levels to several neighbouring properties are acknowledged to be significant, harmful and a notable negative of the proposal, on balance this is not considered to outweigh the benefits of optimising an under-developed site to provide much needed additional housing, a library and a medical centre within Archway town centre at a scale of development that is contextual and takes advantage of the excellent transport accessibility.

Overlooking and Outlook

11.192 In the supporting text of Development Management Policy DM2.1 paragraph 2.14 and draft Local Plan Policy PLAN1 paragraph 1.67 and it states that to protect privacy for residential developments and existing residential properties, there should be a minimum distance of 18 metres between windows of habitable rooms. This does not apply across the public highway; overlooking across a public highway does not constitute an unacceptable loss of privacy.

11.193 The Mayor's Housing SPG introduces some flexibility to this stating that 'in the past, planning guidance for privacy has been concerned with achieving visual separation between dwellings by setting a minimum distance of 18 – 21m between facing homes (between habitable room and habitable room as opposed to between balconies or terraces or between habitable rooms and balconies/terraces). These can still be useful yardsticks for visual privacy, but adhering rigidly to these measures can limit the variety of urban spaces and housing types in the city, and can sometimes unnecessarily restrict density.'

11.194 In the assessment of privacy, consideration must be given also to the nature of views between windows of the development and neighbouring habitable rooms. For instance, where the views between habitable rooms are oblique because of angles or height difference between windows, there may be no harm.

11.195 Existing residential properties that surround the proposal site are either located at distance of more than 18 metres or are separated by a public highway. Windows of units that are on the northern elevation of the blocks, facing the servicing yard at the rear of the proposal site, have angled windows, or are secondary windows with the main aspect not facing the service yard.

11.196 The windows and doors of units on the eastern side of proposed Building A onto the balcony (southern elevation) and the windows of units on the western elevation of Building B (western elevation) would be 12 to 16 metres apart at the closest window to window relationships however these are positioned at near 90 degrees to each other and therefore are considered not to compromise privacy for these units within the development.

11.197 In terms of outlook impacts, the most sensitive end of the development is to the east of the site where the proposal approaches 2 Vorley Road/34 Junction Road and 32 Junction Road. Although there would be an impact to sense of enclosure to these buildings from the introduction of the increased massing, Building C has been cut back at the second floor upwards to allow for an separation distance to these buildings and Building B is still further. This relationship is considered not to result in an unacceptable outlook nor sense of enclosure.

Development Site to the North

11.198 Recognising that the land to the north of the site, which is currently largely vacant, has development potential of a scale and use that reflects its town centre location, the northern elevation has been designed to mitigate overlooking towards this neighbouring site.

11.199 The majority of the plans on the north side for the individual buildings would be comprised of non-habitable rooms, thus reducing the requirement for large windows.

- 11.200 A typical residential floor in Building A on the northern elevation would be comprised of servicing requirements such as ASHP rooms, buffer tank and bike stores with windows limited to the stairwell and angled windows on the western end. In any case the north elevation overlooks the flank of the Leisure Centre.
- 11.201 On levels 2-5 of Building B, clear glass windows of habitable rooms are angled away from the site to the north, while windows facing the site would be obscured.
- 11.202 On floors 06 – 13 of Building B, windows use translucent and clear glass; the latter is angled away from the adjacent site to ensure that opportunities for overlooking are reduced. The obscured glass windows that serve Living / Kitchen / Dining Rooms would be for additional light and cross ventilation, rather than providing views out.
- 11.203 Some small kitchen windows of flats in Building C would be 11 metres from the boundary however these would be across the gallery access and not principle windows for these flats which are orientated to their main southern aspect.
- 11.204 For the above reasons the development site to the north would not be considered to be unduly prejudiced by the proposed development.

Wind Microclimate

- 11.205 A Wind Microclimate Assessment by BRE dated 16th March was submitted with the application which presented findings from a wind tunnel study to assess the pedestrian level wind microclimate resulting from the proposed development.
- 11.206 The report found that in summer the wind conditions are the same as the existing wind conditions but in winter the conditions would be marginally slightly. Nevertheless, the majority of the test locations have wind conditions that are suitable for any pedestrian activity, including long-term sitting, entrance door usage, strolling and other more strenuous pedestrian activities. Eight windier test locations are identified that have wind conditions that are suitable for strolling and other more strenuous pedestrian activities. Many of these locations are sited on roadways or locations where pedestrians are likely to be walking and the assessed wind conditions are suitable for these activities along Vorley Road. All entrances were found to have suitable wind conditions.
- 11.207 It is therefore considered that the impact on wind microclimate conditions would be limited and acceptable.

11.208 Noise and Disturbance

- 11.209 Development Management Policy DM3.7 states that residential developments should be adequately separated from major sources of noise, such as road, rail and certain types of development. Mitigation will be required where the noise environment necessitates this.
- 11.210 The emerging Local Plan Policy states that all development proposals which have the potential to cause or exacerbate unacceptable noise and vibration impacts on land uses and occupiers in the locality must fully assess such impacts. Where noise and/or vibration impacts are identified suitable mitigation measures must be put in place to reduce these impacts to acceptable levels.
- 11.211 An updated Noise and Vibration Impact Assessment prepared by Max Fordham Ltd. – dated 9th June 2022 was submitted that included assessment of a significant plant equipment serving the adjacent Archway Leisure Centre which had previously been missing, in addition to noise impacts from road traffic around the site.

- 11.212 In terms of noise from the Archway Leisure Centre main Air Handling Unit serving the pools and the Electrical Substation, both located on the south end adjacent to the proposed development, the

report found that internal noise levels and noise levels at the external amenity spaces comfortably comply with the maximum noise levels allowed at these spaces.

- 11.213 In terms of traffic noise, the sound insulation performance of the facade glazing elements have been specified such that LBI internal noise targets in residential units would be met. The noise levels meet the guidelines in the vast majority of external amenity spaces except for four Block C 1st floors balconies facing Vorley Road where the noise levels are expected to be marginally above guidelines (up to 57 dB LAeq whereas the guidelines recommend 55 dB LAeq). This is a very minor exceedance (limited to four balconies) and is not expected to result in any significant reduction of the quality and usability of the amenity space.
- 11.214 When bedrooms and living rooms windows are 30% open for overheating control all residential units of the proposed development meet both the overheating criteria (TM59) and the internal noise levels guidelines published in the AVOG documentation.
- 11.215 In terms of plant on the site (twenty-six ASHP and seventy-seven MVHR units and two chillers) impacting neighbouring development, the noise levels generated by these units meet LBI targets at the nearest sensitive receptor. The noise level emitted by the emergency generator (to be installed in the basement of Block B) has been specified not to exceed 10 dB above background noise levels at the 1m from the façade of the nearest premises (GP Surgery staff entrance). This target applies when the equipment is being tested. Details of sound insulation and noise control measures to achieve internal targets, appropriate sound insulation between the proposed ground floor library and medical centre uses and upper floors residential use of the buildings as well as controls from plant noise and standby generator would be secured by conditions (14, 15, 16 & 42).
- 11.216 Vibration levels at the proposed development would also comfortably meet planning requirements for both day-time and night-time periods.

Impact on Neighbouring Amenity: Conclusion

- 11.217 The proposal would result in the development of a currently under-developed site. As such, the proposal would deliver a notable increase in massing and building height on site, which would in turn have a greater impact on neighbouring amenities when compared to the existing built form. It is acknowledged that especially in terms of daylight there are significant negative impacts however the level of impact is considered to not present an unacceptable impact.

Transport and Highways

- 11.218 The NPPF Chapter 9 emphasises the role transport policies have to play in achieving sustainable development and that people should have real choice in how they travel. Developments should be located and designed to give priority to pedestrian and cycle movements, and have access to high quality public transport facilities, and consider the needs of people with disabilities and reduced mobility.
- 11.219 London Plan Policies T1, T2, T3, T4, T5, T6 and T6.1 seek for all new development to identify opportunities to improve the balance of space given to people to dwell, walk, cycle, and travel on public transport and in essential vehicles, so space is used more efficiently and streets are greener and more pleasant. Adding to this, policies also set out requirements for levels of on-site vehicle parking, cycle parking and for servicing.
- 11.220 London Plan Policy T6 states that car-free development should be the starting point for all development proposals in places that are well-connected by public transport, but with provision made for disabled persons parking bays.
- 11.221 London Plan Policy T6.1 states that disabled persons parking should be provided for new residential developments. Residential development proposals delivering ten or more units must, as a minimum:

1) ensure that for three per cent of dwellings, at least one designated disabled persons parking bay per dwelling is available from the outset

2) demonstrate as part of the Parking Design and Management Plan, how an additional seven per cent of dwellings could be provided with one designated disabled persons parking space per dwelling in future upon request as soon as existing provision is insufficient.

11.222 London Plan Policy T7 states that seeks to ensure that development proposals facilitate safe, clean and efficient deliveries and servicing.

11.223 Development Management Policy DM8.2 requires that proposals meet the transport needs of the development and address its transport impacts in a sustainable manner and in accordance with best practice.

11.224 Draft Local Plan Policy T1: Enhancing the public realm and sustainable transportation requires that applicants must provide appropriate information to allow proper assessment of transport impacts and show how these impacts can be addressed. This policy goes on to state that all new development will be car-free, which will contribute to the strategic aim for a modal shift to sustainable transport modes.

11.225 The application site has a PTAL of 6a, which is considered 'excellent', due to the proximity to Archway Underground station and the numerous bus links from Archway.

11.226 The application site is located within a Controlled Parking Zone (CPZ) 'Zone P', which operates weekdays between 08:30 – 18:30 and on Saturdays between 08:30 – 13:30.

Servicing, Deliveries and Refuse collection

11.227 It is not proposed to provide any vehicle access into the site, and all deliveries, refuse collection and emergency vehicle access will all be undertaken on-street from Vorley Road (for Medical Centre and residential uses) and MacDonald Road (for the Library and residential uses) as well as double yellow lines near the substation which would allow deliveries for up to 40 minutes.

11.228 The Vorley Road loading bay was moved further west following tracking which showed that the carriageway width in the original position was too tight for buses to pass safely and might lead to vehicles mounting the pavement affecting pedestrian safety and movement. It is now in a safe and acceptable location.

11.229 LB Islington Highways/Traffic and accept the proposed delivery and servicing arrangements on Vorley Road.

11.230 Concerning the hours of delivery, a condition (21) would be attached requiring details of a final Delivery and Servicing Plan, including hours, frequency, location and size of vehicles to be submitted to and approved in writing by the Local Planning Authority.

Vehicle Parking

Residents

11.231 The proposal would be a car-free development, which would be secured by planning obligation that restricts future residents, with the exception of blue badge holders, from applying for parking permits.

11.232 There would be 2 blue badge spaces provided for the development from the outset. These would be located on-street along Vorley Road. The remaining 5 wheelchair accessible units (7% of total units) could be provided with one space per dwelling when required, provided through the conversion of existing on-street parking bays on Vorley Road. There are sufficient bays (total 16)

along Vorley Road within 70m access to the development to provide the required number of wheelchair accessible parking bays should there be demand. A Parking Design and Management Plan would also be secured by condition (22) to demonstrate the location of these bays and how the process of converting them to Blue Badge bays by request would be managed. A financial contribution is proposed to be secured in the Directors' Agreement for the provision of these spaces, or where they cannot be delivered to provide a contribution towards other accessible transport initiatives.

Medical Centre

11.233 The medical centre would have an ambulance drop-off bay directly outside of the entrance on Vorley Road. There would also be a doctors bay next to the ambulance drop-off bay for the duty doctor. Two business parking permits granted that allow parking within the CPZ. For service users there would be an additional accessible parking bay provided on Vorley Road next to the two residents' accessible bays. These are proposed to be secured within the Directors' Letter.

Cycle Parking

11.234 The draft Local Plan Policy T2 states that all new developments must provide cycle parking, and associated circulation space for ease of use of cycle parking, in accordance with the minimum cycle parking standards set out in Appendix 4 Table A4.1. These standards are either in line or exceed the London Plan standards.

11.235 It states that 20% of spaces must be for accessible cycle parking (2 sqm) to provide for non-standard cycles and ambulant disabled cyclists using regular cycle. The Council will prioritise cycle parking that is practically useful for the majority of people, which is secure and allows convenient ease of access and avoids the risk of personal injury when manoeuvring a bicycle; Sheffield stands in particular are supported.

Residents

11.236 The London Plan and draft Local Plan and guidance in the London Cycling Design Standards require; One long-stay space per studio or one-bedroom (one-person) dwelling; One and a half long-stay spaces per one-bedroom (two-person) dwelling; and Two long-stay spaces per two or more bedroom dwelling. The application requirements are set out below:

Flat Type	No. Flats	Bike Spaces required per Flat	Bike Spaces Required
1B2P	25	1.5	37.5
2B3P, 2B3P, 3B5P and 3B6P	47	2	94
			132

11.237 For residents of Building A there would be cycle stores adjacent to the stair/lift core on each of the six residential levels (02-07) accessed by a lift big enough for bikes. There would be 36 spaces in total within Building A (6 semi-vertical on each floor). In addition to this there would be 2 Sheffield cycle spaces in the covered, gated entrance between Building A and B.

11.238 Residents of Building B would have access to two stores. There would a ground floor store at the rear of the site accessed from the courtyard between Building A and B providing 28 spaces (24 in double stacked and 4 Sheffield) and basement store in Building B providing 32 spaces (all double stacked) with access from a bike lift directly from the entrance between Building A and B.

11.239 A ground floor store in Building C, adjacent to the stair core providing 30 more spaces (4 Sheffield, 10 semi-vertical and 16 double stacked).

11.240 This would provide a total of 128 long-stay residents' cycle spaces falling marginally short of the draft Local Plan policy requirement of 132 and the adopted Local Plan requirement of 130. It is noted that there would be an imbalance across the stores with more spaces than the policy requirement for Building A (+14) and less for Building B (-3) and Building C (-15).

11.241 For visitor cycle parking, the draft Local Plan cycle standards (which exceed the London Plan's requirements) state that residential development should provide 5 short-stay visitor cycle spaces for the first 40 dwellings and 1 space per 40 dwellings thereafter. At just under 80 dwellings 6 is considered the policy requirement. 7 visitor cycle spaces are provided in the covered, gated entrance between Building A and B, exceeding policy, and include some

11.242 It should be noted that in accordance with the London Cycling Design Standards (LCDS), 5% of cycle parking spaces for residents should be suitable for larger, accessible and cargo bikes. A total of six accessible cycle parking spaces would be spread across the scheme with two spaces in each of the three main stores. This is equivalent to 4.5% falling marginally under the 5% target. In addition, three mobility scooter spaces would be provided in Block C.

Medical Centre

11.243 The draft Local Plan and London Plan require 1 long-stay cycle space per 5 staff for health facilities and 1 visitor space per 3 staff. The proposed medical centre is projected to have up to 40 staff. Therefore, the policy requirement is to have 8 staff cycle spaces and 13 visitor spaces. 8 staff cycle spaces are provided in the covered, gated entrance between Building A and B. 14 visitor cycle spaces are provided on the pavement outside the development along Vorley Road, exceeding policy.

Library

11.244 The draft Local Plan and London Plan require 1 long-stay staff cycle space per 8 staff for libraries and 1 visitor space per 100sqm. The proposed library is projected to have up to 8 staff and is 826 sqm. Therefore, the policy requirement is to have 1 staff space and 8 visitor spaces. 1 staff cycle space is provided in the covered, gated entrance between Building A and B and 10 visitor spaces are provided on the pavement outside the development along Vorley Road, exceeding policy.

Cycle Numbers Conclusion

11.245 It is acknowledged that applicant has met, or very nearly met in the case of the residential use, the overall numbers of spaces for long-stay and visitor cycle parking spaces. However, the distribution of spaces across the residential long-stay stores is somewhat imbalanced and also falls short of the number of accessible parking spaces. It is acknowledged that the site is working hard to deliver two high-quality new community facilities on the lower floors, both with rigid space requirements, and that further pressure was also put on the scheme from enhanced fire safety measures adopted during the application in response to HSE which reduced the size of the Building B basement cycle store. The imbalance across the residential stores is accepted on balance owing to these constraints but the cycle parking strategy must meet the minimum policy requirements for total numbers and accessible cycle parking. Therefore, a condition (17) to secure minimum cycle parking details would be attached.

Trip Generation

11.246 The projected trip generation is set out in detail within the Transport Statement dated 21st October 2022 setting out a range of measures to encourage active and sustainable travel.

11.247 TfL reviewed this and stated 'that the approach to trip generation is acceptable and is not considered to result in an unacceptable impact on the highway network. The trip generation predicts 15 and 8 two-way vehicle trips, including servicing, in the AM and PM peaks respectively. On this basis, the number of vehicle trips is unlikely to generate any significant impacts on the road network but on-going monitoring by the LB of Islington is required to ensure that vehicle trips and associated security measures are accommodated safely and efficiently without causing delay to other road users including pedestrians and cyclists. Officers consider that given the excellent PTAL of the location, 6a on a scale of 1 to 6b being the best the largest proportion of trips to be carried out would be via public transport and active travel.' The Transport Statement demonstrates that there would be minimal impact on vehicle trips and sustainable modes would be the overwhelming way to get to and from the site. As an area with excellent public transport connectivity, the public transport network would be comfortably able to absorb the trips generated from the scheme.

Travel Plan

11.248 An outline Travel Plan was submitted by the applicant at appendix G of the Transport Statement, dated 30th March 2022 setting out a range of measures to encourage active and sustainable travel.

11.249 It highlights the wide range of public transport and active travel facilities surrounding the site. TfL stated initially raised concerns that there were no details regarding mode share targets as part of the plan but these have since been included in the updated Travel Plan, dated 21st October 2022 and TfL are now satisfied in this regard. This final travel plan and monitoring would be secured within the Directors' Agreement.

Construction

11.250 An outline Construction Management Plan (CMP) dated 31st March 2022 was submitted with the application that sets out the initial construction programme, vehicles, routing and how construction impacts will be minimised. It relates to both on site activity and the transport arrangements for vehicles servicing the site.

11.251 The CMP has been reviewed by Islington Highways and Traffic who stated that they were happy with the route plan as set out but highlighted a new School Street restriction on Hargrave Park and Bredgar Road and a 7.5tn weight restriction for all streets between Junction Road and Dartmouth Park Hill. These restrictions would need to be borne in mind for the detailed CMP that would be secured by condition (3).

11.252 The CMP was also reviewed by TfL who initially raised concern regarding potential impacts to the bus stand on MacDonald Road. TfL would not support any closure of Vorley Road during construction as this would cause disruption to the existing bus stands and bus routes on MacDonald Road. Following this it is confirmed that full closure of Vorley Road would not occur during the hours of operation of bus services and TfL are now satisfied in this regard.

11.253 A final Construction Logistics Plan is recommended to be secured by condition (3).

Energy and Sustainability

11.254 The NPPF confirms that the purpose of the planning system is to contribute to the achievement of sustainable development, and policies relevant to sustainability are set out throughout the NPPF.

11.255 The Council requires all developments to meet the highest standards of sustainable design and construction and make the fullest contribution to the mitigation of and adaptation to climate change. Developments must demonstrate that they achieve a significant and measurable reduction in carbon dioxide emissions, following the London Plan energy hierarchy. All developments will be expected to demonstrate that energy efficiency has been maximised and that their heating, cooling and power systems have been selected to minimise carbon dioxide emissions.

- 11.256 Islington's Core Strategy policy CS10 (part A) states that all developments should maximise on-site reduction in total (regulated and unregulated) carbon dioxide emissions. The Core Strategy also requires developments to address a number of other sustainability criteria such as climate change adaptation, sustainable transport, sustainable construction and the enhancement of biodiversity.
- 11.257 Development Management Policy DM7.1 requires development proposals to integrate best practice sustainable design standards and states that the council will support the development of renewable energy technologies, subject to meeting wider policy requirements. Details are provided within Islington's Environmental Design SPD, which is underpinned by the Mayor's Sustainable Design and Construction Statement SPG.
- 11.258 Draft Local Plan Policy S3 outlines that all non-residential and mixed-use developments proposing 500sqm or more net additional floorspace are required to achieve a final (post-construction stage) certified rating of Excellent as part of a fully fitted assessment within BREEAM New Construction 2018 (or equivalent scheme), and must make reasonable endeavours to achieve an Outstanding rating. A 'verification stage' certification at post occupancy stage must also be achieved, unless it can be demonstrated that this is not feasible.
- 11.259 An Energy and Sustainability Statement prepared by Etude – dated March 2022 was submitted with the application.

Carbon Emissions

- 11.260 The London Plan sets out a CO2 reduction target, for regulated emissions only, of 40% against Building Regulations 2010 and 35% against Building Regulations 2013. Based on SAP10 carbon factors, a regulated emissions saving of 86.7% against a Part L 2013 baseline is predicted, well exceeding the policy target.
- 11.261 Core Strategy Policy CS10 requires onsite total CO2 reduction targets (regulated and unregulated) against Building Regulations 2010 of 40% where connection to a decentralised energy network is possible, and 30% where not possible. Draft Local Plan Policy S4 adjusts this for Building Regulations 2013 to reductions of 39% where connection to a decentralised energy network is possible, and 27% where not possible. Based on SAP10 carbon factors, a saving of 56% is predicted for the entire development, well exceeding the 27% policy target.
- 11.262 A further breakdown of the unregulated emissions was provided which predicts a saving of 57.9% across all emissions.
- 11.263 The Environmental Design SPD states "after minimising CO2 emissions onsite, developments are required to offset all remaining CO2 emissions (Policy CS10) through a financial contribution". All in this regard means both regulated and unregulated emissions. Based on remaining total emissions of 63 tonnes and the standard offset rate of £920 per tonne, an offset financial contribution of £57,542 would be secured by planning obligation.

Sustainability Design Standards

- 11.264 Development Management Policy DM7.4 states major non-residential developments are required to achieve 'Excellent' under the relevant BREEAM or equivalent scheme and make reasonable endeavours to achieve 'Outstanding'.
- 11.265 The council's Environmental Design Guide states schemes are required to demonstrate that they will achieve the required level of the CSH/BREEAM via a pre-assessment as part of any application and subsequently via certification.

11.266 BREEAM pre-assessments have been provided for both the Library and Medical Centre elements of the development. This indicates that both the non-residential elements are expected to achieve 'Excellent' ratings, with overall scores of 74.10% for the Library and 72.57% for the Medical Centre.

Energy Demand Reduction (Be Lean) and minimising overheating

Energy Reduction Measures & the Cooling Hierarchy

11.267 Development Management Policy DM 7.1 states "Development proposals are required to integrate best practice sustainable design standards (as set out in the Environmental Design SPD), during design, construction and operation of the development."

11.268 Development Management Policy DM7.5 states that "developments are required to demonstrate how the proposed design has maximised incorporation of passive design measures to control heat gain and to deliver passive cooling, following the sequential cooling hierarchy".

11.269 The proposed U-values for the residential areas are walls = 0.14, roof = 0.10, floor = 0.15 & 0.20 and windows = 0.8. An air permeability of 0.6m³/hr/m² is proposed across the development. Mechanical ventilation with heat recovery is proposed for all areas, and this is considered to be an appropriate value.

11.270 Low-energy lighting is proposed throughout the development, with daylight sensors and presence detection controls in the non-residential areas.

11.271 London Plan Policy SI 2 sets a target of 10% reduction for residential development, and 15% for non-residential developments through energy efficiency measures. The residential element achieves energy efficiency savings on regulated emissions of 34.5% and the non-residential element achieves savings of 19.0%, exceeding this policy target.

11.272 Passivhaus standard is sought in the residential elements, which is welcomed.

Dynamic thermal modelling

11.273 Draft Local Plan Policy S6 states that applications for major developments are required to include details of internal temperature modelling under projected increased future summer temperatures to demonstrate that the risk of overheating has been addressed.

11.274 Dynamic thermal modelling using CIBSE TM59 methodology has been carried out for the residential element, via a representative sample of 13 units. All the underlying assumptions used are considered to be reasonable. The results from this show that the vast majority of the rooms tested pass the overheating criteria, with a handful of cases failing. The overheating analysis proposes that this small number of failures is resolved via allowing the windows to open to a greater threshold. As such active cooling is not proposed for the residential element.

11.275 A TM52 overheating assessment was also undertaken for the non-residential element, covering the library and health centre. This showed the non-residential elements to meet the overheating criteria.

Active Cooling

11.276 Council policy states "Use of technologies from lower levels of the hierarchy shall not be supported unless evidence is provided to demonstrate that technologies from higher levels of the hierarchy cannot deliver sufficient heat control".

11.277 Discussion of the cooling hierarchy is provided in the Overheating Assessment and the wider SDCS. This covers approaches including thermal mass and ceiling heights, external shading via balconies, and natural and mechanical ventilation via MVHR.

11.278 Active cooling is not proposed for the residential units but is proposed for the non-residential areas. This is justified for the library owing to limitations on window opening, and for the medical centre, because the subdivision of the space would reduce opportunities for cross-ventilation and potential trapping heat. As such some use of active cooling is accepted, on balance, in this instance. However, it should be ensured that all lower levels of the cooling hierarchy are maximised in these non-residential areas, in order to minimise active cooling demands.

Low Carbon Energy Supply (Be Clean)

Energy (heating and cooling) supply strategy

11.279 In terms of the residential element, heating and hot water to Building A would be provided via individual air source heat pumps. For Buildings B and C, these would be provided via a separate communal air source heat pump system, with heating and hot water delivered to the via HIUs within the individual properties.

11.280 Radiators would be used in the residential properties. The library and medical centre ASHPs are currently envisaged to each have standalone units – but a connection to the residential system is also being considered.

District heating and cooling networks

11.281 Development Management Policy DM7.3 states all major developments within 500 metres of an existing or planned DEN... are required to submit a feasibility assessment of connection to that network, to determine whether connection is reasonably possible.

11.282 The submitted SDCS does not address future-proofing for connection to a heat network. It is confirmed that there is not an existing or committed network within 500m of the site. Therefore, it would not be expected that the development would connect to a network in the short to medium term. However, the proposed communal air source heat pump should be future-proofed for connection, as far as reasonably possible.

Shared energy networks

11.283 Development Management Policy DM7.3 states where connection to an existing or future DEN is not possible, major developments should develop and/or connect to a Shared Heating Network (SHN) linking neighbouring developments and/or existing buildings, unless it can be demonstrated that this is not reasonably possible.”

11.284 Islington’s Energy Team are not aware of feasible opportunities within the immediate area and therefore are not seeking for the development to connect to a shared heat network.

11.285 CHP/CCHP or alternative low carbon on-site plant

11.286 The Council’s Environmental Design Guide (page 12) states “Combined Heat and Power (CHP) should be incorporated wherever technically feasible and viable. Large schemes of 50 units or more, or 10,000sqm floorspace or more, should provide detailed evidence in the form of an hourly heating profile (and details of electrical baseload) where the applicant considers that CHP is not viable; simpler evidence will be accepted on smaller schemes.”

11.287 On-site CHP is not proposed, on grounds of carbon factors and air quality, among other reasons. Given this, and the current GLA position on CHP, this is accepted.

Renewable Energy Supply (Be Green)

11.288 The Mayor’s Sustainable Design & Construction SPD states although the final element of the Mayor’s energy hierarchy, major developments should make a further reduction in their carbon

dioxide emissions through the incorporation of renewable energy technologies to minimise overall carbon dioxide emissions, where feasible.

11.289 The Council's Environmental Design SPD (page 12) states the use of renewable energy should be maximised to enable achievement of relevant CO2 reduction targets.

11.290 Air source heat pumps have been discussed above. Solar PV is also proposed for the development. This is to be spread across two principal arrays, with a total output of 106kWp, and cover a large portion of the available roof area, which is supported.

Draft Green Performance Plan

11.291 Development Management Policy DM7.1 states "applications for major developments are required to include a Green Performance Plan (GPP) detailing measurable outputs for the occupied building, particularly for energy consumption, CO2 emissions and water use, and should set out arrangements for monitoring the progress of the plan over the first years of occupancy." The council's Environmental Design SPD provides detailed guidance and a contents check-list for a Green Performance Plan.

11.292 A draft Green Performance Plan has been submitted and Islington Energy team are content with the information provided. It is recommended that the GPP is updated to also include targets/measures of performance for covering renewable technologies – such as kWh consumption, heat output and CoP for the heat pump systems, and kWh output for the solar PV. The applicant has stated that these updates would be made at the next stage of the GPP process which is considered acceptable.

11.293 The submitted Energy Statement is recommended to be secured by condition (23).

Whole Life-Cycle Carbon

11.294 London Plan Policy SI2 states that 'all major development proposals must calculate whole life-cycle carbon emissions through a nationally recognised whole life-cycle carbon assessment and demonstrate actions taken to reduce life-cycle carbon emissions.'

11.295 Draft Local Plan policy S4 states that 'all major development proposals must calculate whole life-cycle carbon emissions through a nationally recognised whole life-cycle carbon assessment and demonstrate actions taken to reduce life-cycle carbon emissions (WLC).'

11.296 The applicant submitted a Sustainable Design and Construction Statement, by Etude dated March 2022 which includes a WLC section. The GLA reviewed this and note that it covers much of the assessment requirements including structure, façade, services, landscaping and low carbon options, however an Excel version to the GLA WLC template must also be submitted to allow a full review to be completed against the relevant GLA guidance. The applicant is required to submit a WLC assessment template in full. This is important to allow results to be recorded and tracked through to the post-construction stages, and to allow a proper review of the results against material quantities and other assumptions made. Final WLC details would be secured by pre-commencement condition (8) in addition to a WLC post-construction report (33) to ensure that CO2 emission reduction targets by energy efficient measures/features and renewable energy are met.

Circular Economy

11.297 London Plan Policy SI7 states that 'referable applications should promote circular economy outcomes and aim to be net zero-waste. A Circular Economy Statement should be submitted, to demonstrate:

- 1) how all materials arising from demolition and remediation works will be re-used and/or recycled
- 2) how the proposal's design and construction will reduce material demands and enable building materials, components and products to be disassembled and re-used at the end of their useful life
- 3) opportunities for managing as much waste as possible on site
- 4) adequate and easily accessible storage space and collection systems to support recycling and re-use
- 5) how much waste the proposal is expected to generate, and how and where the waste will be managed in accordance with the waste hierarchy 6) how performance will be monitored and reported.

11.298 Draft Local Plan Policy S2 states that all development proposals are required to submit a Sustainable Design and Construction Statement which must demonstrate that the proposal meets all relevant sustainable design policies. The Sustainable Design and Construction Statement must show how sustainable design has been considered holistically from the start of the design process and is integrated throughout the construction and operation of the development.

11.299 The applicant has submitted a Circular Economy Statement (CES) dated September 2021.

11.300 The GLA reviewed the CES and noted that it refers to previous guidance version (Draft for Consultation (October 2020)) rather than the adopted London Plan Guidance: Circular Economy Statements (2022). They noted that there is significantly more information and exploration required from the applicant with respect to the Circular Economy. The outstanding information includes the reporting of key metrics and commitments to London Plan policy targets. The applicant is required to provide a more in-depth exploration of the circular economy measures to be implemented with specific reference to the principles of the Circular Economy. Final CE details would be secured by pre-commencement condition (9) in addition to a CE post-construction report (34) to ensure that CO2 emission reduction targets by energy efficient measures/features and renewable energy are met.

Energy & Sustainability Conclusion

11.301 The passivhaus principles and high standard of energy efficiency of the scheme are exemplary. Subject to conditions, the energy and sustainability aspects of the scheme are considered acceptable.

Waste Management

11.302 Islington's Core Strategy Policy CS11 states that sufficient waste storage facilities should be provided in order to fit current and future collection practices and targets. Facilities must be accessible to all in accordance with.

11.303 Development Management Policy DM8.6 states that, for major developments, details of refuse and recycling collection must be submitted, indicating locations for collection vehicles to wait and locations of refuse and recycling bin stores.

11.304 Draft Local Plan Policy ST2 states that development proposals must provide waste and recycling facilities which: (i) fit current and future collection practices and targets; (ii) are accessible to all; (iii) are designed to provide convenient access for all people, helping to support people to recycle; and (iv) provide high quality storage and collection systems in line with Council guidance.

11.305 Vehicular collection is proposed from communal refuse stores along Vorley Road. The bin stores have been grouped so that there would be two locations where collection can be made for all 4 bin stores.

- 11.306 The library bin store would be located to the right of the shared gated entrance to building A and B. A bulky waste store and bin store for Buildings A and B would be located to the right of this entrance.
- 11.307 The medical centre bin store would be located to the left of Building C's entrance. The bin store for Building C would be located to the right of this entrance.
- 11.308 The number of bins provided would be in accordance Islington's Waste Guidelines. The split of refuse and recycling bins would be 50/50. Each bin store would contain clearly marked refuse and recycling wheelie bins. The quantity of waste capacity for the medical centre was specified by the health care planner.
- 11.309 The proposed flats would have built-in recycling and waste storage areas.
- 11.310 The location and provision for refuse and recycling is considered acceptable and it should be in place prior to occupation of the development. This would be secured by condition (37).

Biodiversity

- 11.311 Chapter 15 of the NPPF states opportunities to improve biodiversity in and around developments should be integrated as part of their design, especially where this can secure measurable net gains for biodiversity or enhance public access to nature where this is appropriate.
- 11.312 London Plan Policy G6 states that development proposals should manage impacts on biodiversity and aim to secure net biodiversity gain. This should be informed by the best available ecological information and addressed from the start of the development process.
- 11.313 Core Strategy Policy CS15 outlines the requirement to protect and enhance biodiversity across the borough and address deficiencies in access to nature.
- 11.314 Development Management Policies Policy DM6.5 states that developments must protect, contribute to and enhance the landscape, biodiversity value and growing conditions of the development site and surrounding area, including protecting connectivity between habitats. Developments are required to maximise the provision of soft landscaping, including trees, shrubs and other vegetation, and maximise biodiversity benefits, including through the incorporation of wildlife habitats that complement surrounding habitats and support the council's Biodiversity Action Plan.
- 11.315 Emerging Local Plan Policy G4 states that all developments must protect, enhance and contribute to the landscape, biodiversity value and growing conditions of the development site and surrounding area, including protecting and enhancing connectivity between habitats.
- 11.316 An Ecological Impact Assessment prepared by AGB Environmental Ltd. - dated 27th April 2022 was submitted with the application.
- 11.317 This report identifies that the existing site contains hardstanding, scattered trees and tall ruderal vegetation. The site is situated within an urban location and surrounded by buildings to the north, south and east, and parkland to the west. The wider area comprises urban settlement, transport infrastructure, urban green spaces and waterbodies.
- 11.318 The site was considered to be of negligible importance due to the management of its habitats, high levels of disturbance, poor plant diversity and the extensive use of artificial lighting.
- 11.319 The report sets out 'General Avoidance and Mitigation Measures' for protecting and preserving biodiversity on site to comply with national and local planning policy, current legislation and best practice.

- 11.320 In terms of potential bat roosts, a precautionary approach using 'soft' felling techniques would be used when it comes to removing the two trees on site. If any bats are discovered at any point, then all work must immediately cease until a suitably experienced ecologist has been contacted for advice and works within the vicinity should be halted.
- 11.321 In terms of nesting birds, Woody vegetation and buildings would be cleared between September and February (inclusive) to avoid the main breeding bird season. Alternatively, an ecologist will check potential nesting habitat immediately before clearance if it is scheduled during the main breeding season (March to August inclusive). Any active nests identified will be retained in situ with a suitable buffer until the ecologist has confirmed that the chicks have fledged and the nest is no longer active.
- 11.322 In terms of retained trees, these would be protected in line with best practice. In terms of nocturnal animals, any trenches, holes or deep pits would be covered before nightfall, or secured planks would be used to allow any animals that fall in to escape during construction or leave a sloping end in the trench.
- 11.323 The report stated that biodiversity net-gain would be achieved with green roofs, general use bird boxes, sparrow terraces for house sparrow and planting a diverse range of native plant species including small trees and shrubs.
- 11.324 The Ecological Impact Assessment was reviewed by Islington's Nature Conservation Team who agree with the report's recommendations for biodiversity enhancements but request swift boxes instead of sparrow terraces. They also request a lighting scheme that is sensitive to ecology is also put in place.
- 11.325 Conditions (40 & 13) are recommended to be attached requiring that the above recommendations are carried out to ensure any on-site biodiversity is enhanced and requesting further details of bird boxes/swift bricks.
- 11.326 Islington's Sustainability team stated that the green roof green roofs should be wildflower based with no more than a maximum of 25% sedum.
- 11.327 A condition would also be attached requiring that details of green roof are submitted to and approved by the Local Planning Authority alongside a maintenance plan. This is to ensure that the green roof will be of good quality, will make a meaningful contribution to on-site biodiversity and greening and will be maintained in the future.

Protection of Trees

- 11.328 Development Management Policies Policy DM6.5 states that developments are required to minimise any impacts on trees, shrubs and other significant vegetation. Any loss of or damage to trees, or adverse effects on their growing conditions, will only be permitted where there are over-riding planning benefits, must be agreed with the council and suitably re-provided.
- 11.329 Emerging Local Plan Policy G4 states any loss of or damage to trees or other significant planting, or adverse effects on their growing conditions or survival, will only be permitted where it is demonstrably unavoidable in order to meet other relevant Local Plan policy requirements (as agreed with the Council). In such circumstances, suitable high-quality re-provision of equal value must be provided on-site. Where on-site re-provision is demonstrably not possible (as agreed with the Council), a financial contribution of the full cost of appropriate re-provision will be required.
- 11.330 There are 14 ash trees (G1) running along the southern boundary of the site within the highway, a further good quality sycamore (T1) is located just outside the northern boundary, a poor-quality ash (T2) is also just outside the northern boundary and an ornamental plum (T3) located at the eastern end of the site. None of the trees on, or adjacent to the site are protected by a Tree Preservation Order (TPO) or are located within a Conservation Area.

11.331 The sycamore (T1) is Category B1 and would be retained. The ash (T2) is Category U and is recommended for removal with the consent of the landowner as it is poor quality and in an awkward location up against the site boundary. The ornamental plum is Category C1 and would also be removed to allow Block C to be built.

11.332 Two of the ash trees in G1 would be removed (G1ii and G1iv) due to their proximity to the proposed development. Islington Tree Protection Officer was consulted on the scheme and would not be considered to have a significant impact on the visual amenity of the road due to the remaining 12 ash trees to be retained. However, concern was raised that the proximity of the Building A façade could result in post-development pressure to excessively prune or remove trees to the detriment of public amenity and eco-system service provision. A condition (5) is recommended securing the Tree Protection Plan and Arboricultural Method Statement and would incorporate the requirement to ensure the ongoing retention and appropriate pruning of the 12 ash trees to ensure no undue detriment to visual amenity or biodiversity.

11.333 Three new trees are proposed, one in the green lobby area and two in the medical centre garden. This would result in an uplift in trees and canopy cover over the existing situation.

Urban Greening Factor

11.334 London Plan Policy G5 sets out the expectation for major development proposals to contribute to the greening of London by including urban greening as a fundamental element of site and building design, and by incorporating measures such as high-quality landscaping (including trees), green roofs, green walls and nature-based sustainable drainage. Boroughs should develop an Urban Greening Factor (UGF) to identify the appropriate amount of urban greening required in new developments.

11.335 The UGF should be based on the factors set out in Table 8.2 of the London Plan, but tailored to local circumstances. In the interim, the Mayor recommends a target score of 0.4 for developments that are predominately residential, and a target score of 0.3 for predominately commercial development (excluding B2 and B8 uses).

11.336 Emerging Local Plan Policy G1 requires that Islington major developments are required to conduct an Urban Greening Factor (UGF) assessment in accordance with the methodology in the London Plan. Schemes must achieve an UGF score of 0.4 for developments that are predominately residential, and a target score of 0.3 for predominately commercial development.

11.337 Through the measures set out in the Biodiversity and Trees sections above the proposal would achieve a UGF score of 0.56 well exceeding the policy target.

Air Quality

11.338 NPPF Chapter 15 requires that planning decisions should ensure that any new development in Air Quality Management Areas and Clean Air Zones is consistent with the local air quality action plan.

11.339 In accordance with Development Management Policies Policy DM6.1 developments in locations of poor air quality should be designed to mitigate the impact of poor air quality to within acceptable limits.

11.340 Emerging Local Plan Policy S1: Delivering Sustainable Design states that all new development must be designed, constructed and operated to limit contribution to air pollution and to improve air quality as far as possible, as well as reducing exposure to poor air quality, especially among vulnerable people.

11.341 The whole of the borough of Islington has been designated by the council as an Air Quality Management Area.

- 11.342 An Air Quality Assessment prepared by Air Quality Consultants - dated 16th September 2021 was submitted with the application.
- 11.343 In terms of construction works, which have the potential to generate dust, a suit of mitigation measures will need to be applied to minimise dust emissions. Details of this are recommended to be secured by condition (4) requesting a detailed Demolition and Construction Management Plan (DCMP) assessing the environmental impacts (including in relation to air quality, dust, smoke and odour).
- 11.344 In terms of the impact on future occupants, the assessment determined that pollutant concentrations at the application site would be below the relevant air quality objectives, thus future occupants would experience acceptable air quality. PM2.5 concentrations would exceed the WHO guidelines, however this is common throughout Greater London. All residents would be provided with fresh air from mechanical ventilation with heat recovery units (MVHR) units.
- 11.345 The additional traffic generated by the proposed development will be below published screening thresholds, and heating and hot water demand will be provided by ASHPs and PVs. Therefore, the operational effects of the proposed development on existing sensitive receptors will be insignificant.
- 11.346 In terms of operational impacts, the assessment finds that the additional traffic generated by the proposed development would be below published screening thresholds, and heating and hot water demand would be provided by ASHPs and PVs. Therefore, the operational effects of the proposed development on existing sensitive receptors will be insignificant.
- 11.347 With low building and transport related emissions it would comply with the requirement that all new developments in London should be at least air quality neutral.
- 11.348 Further, emissions from non-road mobile machinery would also need to be addressed in submissions made pursuant to condition (41).

Fire Safety

- 11.349 London Plan Policy D12 states that 'all major development proposals should be submitted with a Fire Statement, which is an independent fire strategy, produced by a third party, suitably qualified assessor.

The statement should detail how the development proposal will function in terms of:

- 1) the building's construction: methods, products and materials used, including manufacturers' details
- 2) the means of escape for all building users: suitably designed stair cores, escape for building users who are disabled or require level access, and associated evacuation strategy approach
- 3) features which reduce the risk to life: fire alarm systems, passive and active fire safety measures and associated management and maintenance plans
- 4) access for fire service personnel and equipment: how this will be achieved in an evacuation situation, water supplies, provision and positioning of equipment, firefighting lifts, stairs and lobbies, any fire suppression and smoke ventilation systems proposed, and the ongoing maintenance and monitoring of these
- 5) how provision will be made within the curtilage of the site to enable fire appliances to gain access to the building
- 6) ensuring that any potential future modifications to the building will take into account and not compromise the base build fire safety provisions and measures.

11.350 A Fire Strategy Report was submitted with the application, by Affinity Fire Engineering, dated 4th April 2022.

11.351 Islington Building Control, HSE and the London Fire Brigade were consulted on the proposal. The London Fire Brigade had 'no further observations to make'. The HSE initially objected to the scheme raising concerns relating to the following:

- there being a single staircase and lift in Building B serving dwellings which both descended to the basement;
- the single staircores in Building B and C being connected with areas of higher fire risk or other non-residential occupancies;
- a basement smoke vent directly below a window serving the Building B staircase;
- a basement smoke vent embedded within the building
- The locations of the dry rising main inlets and outlets and use of horizontal fire mains

11.352 In response to the concerns from HSE, and questions raised by Islington Building Control, a revised Fire Strategy Report dated 7th September 2022 and updated plans were submitted.

11.353 Principally, the latest plans show that a separate lift and staircase is now provided to serve the basement in Building B and the staircore of Building B and Building C would now be separated from areas of higher fire risk. The roof level pump room on Building B would be accessed via the roof terrace and there is no direct connection from the stair to the pump room. The staircore of Building C would be separated from the buggy store and bike store which are now accessed via an external area that is open to air and the intake room would also lobbied from the external undercroft that serves Core C.

11.354 Following revisions to the plans and Fire Strategy Report HSE confirmed that they were satisfied with the information provided with the application.

11.355 It is noted that the author of the submitted Fire Statement is a qualified person with expertise in fire safety and engineered solutions, and as such, the applicant has used the relevant expertise to consider the fire safety of the development as part of the overall scheme.

11.356 Planning and Building Control Officers have undertaken a review of the Fire Statement in accordance with the requirements of policy D12(B). The submitted information is specific and relevant to the proposal and the Fire Statement references BS 9999. For the purposes of compliance with Policy D12 (B) it is considered that sufficient information has been provided to demonstrate that the fire safety of the development has been considered comprehensively and address all six criteria set out in this policy. Relating to part 5), how provision will be made within the curtilage of the site to enable fire appliances to gain access to the building, it is demonstrated how fire vehicles would have appropriate access via Vorley Road. Should there be a need for further alterations of the building or Fire Strategy Report to meet building regulations the applicant would need to make further applications to amend this current proposal. A condition is recommended (35) to ensure that the development should only be occupied and managed in accordance with the submitted Fire Strategy Report.

Crime Prevention

11.357 London Plan Policy D11 state that 'boroughs should work with their local Metropolitan Police Service 'Design Out Crime' officers and planning teams, whilst also working with other agencies such as the London Fire Commissioner, the City of London Police and the British Transport Police to identify the community safety needs, policies and sites required for their area to support provision

of necessary infrastructure to maintain a safe and secure environment and reduce the fear of crime.'

- 11.358 The supporting text of Development Management Policy DPD Policy DM2.1 states at paragraph 2.8 that 'developments must meet the principles set out in Safer Places (2004) and Secured by Design through consultation with Islington's crime prevention design advisor.'
- 11.359 Paragraph 1.41 within Objective 3 of the draft Local Plan states that 'designing out crime is a key planning principle, which incorporates a number of design techniques to limit incidences of crime; this includes increases in natural surveillance and designing space, so it is conducive to positive behaviour.'
- 11.360 Secured by Design principles have been considered throughout the design development. Discussions with the Metropolitan Police in Designing Out Crime Group took place on 8th July 2021 to discuss and review the scheme.
- 11.361 The new residential cores and ground floor uses would be accessed from the public areas along the Vorley Road street frontage. The design of the 'plinth' uses is specifically designed to overlook the street and park beyond. As such the development would increase street activity and promote passive surveillance along the Vorley Road frontage. All homes would be dual or treble aspect and provide good surveillance throughout. There would be no blank flank walls without windows mitigating the risk of vandalism or other anti-social behaviour.
- 11.362 The perimeter public areas would be well-lit and appropriately sign posted. Banks of planting would be utilised to dissuade people from approaching the façades at ground floor level. The walls and railings, planting and tactile barriers of materials would all provide physical protection to the private defensible spaces that face on to the public areas. The selection of building materials for accessible parts of all buildings have been chosen with resistance to graffiti in mind, and ease of maintenance.
- 11.363 Post boxes would be located behind access-controlled gates, jointly for cores A&B and then for core C. This would ensure delivery personnel won't require access to the lobbies of each individual core.
- 11.364 Secure doors and access control would be provided throughout including enclosed lockable bin stores within the footprint of the building.
- 11.365 The proposal was reviewed by a Metropolitan Police Designing Out Crime Officer who recommended a range of security measures: including compartmentalisation, secure lobbies, audio visual control access points, Gerder boxes, security rated door sets, use of self-closing and self-locking bins, spyholes for front doors, specification standards for communal doors and windows. They requested a condition (18) be attached to ensure the proposal gains Secure by Design Certification.
- 11.366 As such the proposal is considered to have been designed in accordance Secure by Design principles and would enhance safety and security in the area.

Flood Risk and Sustainable Drainage

- 11.367 Policy DM6.6 is concerned with flood prevention and requires that schemes must be designed to reduce surface water runoff to a 'greenfield rate' (8 litre/sec/ha), where feasible.
- 11.368 The London Plan requires that drainage run offs in new developments be reduced by 50% including an allowance for climate change.
- 11.369 A Flood Risk Assessment & Drainage Strategy (FRAD) Rev. C was submitted with the application, by Calford Seadon dated 30th March 2022. The proposed development is in Flood Zone 1 and therefore at low risk of flooding. It is proposed to use a combination of sustainable drainage (SuDS)

measures to control surface water runoff, including pervious pavements and sub-surface storage tanks. Islington Sustainability Officer requested the inclusion of blue roofs which have since been incorporated above the Block A cycle store in an updated Flood Risk Assessment & Drainage Statement Rev. D, dated 23rd August 2022.

11.370 The above FRAD and details on the blue roofs are recommended to be secured by conditions (45 & 20).

Land Contamination

11.371 Development Management Policy DM6.1 states that the council will require adequate treatment of any contaminated land before development can commence.

11.372 Emerging Local Plan Policy S9 states the development of land affected by contamination must not create unacceptable risks to human health and the wider environment, including local water resources. Assessment and adequate treatment of any contaminated land must be carried out before any development commences on site.

11.373 A Phase 1 Contaminated Land Assessment prepared by Lustre Consulting Ltd. – dated 22nd March 2022 was submitted with the application. This found that it is possible that contamination is present on site in a circumstance which could lead to unacceptable risks. As such a full investigation is required and likely a remediation strategy. This was reviewed by Islington's Environmental Protection team who recommended a condition (6) to be attached securing this.

Affordable Housing and Financial Viability

11.374 London Plan Policy H4 identifies a strategic target for 50% of all new homes to be built as genuinely affordable homes. The policy notes that on public sector land, as is the case here, proposals should deliver at least 50% affordable housing.

11.375 Islington Core Strategy (2011) Policy CS 12 sets out that the Council will seek the: "maximum reasonable amount of affordable housing, especially social rented housing..... taking into account the overall borough wide strategic target. It is expected that many sites will deliver at least 50% of units as affordable subject to a financial viability assessment, the availability of public subsidy and individual circumstances of the site."

11.376 This policy then further seeks to increase delivery of affordable housing from other sources, such as 100% affordable housing schemes by Registered Providers and building affordable homes on the council's own land.

11.377 Policy H3 of the Draft Islington Local Plan requires that "a minimum of 50% of the total net additional conventional housing built in the borough over the plan period must be genuinely affordable". With regard to land which is currently or has been in public sector ownership, as is the case here, the development must provide 50% on-site affordable housing (by net additional unit) without public subsidy and demonstrate how all public subsidy options for maximising the delivery of on-site affordable housing in excess of 50% (by net additional unit) have been utilised, and demonstrate additional delivered using any and all forms of public subsidy.

11.378 Affordable Housing Officer: The proposal would provide: 37 social rent dwellings (124 habitable rooms) and 35 private dwellings (89 habitable rooms), which equates to 51% social rented dwellings and 49% private dwellings. In terms of habitable rooms, there will be 124 habitable rooms for social rent (58%) and 89 habitable rooms for private sale (42%).

11.379 Within an affordable housing provision, the existing and draft Local Plan policy is a requirement to provide 70% of the affordable units as social rented and 30% of the units as intermediate/shared ownership. While the proposal does not include any intermediate/shared ownership units, it is considered that the delivery of 51% of the units in social rent makes a significant contribution to the

delivery of genuinely affordable housing in the borough and addressed the Planning Policy which encourages the delivery of social rented units.

- 11.380 Noting that the proposal would not provide the aspiration of 100% affordable housing on Council own land as set out in Core Strategy Policy CS12 and the requirements of Draft Local Plan Policy H3, which requires applicants to demonstrate how delivery of affordable housing above 50% has been maximised on site, the applicant has submitted a financial viability assessment.
- 11.381 In order to properly and thoroughly assesses the Financial Viability Assessment, the Council engaged an independent viability assessor, BPS Chartered Surveyors, to undertake a detailed review of the submission.
- 11.382 While the submitted Financial Viability Assessment demonstrated that the scheme would have a deficit, and therefore further affordable housing provision could not be delivered on site, the assessment did not include standardised commercial inputs and assessments.
- 11.383 Noting this, the Council's appointed Financial Viability Assessor undertook a detailed review of the proposals and updated the assumptions/inputs to accord with relevant policy and guidance. This assessment determined that the proposal would have a deficit of £2.8 million and therefore it was not commercially viable, and therefore cannot deliver any additional affordable housing.
- 11.384 It should also be noted that the proposal includes the delivery of both a library and a medical centre, which, as set out above, represent benefits of the proposal but also incur cost within the viability assessment.
- 11.385 As such, it is considered that the proposal has demonstrated that the proposed affordable housing offer, which is inclusive of grant funding, is the maximum viable at the site. The proposal therefore meets Islington Core Strategy Policy CS12, Draft Local Plan Policy H3 and London Plan Policy H4.

Planning Obligations, Community Infrastructure Levy and local finance considerations

- 11.386 Part 11 of the Community Infrastructure Levy (CIL) Regulations 2010 introduced the requirement that planning obligations under Section 106 must meet 3 statutory tests, i.e. that they are (i) necessary to make the development acceptable in planning terms, (ii) directly related to the development, and (iii) fairly and reasonably related in scale and kind to the development. Under the terms of the Planning Act 2008 (as amended) and Community Infrastructure Levy Regulations 2010 (as amended), the Mayor of London's and Islington's Community Infrastructure Levy (CIL) would be chargeable on the proposed development on grant of planning permission. This is calculated in accordance with the Mayor's adopted Community Infrastructure Levy Charging Schedule 2012 and the Islington adopted Community Infrastructure Levy Charging Schedule 2014.
- 11.387 A Section 106 legal agreement including relevant Heads of Terms would be necessary in order to mitigate the impacts of the proposed development. The necessary Heads of Terms are:
- On-site provision of affordable housing: 37 social rented units
 - A financial contribution of £260,000 for Girdlestone Park play space upgrades.
 - Construction Management Plan and Monitoring: in adherence to the Council's Code of Practice for Construction Sites, the provision of a Construction Management Plan, and a monitoring contribution of £17,400 (calculated at 72 residential units x £200, and 1561sqm of community floorspace – rounded to 1500sqm – calculated at 15 x £200)
 - Compliance with the Code of Employment and Training.
 - Compliance with the Code of Local Procurement.

- Compliance with the Code of Construction Practice, including a monitoring fee TBC and submission of site-specific response document to the Code of Construction Practice for approval of LBI Public Protection.
- The provision of 7 accessible parking bays or a contribution of £14,000 (£2,000 cost per bay) towards bays or other accessible transport initiatives.
- Highways Reinstatement: agreement from the developer to cover the cost of reinstating any highway damaged by the developer during the construction works
- Carbon Offset: A financial contribution of £57,542 towards offsetting any projected residual CO2 emissions of the development (63 tonnes and the standard offset rate of £920 per tonne)
- Green Performance Plan: to be submitted and approved prior to the proposed use being occupied
- Car free development- Removal of eligibility for residents' parking permits.
- Submission of draft full Travel Plan for Council approval prior to occupation, and of a full Travel Plan for Council approval 6 months from first occupation of the development or phase.
- Council's legal fees in preparing the Directors' agreement and officer's fees for the preparation, monitoring and implementation of the Directors agreement.
- All payments to the Council would be index-linked from the date of Committee and would be due upon implementation of the planning permission.

12. SUMMARY AND CONCLUSION

- 12.1 The proposed new library, medical centre and housing would deliver on the draft Site Allocation for a proposal providing a residential-led development with community uses that includes a tall building forming part of an Archway cluster of tall buildings. The scale, form, materiality, elevation design and layout is well-considered and results in a scheme of high quality design that is sensitive to the local and wider context. The community uses would create a civic hub on Vorley Road that would support the vitality of Archway Town Centre. Although there would be some amenity and heritage impacts these are limited and must be weighed against the considerable public benefits, which in this case comprise the delivery of over 50% of onsite affordable housing, contribution of high-quality housing, the delivery of two new community uses and the delivery of an exemplary scheme with regard to sustainability and energy efficiency.
- 12.2 The proposal is considered acceptable in planning terms and it is recommended that planning permission be granted subject to conditions and completion of a legal agreement securing relevant planning obligations.
- 12.3 It is recommended that planning permission be granted subject to conditions and s106 legal agreement heads of terms for the reasons and details as set out in Appendix 1 - RECOMMENDATIONS.

APPENDIX 1 – RECOMMENDATIONS

RECOMMENDATION A

That planning permission be granted subject to the prior completion of a Deed of Planning Obligation made under section 106 of the Town and Country Planning Act 1990 between the Council and all persons with an interest in the land (including mortgagees) in order to secure the following planning obligations to the satisfaction of the Head of Law and Public Services and the Service Director, Planning and Development / Head of Service – Development Management or, in their absence, the Deputy Head of Service:

- On-site provision of affordable housing: 37 social rented units
- A financial contribution of £260,000 for Girdlestone Park play space upgrades.
- Construction Management Plan and Monitoring: in adherence to the Council's Code of Practice for Construction Sites, the provision of a Construction Management Plan, and a monitoring contribution of £17,400 (calculated at 72 residential units x £200, and 1561sqm of community floorspace – rounded to 1500sqm – calculated at 15 x £200)
- Compliance with the Code of Employment and Training.
- Compliance with the Code of Local Procurement.
- Compliance with the Code of Construction Practice, including a monitoring fee TBC and submission of site-specific response document to the Code of Construction Practice for approval of LBI Public Protection.
- The provision of 7 accessible parking bays or a contribution of £14,000 (£2,000 cost per bay) towards bays or other accessible transport initiatives.
- Highways Reinstatement: agreement from the developer to cover the cost of reinstating any highway damaged by the developer during the construction works
- Carbon Offset: A financial contribution of £57,542 towards offsetting any projected residual CO2 emissions of the development (63 tonnes and the standard offset rate of £920 per tonne)
- Green Performance Plan: to be submitted and approved prior to the proposed use being occupied
- Car free development- Removal of eligibility for residents' parking permits.
- Submission of draft full Travel Plan for Council approval prior to occupation, and of a full Travel Plan for Council approval 6 months from first occupation of the development or phase.
- Council's legal fees in preparing the Directors' agreement and officer's fees for the preparation, monitoring and implementation of the Directors agreement.
- All payments to the Council would be index-linked from the date of Committee and would be due upon implementation of the planning permission.

That, should the Section 106 Deed of Planning Obligation not be completed within 13 weeks from the date when the application was made valid or within the agreed extension of time, the Service Director, Planning and Development / Head of Service – Development Management or, in their absence, the Deputy Head of Service may refuse the application on the grounds that the proposed development, in the absence of a Deed of Planning Obligation is not acceptable in planning terms.

ALTERNATIVELY, should this application be refused (including refusals on the direction of The Secretary of State or The Mayor) and appealed to the Secretary of State, the Service Director, Planning and Development / Head of Service – Development Management or, in their absence, the Deputy Head of Service be authorised to enter into a Deed of Planning Obligation under section 106 of the Town and Country Planning Act 1990 to secure to the heads of terms as set out in this report to Committee.

RECOMMENDATION B

That the grant of planning permission be subject to **conditions** to secure the following, and that there is delegated to each of the following: the Head of Development Management the Team Leader Major Applications and the Team Leader Planning Applications to make minor changes (additions removals or amendments) to the conditions:

1	Commencement
	<p>CONDITION: The development hereby permitted shall be begun not later than the expiration of three years from the date of this permission.</p> <p>REASON: To comply with the provisions of Section 91(1)(a) of the Town and Country Planning Act 1990 as amended by the Planning and Compulsory Purchase Act 2004 (Chapter 5)</p>
2	Approved plans and documents list
	<p>CONDITION: The development hereby approved shall be carried out in accordance with the following approved plans and documents:</p> <p><u>Plans:</u></p> <p><i>Existing:</i></p> <p>3468A-LB-XX-00-DP-A-1010-2 – Site Location Plan 3468A-LB-XX-00-DP-A-1020-1 – Existing Site Plan 3468A-LB-ZZ-XX-DE-A-1040-1 – Existing Site Elevations 3468A-LB-ZZ-XX-DE-A-1041-1 – Existing South Elevation 3468A-LB-ZZ-XX-DE-A-1042-1 – Existing North Elevation 3468A-LB-BB-XX-DS-A-1050-1 – Existing Sections</p> <p><i>Proposed:</i></p> <p>3468A-LB-ZZ-00-DP-A-1600-1 – Demolition Plan 3468A-LB-ZZ-G1-DP-A-1099-17 – Layout Basement 3468A-LB-ZZ-00-DP-A-1100-25 – Layout GF 3468A-LB-ZZ-01-DP-A-1101-18 - Layout Level 01 3468A-LB-ZZ-02-DP-A-1102-17 - Layout Level 02 3468A-LB-ZZ-03-DP-A-1103-16 - Layout Level 03 3468A-LB-ZZ-04-DP-A-1104-15 - Layout Level 04 3468A-LB-ZZ-05-DP-A-1105-15 - Layout Level 05 3468A-LB-ZZ-06-DP-A-1106-15 - Layout Level 06 3468A-LB-ZZ-07-DP-A-1107-15 - Layout Level 07 3468A-LB-ZZ-08-DP-A-1108-15 - Layout Level 08 3468A-LB-ZZ-09-DP-A-1109-15 - Layout Level 09 3468A-LB-ZZ-10-DP-A-1110-15 - Layout Level 10 3468A-LB-ZZ-11-DP-A-1111-15 - Layout Level 11 3468A-LB-ZZ-12-DP-A-1112-15 - Layout Level 12 3468A-LB-ZZ-13-DP-A-1113-17 - Layout Level 13 3468A-LB-ZZ-14-DP-A-1114-5 - Layout Roof Plan 3468A-LB-ZZ-XX-DE-A-1300-15 – North Elevation 3468A-LB-ZZ-XX-DE-A-1301-15 – South Elevation 3468A-LB-ZZ-XX-DE-A-1302-15 – East Elevation</p>

3468A-LB-BA-XX-DS-A-1400-14 – Section A
3468A-LB-BB-XX-DS-A-1401-14 – Section B
3468A-LB-BC-XX-DS-A-1402-14 – Section C
3468A-LB-BB-XX-DS-A-1404-11 – Site Sections
3468A-LB-ZZ-XX-DE-A-1405-10 – Site Elevations
3468A-LB-BA-XX-DS-A-1406-7 – Section A East Elevation
3468A-LB-BB-XX-DS-A-1407-7 – Section B West Elevation
3468A-LB-BC-XX-DS-A-1408-7 – Section C West Elevation
3468A-LB-ZZ-XX-DS-A-1409-7 – Section C East Elevation
3468A-LB-ZZ-ZZ-DS-A-1450-1 – Covered Entrance Section West
3468A-LB-ZZ-ZZ-DS-A-1451-1 – Covered Entrance Section East
3468A-LB-ZZ-ZZ-DE-A-3310-3 - A&B Gated Entrance
3468A-LB-ZZ-ZZ-DE-A-350020-2 – Library Façade Bay Study
3468A-LB-BC-ZZ-DE-A-350030-2 – Medical Centre bay Study
3468A-LB-ZZ-ZZ-DD-A-3520-3 – Typical Window Detail
3468A-LB-XX-ZZ-DD-A-3530-2 – Typical Parapet Details
3468A-LB-XX-XX-DT-A-3540-1 – Louvre Types
3468A-LB-XX-00-DD-A-3541-1 - Entrance gate & Bin Store Detail
3468A-LB-ZZ-ZZ-DD-A-3800-3 – Balcony Detail
3468-LB-ZZ-XX-SA-A-5001-7 – Tenure Plans
3468A-LB-ZZ-XX-DR-L-6000 – Landscape Hardwork Plan
3468A-LB-ZZ-XX-DR-L-6002 – Landscape Softwork Plan

Documents:

Application and CIL Forms, HTA Design, dated 04/04/2022
Design & Access Statement, Levitt Bernstein, dated 29/03/22
Planning Statement including Affordable Housing Statement and Health Impact Assessment, HTA Design, dated 04/04/2022
Statement of Community Involvement; HTA Design, dated 04/04/2022
Townscape Heritage and Visual Impact Assessment, KMHeritage, dated 03/2022
Ecological Impact Assessment, P4106.2.2, agb Environmental P4106.2.2, dated 27/04/2022
Biodiversity Net Gain Report, agb Environmental, P4106.3.3, dated 13/07/2022
Daylight and Sunlight Report-June 2022, Waterslade, Rev 3, dated 28/06/2022
Sustainable Design and Construction Statement, Etude, Rev 3, dated 03/22
Green Performance Plan, Etude, Rev B, dated 09/21
Circular Economy Statement, Etude, Rev D, dated 09/21
Flood Risk Assessment and Drainage strategy, calfordseaden Rev. B, dated 23/08/2022
Arboricultural Report: Arboricultural Impact Assessment and Arboricultural Method Statement, David Clarke, dated 03/2022
Air Quality Assessment, Air Quality Consultants, (J4478A/1/F5)1, dated 09/2021
Noise and Vibration Impact Assessment, MaxFordham, Rev E 09, dated 06/2022
Healthy Street Transport Assessment (including CLP), Lime Transport, 20073 D2g, dated 23/10/2022
Fire Statement Form, Affinity Fire Engineering, Rev 12 – 07/09/2022
Vorley Road Structural Comments for Stage 3 Report, Calford Seaden, Rev B, dated 04/04/2022
Wind Microclimate Assessment; BRE, Issue 6, dated 16/03/2022
Draft Construction Management Plan, Calfordseaden, v3, dated 31/03/2022
BREEAM excellent tracker for Vorley Library, Ingleton Wood, dated 03/2022
Stage 2 BREEAM Options Appraisal Executive Summary, Ingleton Wood, dated 04/2022
Vorley Road Library BREEAM 2018, Ingleton Wood, dated 06/2021
Vorley Road Medical Centre BREEAM 2018, Ingleton Wood, dated 06/2021
Landscape UGF Calculation 3468A-LB-ZZ-XX-DR-L-6001, Levitt Bernstein 05/04/2022
Basement plant room layout, Ingleton Wood Rev.2, dated 04/2022
SK_16_External Services Layout, Ingleton Wood P02, dated 04/2022

	<p>UKPN Grid layout and connection details, dated 04/02/2021</p> <p>REASON: To comply with Section 70(1)(a) of the Town and Country Act 1990 as amended and the Reason for Grant and also for the avoidance of doubt and in the interest of proper planning.</p>
3	<p>Construction Management Plan and Construction Logistics Plan - (Details)</p> <p>CONDITION: A Construction Management Plan (CMP) and a Construction Logistics Plan (CLP) shall be submitted to, and approved in writing by, the Local Planning Authority as follows:</p> <p>a) Prior to demolition works: a Demolition Management and Logistics Plan</p> <p>b) Prior to construction works: a Construction Management and Logistics Plan</p> <p>The reports shall assess the impacts on surrounding streets, along with nearby residential amenity and other occupiers together with means of mitigating any identified impacts. The CMP must refer to the new LBI Code of Practice for Construction Sites.</p> <p>The development shall be carried out strictly in accordance with the approved documents during demolition and construction, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In the interests of residential amenity, highway safety, and the free flow of traffic on streets, and to mitigate the impacts of the development.</p>
4	<p>Air Quality and Dust Management Plan (Details)</p> <p>CONDITION: An Air Quality and Dust Management Plan (AQDMP) shall be submitted to, and approved in writing by, the Local Planning Authority as follows:</p> <p>c) Prior to demolition works and relating to the demolition</p> <p>d) Prior to construction works and relating to the construction</p> <p>The development shall be carried out strictly in accordance with the approved details and maintained as such thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To ensure adequate air quality</p>
5	<p>Tree Protection (Details)</p> <p>CONDITION: Prior to the commencement of works, a scheme for the protection of the retained trees, in accordance with BS 5837:2012, including a tree protection plan(s) (TPP) and an arboricultural method statement (AMS) shall be submitted to, and approved in writing by, the Local Planning Authority.</p> <p>Specific issues to be dealt with in the TPP and AMS:</p> <p>p. Location and installation of services/ utilities/ drainage.</p> <p>q. Methods of demolition within the root protection area (RPA as defined in BS 5837: 2012) of the retained trees.</p> <p>r. Details of construction within the RPA or that may impact on the retained trees.</p> <p>s. A full specification for the installation of boundary treatment works.</p> <p>t. A full specification for the construction of any roads, parking areas and driveways, including details of the no-dig specification and extent of the areas of the roads,</p>

	<p>parking areas and driveways to be constructed using a no-dig specification. Details shall include relevant sections through them.</p> <ul style="list-style-type: none"> u. Detailed levels and cross-sections to show that the raised levels of surfacing, where the installation of no-dig surfacing within Root Protection Areas is proposed, demonstrating that they can be accommodated where they meet with any adjacent building damp proof courses. v. A specification for protective fencing to safeguard trees during both demolition and construction phases and a plan indicating the alignment of the protective fencing. w. A specification for scaffolding and ground protection within tree protection zones. x. Tree protection during construction indicated on a TPP and construction and construction activities clearly identified as prohibited in this area. y. Details of site access, temporary parking, on site welfare facilities, loading, unloading and storage of equipment, materials, fuels and waste as well concrete mixing and use of fires z. Boundary treatments within the RPA <ul style="list-style-type: none"> aa. Methodology and detailed assessment of root pruning bb. Reporting of inspection and supervision cc. Methods to improve the rooting environment for retained and proposed trees and landscaping dd. Veteran and ancient tree protection and management <p>The development thereafter shall be implemented in strict accordance with the approved details, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: Required prior to commencement of development to satisfy the Local Planning Authority that the trees to be retained will not be damaged during demolition or construction and to protect and enhance the appearance and character of the site and locality.</p>
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6	Land Contamination (Details)
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	<p>CONDITION: Prior to the commencement of works, the following assessment in response to the NPPF and in accordance with Land Contamination Risk Management (LCRM) guidance (Environment Agency as updated 2021) and BS10175:2011+A2:2017 shall be submitted to and approved in writing by the Local Planning Authority:</p> <ul style="list-style-type: none"> e) A land contamination investigation for all of the site excluding the existing footprint of 4 Vorley Road. <p>No above ground works shall commence until the following assessment in response to the NPPF and in accordance with Land Contamination Risk Management (LCRM) guidance (Environment Agency as updated 2021) and BS10175:2011+A2:2017 have been submitted to and approved in writing by the Local Planning Authority:</p> <ul style="list-style-type: none"> f) A land contamination investigation, following demolition, covering the area previously occupied by 4 Vorley Road. <p>The investigations (a & b) shall be based upon and target the risks identified in the approved preliminary risk assessment and shall provide provisions for, where relevant, the sampling of soil, soil vapour, ground gas, surface and groundwater.</p> <p>Following the agreement to details relating to points a) and b); no above ground works shall commence until details of the following works have been submitted to, and approved in writing by, the Local Planning Authority:</p> <ul style="list-style-type: none"> g) A remediation method statement of any necessary land contamination remediation works arising from the land contamination investigation.
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	<p>If, during development, contamination not previously identified is found to be present at the site, the Council is to be informed immediately and no further development (unless otherwise agreed in writing by the Council) shall be carried out until a report indicating the nature of the contamination and how it is to be dealt with is submitted to, and agreed in writing by, the Council.</p> <p>Following the agreement to details relating to point c); no above ground works shall commence until details of the following report has been submitted to, and approved in writing by, the Local Planning Authority:</p> <p>h) A verification report, that demonstrates the effectiveness of the remediation carried out.</p> <p>This report shall include details of the remediation works carried out; results of any verification sampling, testing or monitoring including the analysis of any imported soil; all waste management documentation showing the classification of waste, its treatment, movement and disposal; and the validation of gas membrane placement.</p> <p>Each part of this condition must be carried out in compliance with and by a competent person who conforms to Land Contamination Risk Management (LCRM) guidance (Environment Agency as updated 2021) or the current UK requirements for sampling and testing.</p> <p>REASON: To ensure the land is safe for development and minimise any potential impacts from land contamination.</p>
7	<p>Piling Method Statement (Details)</p>
	<p>CONDITION: No piling shall take place until a Piling Method Statement (detailing the depth and type of piling to be undertaken and the methodology by which such piling will be carried out, including measures to prevent and minimise the potential for damage to subsurface sewerage infrastructure, and the programme for the works) has been submitted to, and approved in writing by, the local planning authority in consultation with Thames Water.</p> <p>Any piling must be undertaken in accordance with the terms of the approved piling method statement.</p> <p>REASON: The proposed works will be close to underground sewerage and water utility infrastructure. Piling has the potential to significantly impact / cause failure to local underground sewerage utility infrastructure and local underground water utility infrastructure.</p>
8	<p>Whole Life Carbon (Details)</p>
	<p>CONDITION: An updated Whole Life Carbon Assessment shall be submitted to, and approved in writing by, the Local Planning Authority as follows:</p> <p>e) Prior to demolition works and relating to the demolition phase; and</p> <p>f) Prior to construction works</p> <p>The updated assessment shall include/address:</p> <ul style="list-style-type: none"> - Further carbon reduction quantification through the detailed design stage material selection and specification; - Completed GLA Whole Life-Cycle Carbon Assessment - Details of how opportunities for retaining and refurbishing/re-purposing existing buildings, materials and other resources on site have been maximised to reduce the need for new materials;

	<ul style="list-style-type: none"> - Details of life cycle of embodied carbon and finite resources relating to the enabling works stage and end of life approach; - Details of the applicant's Principals of Sustainable Procurement and details of specific measures being taken on the site for specification and sourcing of materials; - Consideration of end of life de-construction; - Cost premiums, supply chain limits and structural constraints for the proposal and Implications of Key Performance Indicators not being met; and - Updated targets for Bill of Materials <p>The development shall be carried out strictly in accordance with the details so approved and no change therefrom unless otherwise specified in writing by the Local Planning Authority.</p> <p>REASON: The revised and updated details and designs will ensure that the embodied carbon emissions associated with the proposed development, taking into account the materials quantities and loads, operational energy consumption of the built scheme, with total emissions estimated and compared to the GLA benchmarks are reduced to their lowest possible levels, having regard to GLA benchmarks in accordance with policy S4 of the London Plan.</p>
9	<p>Circular Economy (Details)</p> <p>CONDITION: An updated Circular Economy Statement shall be submitted to, and approved in writing by, the Local Planning Authority as follows:</p> <ul style="list-style-type: none"> g) Prior to demolition works and relating to the demolition phase; and h) Prior to construction works <p>The updated statement shall include outstanding information including the reporting of key metrics and commitments to London Plan policy targets.</p> <p>The development shall be carried out strictly in accordance with the details so approved and no change therefrom unless otherwise specified in writing by the Local Planning Authority.</p> <p>REASON: The revised and updated details and designs will ensure that the embodied carbon emissions associated with the proposed development, taking into account the materials quantities and loads, operational energy consumption of the built scheme, with total emissions estimated and compared to the GLA benchmarks are reduced to their lowest possible levels, having regard to GLA benchmarks in accordance with policy S4 of the London Plan.</p>
10	<p>Facing Materials (Details)</p> <p>CONDITION: No above ground works shall commence until detailed drawings and samples of all facing materials are submitted to and approved in writing by the Local Planning Authority.</p> <p>The details and samples shall include:</p> <ul style="list-style-type: none"> h. Plan, elevation and section drawings, including jambs, head and sill, of all external windows and doors at a scale of 1:10; i. Samples and manufacturer's details at a scale of 1:10, of all main facing materials including brickwork in both primary base bricks, accent bricks and brick clad soffits and architectural pre-cast concrete balcony slabs, columns, copings, soffits, ground floor sills and entrance signage; j. Samples and manufacturer's details of all metalwork including PPC aluminium window system, sills, canopies, ventilation grilles and soffits and rainwater goods and bronze PPC steel balustrades, gates, shutters, soffits and gallery access railings;

	<p>k. A full scale sample panel should be erected on-site to show a typical window detail and should be approved by the Council before the relevant parts of the work are commenced. This should demonstrate the exact facing brick blend and detail demonstrating the proposed colours, texture, face-bond and pointing and include a junction with a window opening;</p> <p>l. Details of the green roof system</p> <p>m. Green Procurement Plan; and</p> <p>n. Any other materials to be used.</p> <p>The development shall be carried out strictly in accordance with the details and samples so approved, shall be maintained as such thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In the interest of securing sustainable development and to ensure that the resulting appearance and construction of the development is of a high standard</p>
11	Architectural Finish (Details)
	<p>CONDITION: No above ground works shall commence until detailed specifications including drawings (plan/section/elevation) at a minimum scale of 1:20 of the following details have been submitted to, and approved in writing by, the Local Planning Authority.</p> <p>i) Projecting corduroy brick banding on Building B</p> <p>j) Plant enclosure on Building B</p> <p>k) Balconies including privacy screens, balustrading and soffits</p> <p>l) Brise soleil</p> <p>m) Parapets</p> <p>n) Rainwater pipes, gutters</p> <p>o) Windows including sills and lintels</p> <p>p) Doors including individual, communal and refuse store entrances.</p> <p>The development shall be carried out strictly in accordance with the details and samples so approved, shall be maintained as such thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: High quality and well-considered detailing is fundamental to the success of the development and must be considered and approved by the Local Planning Authority prior to above ground works occurring</p>
12	Landscaping (Details)
	<p>LANDSCAPING: No above ground works shall commence until a detailed landscaping scheme has been submitted to, and approved in writing by, the Local Planning Authority.</p> <p>The landscaping scheme shall comprise the following details:</p> <p>m. an updated Access Statement detailing routes through the landscape and the facilities it provides;</p> <p>n. a biodiversity statement detailing how the landscaping scheme maximises biodiversity;</p> <p>o. existing and proposed underground services and their relationship to both hard and soft landscaping;</p> <p>p. proposed trees: their location, species, size and section showing rooting area; to include trees planted below deck level with canopy to spread above podium level;</p> <p>q. soft planting;</p> <p>r. topographical survey: including earthworks, ground finishes, top soiling with both conserved and imported topsoil(s), levels, drainage and fall in drain types;</p> <p>s. enclosures and boundary treatment: including types, dimensions and treatments of walls, fences, screen walls, barbed wire, retaining walls and hedges;</p>

	<p>t. hard landscaping: kerbs, edges, steps, ridge and flexible paving, including patterned coloured paving ‘entrance carpets’ to all buildings, unit paving, wet pour play safety surfaces and furniture including bike racks, seating and planters;</p> <p>u. wayfinding and signage;</p> <p>v. phasing of landscaping and planting;</p> <p>w. all play equipment and structures to include table tennis table, climbing features, all swings, fitness module, play sculptures, trampolines, slides and see saws; and</p> <p>x. any other landscaping feature(s) forming part of the scheme.</p> <p>All landscaping in accordance with the approved scheme shall be completed / planted during the first planting season following practical completion of the relevant phase of the development hereby approved in accordance with the approved planting phase. The landscaping and tree planting shall have a two year maintenance / watering provision following planting and any existing tree shown to be retained or trees or shrubs to be planted as part of the approved landscaping scheme which are removed, die, become severely damaged or diseased within five years of completion of the development shall be replaced with the same species or an approved alternative to the satisfaction of the Local Planning Authority within the next planting season. The development shall be carried out strictly in accordance with the details so approved and shall be maintained as such thereafter.</p> <p>REASON: In the interest of biodiversity, sustainability, playspace and to ensure that a satisfactory standard of visual amenity is provided and maintained.</p>
13	<p>Bird Boxes (Details)</p>
	<p>CONDITION: No above ground works shall commence until details of bird boxes across all new buildings have been submitted and approved in writing by the Local Planning Authority prior to superstructure works commencing on site.</p> <p>The development shall be carried out strictly in accordance with the approved details and maintained as such thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To ensure the development provides the maximum possible provision towards creation of habitats and valuable areas for biodiversity.</p>
14	<p>Sound Insulation (Details)</p>
	<p>CONDITION: No above ground works shall commence until a scheme for sound insulation and noise control measures has been submitted to, and approved in writing by, the Local Planning Authority. The sound insulation and noise control measures shall achieve the following internal noise targets:</p> <ul style="list-style-type: none"> - Bedrooms (23.00-07.00 hrs) 30 dB LAeq,8 hour and 45 dB Lmax (fast) - Living Rooms (07.00-23.00 hrs) 35 dB LAeq, 16 hour - Dining rooms (07.00 –23.00 hrs) 40 dB LAeq, 16 hour <p>The sound insulation and noise control measures shall be implemented prior to the first occupation of the development hereby approved and carried out strictly in accordance with the details so approved and maintained as such permanently thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To ensure that an appropriate standard of accommodation is provided.</p>
15	<p>Sound Insulation Strategy – Community Uses and Residential Uses (Details)</p>
	<p>CONDITION: No above ground works shall commence until full particulars and details of a scheme for sound insulation between the proposed community uses (library and medical</p>

	<p>centre) and residential use of the building have been submitted to, and approved in writing by, the Local Planning Authority.</p> <p>The sound insulation and noise control measures shall be carried out strictly in accordance with the details so approved, shall be implemented prior to the first occupation of the development, and maintained as such permanently thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To ensure that an appropriate standard of accommodation is provided.</p>
16	Standby Generator (Details)
	<p>CONDITION: No above ground works shall commence until a written code for the management of noise from emergency plant and equipment is submitted to, and approved in writing by, the Local Planning Authority.</p> <p>The management code shall identify measures to reduce the impact of the noise and vibration on the community and shall include details of equipment testing. The code shall be fully implemented and operated at all times in strict accordance with the approved details and maintained permanently thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To ensure that the operation of the generator does not impact on residential amenity.</p>
17	Cycle Parking (Details)
	<p>CONDITION: Notwithstanding the plans hereby approved, details of 132 long-stay resident's cycle parking spaces, 7 residential visitor cycle parking spaces, 9 staff cycle parking spaces and 24 visitor cycle parking spaces (for the library and medical uses), inclusive of the provision of accessible parking spaces, shall be submitted to and approved in writing by the Local Planning Authority.</p> <p>The approved cycle storage shall be provided prior to the first occupation of the relevant part of the development hereby approved, shall be maintained as such thereafter and no change therefrom shall take place unless otherwise agreed in writing by the Local Planning Authority.</p> <p>REASON: To ensure adequate and suitable bicycle parking is available and easily accessible on site and to promote sustainable modes of transport.</p>
18	Secure by Design (Details)
	<p>CONDITION: No above ground works shall commence until details have been submitted to, and approved in writing, by the Local Planning Authority to demonstrate that the development can achieve 'Secured by Design' accreditation.</p> <p>The development shall be constructed and operated strictly in accordance with the approved details and maintained as such thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In the interest of creating safer, sustainable communities.</p>
19	Accessible Housing (Compliance and Details)
	<p>CONDITION: Notwithstanding the plans hereby approved, 65 x of the residential units shall be constructed to Category 2 of the National Standard for Housing Design as set out in the Approved Document M 2015 'Accessible and adaptable dwellings' M4 (2) and 7 x residential units shall be constructed to Category 3 of the National Standard for Housing Design as set out in the Approved Document M 2015 'Wheelchair user dwellings' M4 (3).</p>

	<p>No above ground works shall commence until Building Regulations Approved Plans and Decision Advice Notice, confirming that these requirements will be achieved, shall be submitted to and approved in writing by the Local Planning Authority. The Approved Plans to show furniture, key dimensions and manoeuvring allowances, as set out in the provisions of the Approved Document M (Volume 1).</p> <p>The development shall be carried out strictly in accordance with the approved details and permanently maintained as such thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To secure the provision of visitable, adaptable and wheelchair user accessible homes appropriate to meet diverse and changing needs.</p>
20	Green/Blue Roof Details and Maintenance Plan (Details)
	<p>CONDITION: Prior to the commencement of relevant works, details of the biodiversity green and blue roofs within the scheme as shown on plan C(0)1003 Rev. D within the revised Flood Risk and Drainage Strategy, dated 23/08/22 shall be submitted to, and approved in writing by, the Local Planning Authority. The biodiversity (green) roofs shall be:</p> <ul style="list-style-type: none"> E) biodiversity based with extensive substrate base (depth 80-150mm); and F) planted/seeded with an agreed mix of species within the first planting season following the practical completion of the building works (the seed mix shall be focused on wildflower planting, and shall contain no more than a maximum of 25% sedum). G) Details of the irrigation and maintenance regime for the proposed green roofs shall be submitted to and approved in writing by the local planning authority before any works thereby affected are begun. H) The biodiversity (green) roof shall not be used as an amenity or sitting out space of any kind whatsoever and shall only be used in the case of essential maintenance or repair, or escape in case of emergency. <p>The biodiversity roofs shall be carried out strictly in accordance with the details so approved and shall be maintained permanently as such thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To ensure the development provides the maximum possible provision towards creation of habitats and valuable areas for biodiversity.</p>
21	Delivery and Servicing Plan (Details)
	<p>CONDITION: Prior to the first occupation of the development hereby approved, a detailed Delivery and Servicing Plan (DSP) shall be submitted to, and approved in writing by, the Local Planning Authority.</p> <p>The development shall be constructed and operated strictly in accordance with the details so approved and shall be maintained as such thereafter unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In order to secure highway safety and free flow of traffic, local residential amenity and to mitigate the impacts of the development.</p>
22	Parking Design and Management Plan (Details)
	<p>CONDITION: Prior to the first occupation of the development hereby approved, a Parking Design and Management Plan shall be submitted to and approved by the Local Planning Authority.</p>

	<p>The plan must set out the parking arrangements across the site, including how drop-off points are properly controlled, how traffic will be suitably managed and how each wheelchair accessible unit can be provided with a designated accessible parking space in future should there be demand.</p> <p>The approved details shall be installed prior to the occupation of the development and shall be maintained as such thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In the interest of traffic safety and traffic management in accordance with Islington Core Strategy Policy CS10.</p>
23	Energy Statement (Details)
	<p>CONDITION: The energy efficiency measures/features and renewable energy technology, as detailed within the 'Energy Strategy' (ref no: Sustainable Design and Construction Statement prepared by Edute and dated March 2022) shall be installed and operational prior to the first occupation of the buildings hereby approved.</p> <p>Should there be any change to the energy features/ measures within the approved Energy Strategy, a revised Energy Strategy shall be submitted to and agreed in writing by the Local Planning Authority prior to the occupation of the development.</p> <p>REASON: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO2 emission reduction targets by energy efficient measures/features and renewable energy are met.</p>
24	BREEAM Excellent – Residential (Details)
	<p>CONDITION: Prior to the first occupation of the residential element of the development, evidence to show that the development will achieve four a four-star rating (as a minimum) under the BRE Home Quality Mark scheme shall be submitted to, and approved in writing, by the Local Planning Authority.</p> <p>Within 6 months of first occupation of the relevant building, certification confirming that the development achieves a four-star rating (as a minimum) under the BRE Home Quality Mark scheme. shall be submitted to, and approved in writing, by the Local Planning Authority.</p> <p>REASON: In the interest of energy efficiency and sustainability.</p>
25	BREEAM Excellent – Non-Residential (Details)
	<p>CONDITION: The library and medical centre element of the development shall achieve a BREEAM rating of no less than 'Excellent', unless otherwise agreed in writing by the Local Planning Authority.</p> <p>REASON: In the interest of addressing climate change and to secure sustainable development.</p>
26	Photovoltaic Panel Installation (Details)
	<p>CONDITION: Prior to first occupation of the development hereby approved, details of the proposed Solar Photovoltaic Panels shall be submitted to, and approved in writing by, the Local Planning Authority.</p> <p>This submission shall demonstrate how Solar PVs have been maximised on site, and details shall include but not be limited to: location; area of panels; how the PV output has been maximised and design (including section drawings showing the angle of panels in-situ, and</p>

	<p>elevation plans).</p> <p>The solar photovoltaic panels as approved shall be installed prior to the first occupation of each of the buildings and maintained as such permanently thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In the interests of securing sustainable development and to ensure that the resulting appearance and construction of the development is of a high standard of design.</p>
27	Lighting (Details)
	<p>CONDITION: Prior to the first occupation of the development hereby permitted details of any general / security lighting measures shall be submitted to, and approved in writing by, the Local Planning Authority.</p> <p>The details shall:</p> <ul style="list-style-type: none"> - include the location and full specification of: all lamps; light levels/spill lamps and support structures where appropriate and hours of operation; - demonstrate how the ecology of the site would not be adversely affected by the proposed lighting. <p>The general lighting and security measures shall be carried out strictly in accordance with the details so approved, shall be installed prior to occupation of the development and shall be permanently maintained as such thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To ensure that any resulting general or security lighting is appropriately located, designed to not adversely impact neighbouring residential amenity nor those with visual impairments, contributes towards safety and security, does not adversely impact biodiversity or ecology and is appropriate to the overall design of the building.</p>
28	External Signage (Details)
	<p>CONDITION: Prior to occupation of the development hereby approved, details of all external signage shall be submitted to, and approved in writing by, the Local Planning Authority.</p> <p>The agreed details shall be installed prior to the occupation of the development and shall be maintained as such permanently thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In the interests of visual amenity and to ensure that the entrance approach is both welcoming and inviting.</p>
29	Inclusive Design (Details)
	<p>CONDITION: All inclusive design measures identified within the application submission shall be installed and operational prior to the first occupation of the development hereby approved.</p> <p>Further details on the following points shall be submitted and approved in writing by the Local Planning Authority prior to the completion of the development hereby approved:</p> <ul style="list-style-type: none"> - the accessibility of bike and bin stores; - Provision of accessible cycle parking spaces; - details on appropriate seating within the public realm, - audio loop systems, - medical garden accessibility - medical garden accessible play

	<p>- confirmation that corridors and lobbies meet relevant standards within Islington's Inclusive Design SPD.</p> <p>The inclusive design measures shall be retained as such in perpetuity unless otherwise approved in writing by the Local Planning Authority</p> <p>REASON: In order to facilitate and promote inclusive and sustainable communities.</p>
30	Obscure Glazing and Privacy Screens (Details)
	<p>CONDITION: Prior to the first occupation of Building B, details of obscured glazing and privacy screens to prevent potential overlooking from proposed Building B to the land to the north shall be submitted to, and approved in writing by, the Local Planning Authority.</p> <p>The obscure glazing and privacy screens shall be installed prior to the occupation of the relevant units and retained as such permanently thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In the interest of preventing undue overlooking between habitable rooms within the development itself, to protect the future amenity and privacy of residents.</p>
31	Management Plan – Library (Details)
	<p>Prior to the first occupation of the library, a management plan for any private hire of the space shall be submitted to, and approved in writing by, the Local Planning Authority.</p> <p>The development shall be carried out strictly in accordance with the approved plan, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In the interest of preventing undue overlooking between habitable rooms within the development itself, to protect the future amenity and privacy of residents.</p>
32	Management Plan – Medical Centre (Details)
	<p>Prior to the first occupation of the medical centre, a management plan for any private hire of the medical centre shall be submitted to, and approved in writing by, the Local Planning Authority.</p> <p>The development shall be carried out strictly in accordance with the approved plan, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In the interest of preventing undue overlooking between habitable rooms within the development itself, to protect the future amenity and privacy of residents.</p>
33	Whole Life Carbon Post-Construction Assessment Report (Details)
	<p>CONDITION: Within 3 months of practical completion of the development hereby approved, a whole life carbon post-construction assessment report shall be submitted to approved by the Local Planning Authority.</p> <p>REASON: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO2 emission reduction targets by energy efficient measures/features and renewable energy are met.</p>
34	Circular Economy Post-Construction Report (Details)
	<p>CONDITION: Within 3 months of practical completion of the development hereby approved, a post-construction circular economy report shall be submitted to the Local Planning Authority for approval in writing.</p>

	<p>REASON: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that circular economy principles have been incorporated into the design, construction and management of the approved development in accordance with London Plan Policy SI7.</p>
35	<p>Fire Strategy (Compliance)</p> <p>CONDITION: The details and measures set out in the Fire Strategy Report, by Affinity Fire Engineering – Rev. 12, dated 07.09.2022 shall be implemented in accordance with the approved document, unless otherwise agreed in writing by the Local Planning Authority.</p> <p>Should any subsequent change(s) be required to secure compliance with the submitted fire strategy, a revised fire strategy shall be submitted to and approved by the Local Planning Authority.</p> <p>The development shall be carried out in accordance with the Fire Strategy under this condition and shall be maintained as such thereafter.</p> <p>REASON: To ensure that the development incorporates the necessary fire safety measures in accordance with the Mayor's London Plan Policy D12.</p>
36	<p>Basement Excavation (Compliance)</p> <p>CONDITION: The development shall be constructed in accordance with the approved 'Structural Method Assessment – Revision P2 – Dated: May 2021 – prepared by: ads consultancy' unless otherwise agreed in writing.</p> <p>The certifying professional that endorsed the Structural Method Statement (or a suitably qualified Chartered Civil Engineer (MICE) or a Chartered Structural Engineer (MIStruct.E) with relevant experience shall be appointed to inspect, approve and monitor the critical elements of both permanent and temporary basement construction works throughout their duration to ensure compliance with Council's Basement Development SPD.</p> <p>REASON: To ensure that structural stability has been evaluated by a suitably qualified and experienced professional.</p>
37	<p>Refuse and Recycling (Compliance)</p> <p>CONDITION: Prior to the first occupation of the development hereby approved the dedicated refuse / recycling enclosure(s) shown on the approved plan - Layout Ground Floor – drawing number: 3468A-LB-ZZ-00-DP-A-1100-25 – Layout GF – dated: 11.10.2022 - shall be provided and shall be maintained as such permanently thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To secure the necessary physical waste enclosures to support the development and to ensure that responsible waste management practices are adhered to.</p>
38	<p>Lifts (Compliance)</p> <p>CONDITION: All lifts hereby approved shall be installed and operational prior to the first occupation of each of the buildings hereby approved.</p> <p>REASON: To ensure that inclusive and accessible routes are provided throughout the floorspace at all floors and also accessible routes through the site are provided to ensure no one is excluded from full use and enjoyment of the site.</p>
39	<p>Plumbing (No pipes to outside of building) (Compliance)</p> <p>CONDITION: No plumbing, down pipes, rainwater pipes or foul pipes shall be located to the external elevations of the development hereby approved, unless otherwise approved in writing by the Local Planning Authority as part of discharging this condition.</p>

	<p>REASON: The Local Planning Authority considers that such plumbing and pipes would potentially detract from the appearance of the building and undermine the current assessment of the application.</p>
40	<p>Biodiversity Mitigation (compliance)</p> <p>CONDITION: The proposal hereby approved shall implement the biodiversity enhancement measures as outlined in approved document Ecological Impact Assessment by AGB Environmental Limited – dated 27/04/2022.</p> <p>The biodiversity enhancement measures shall be installed prior to the occupation of the development and maintained as such permanently thereafter, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: In the interests of improving on-site biodiversity.</p>
41	<p>Plant Emissions (Compliance)</p> <p>CONDITION: On-site plant and machinery must comply with the London Non-Road Mobile Machinery (NRMM) Low Emission Zone standards (London Plan Policy SI 1 (D)).</p> <p>REASON: To ensure adequate air quality.</p>
42	<p>Fixed Plant (Compliance)</p> <p>CONDITION: The design and installation of new items of fixed plant shall be such that when operating the cumulative noise level LAeq Tr arising from the proposed plant, measured or predicted at 1m from the facade of the nearest noise sensitive premises, shall be a rating level of at least 5dB(A) below the background noise level LAF90 Tbg. The measurement and/or prediction of the noise should be carried out in accordance with the methodology contained within BS 4142: 2014+A1:2019.</p> <p>REASON: To ensure that an appropriate standard of accommodation is provided.</p>
43	<p>Hours of Operation – Medical Centre (Compliance)</p> <p>CONDITION: The medical centre shall be operational only between the following hours</p> <ul style="list-style-type: none"> - 8am to 10pm (Monday to Saturday) - 11am to 5pm (Sundays) <p>The restrictions shall be applied and permanently adhered to unless otherwise agreed with the Local Planning Authority.</p> <p>REASON: In the interests of protecting neighbouring residential amenity.</p>
44	<p>Hours of Operation – Library (Compliance)</p> <p>CONDITION: The library shall be operational only between the following hours</p> <ul style="list-style-type: none"> - 8am to 10pm (Monday to Saturday) - 11am to 5pm (Sundays) <p>The restrictions shall be applied and permanently adhered to unless otherwise agreed with the Local Planning Authority.</p> <p>REASON: In the interests of protecting neighbouring residential amenity.</p>
45	<p>Sustainable Urban Drainage (Compliance)</p>

	<p>CONDITION: The details and measures set out in the Flood Risk Assessment and Drainage strategy by Calford Seaden Rev. B, dated 23/08/2022 shall be implemented in accordance with the approved document, unless otherwise approved in writing by the Local Planning Authority.</p> <p>REASON: To ensure that sustainable management of water and minimise the potential for surface level flooding.</p>
46	Restriction of Use (Compliance)
	<p>CONDITION: Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended), or the provisions of any Order revoking and re-enacting that Order, no change of use of the 735 sqm (GIA) of approved E(e) floorspace and 826sqm (GIA) of approved F1(d) floorspace to any other use within Class E and Class F of the Schedule to the Town and Country Planning (Use Class) Order 1987 as amended 2005 (or the equivalent use within any amended/updated subsequent Order) or any other uses within any other use Class, unless otherwise agreed in writing by the Local Planning Authority.</p> <p>REASON: To safeguard the amenities of the adjoining residential units and the area generally and in the interest of securing the specific community uses.</p>
47	Restriction of PD Rights - Class E to residential (Compliance)
	<p>CONDITION: Notwithstanding the provisions of Schedule 2, Part 3, Class MA the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modifications), no change of use from Class E (commercial, business and service) to a use falling within Class C3 (dwellinghouses) shall take place without obtaining the express planning permission from the Local Planning Authority.</p> <p>REASON: For the avoidance of doubt and to ensure that the Local Planning Authority can restrict the use of the building to this specific use only, in order to protect the supply of office and commercial floorspace in this location.</p>

INFORMATIVES

List of Informatives:

1	Working with the applicant
	<p>In dealing with this application, Islington Council has implemented the requirements of the National Planning Policy Framework and of the Town and Country Planning (Development Management Procedure) (England) Order 2015 to work with the applicant in a positive and proactive manner. As with all applicants, we have made available detailed advice in the form of our statutory policies in the relevant constituent parts of the Local Plan and London Plan, Supplementary Planning documents, and all other Council guidance, as well as offering a full pre-application advice service, so as to ensure that applicant has been given every opportunity to submit an application which is likely to be considered favourably.</p>
2	Director's Letter
	<p>You are advised that this permission has been granted subject to a Director's Letter.</p>

3	<p>Superstructure</p> <p>DEFINITION OF 'SUPERSTRUCTURE' AND 'PRACTICAL COMPLETION'</p> <p>Some conditions attached to this permission have the time restrictions 'prior to superstructure works commencing on site' and/or 'following practical completion'. The council considers the definition of 'superstructure' as having its normal or dictionary meaning, which is: the part of a building above its foundations. The council considers the definition of 'practical completion' to be: when the work reaches a state of readiness for use or occupation even though there may be outstanding works/matters to be carried out.</p>
4	<p>Community Infrastructure Levy (CIL) (Granting Consent)</p> <p>INFORMATIVE: Under the terms of the Planning Act 2008 (as amended) and Community Infrastructure Levy Regulations 2010 (as amended), this development is liable to pay the Mayor of London's Community Infrastructure Levy (CIL). This will be calculated in accordance with the Mayor of London's CIL Charging Schedule 2012. One of the development parties must now assume liability to pay CIL by submitting an Assumption of Liability Notice to the Council at cil@islington.gov.uk. The Council will then issue a Liability Notice setting out the amount of CIL that is payable.</p> <p>Failure to submit a valid Assumption of Liability Notice and Commencement Notice prior to commencement of the development may result in surcharges being imposed. The above forms can be found on the planning portal at: www.planningportal.gov.uk/planning/applications/howtoapply/whattosubmit/cil</p> <p>Pre-Commencement Conditions: These conditions are identified with an 'asterix' * in front of the short description. These conditions are important from a CIL liability perspective as a scheme will not become CIL liable until all of these unidentified pre-commencement conditions have been discharged.</p>
5	<p>Car-Free Development</p> <p>INFORMATIVE: (Car-Free Development) All new developments are car free in accordance with Policy CS10 of the Islington Core Strategy 2011. This means that no parking provision will be allowed on site and occupiers will have no ability to obtain car parking permits, except for parking needed to meet the needs of disabled people.</p>
6	<p>Groundwater Risk Permit</p> <p>A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. We would expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 020 3577 9483 or by emailing trade.effluent@thameswater.co.uk . Application forms should be completed on line via www.thameswater.co.uk. Please refer to the Wholesale; Business customers; Groundwater discharges section.</p>
7	<p>Working Near Water Mains</p> <p>There are water mains crossing or close to your development. Thames Water do NOT permit the building over or construction within 3m of water mains. If you're planning significant works near our mains (within 3m) we'll need to check that your development doesn't reduce capacity, limit repair or maintenance activities during and after construction, or inhibit the services we provide in any other way. The applicant is advised to read our guide working near or diverting our pipes. https://www.thameswater.co.uk/developers/larger-scale-developments/planning-your-development/working-nearour-pipes</p>

8	Water Pressure
	Thames Water will aim to provide customers with a minimum pressure of 10m head (approx. 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.
9	Management Code – Noise from emergency plant and equipment
	<p>The Management code shall include measures to address the following matters:</p> <ol style="list-style-type: none"> 6. The testing of equipment not to take place between the hours of 1800 and 0800 on any day, and not at any time on Sundays, Bank Holidays or after 1300 on a Saturday. 7. The duration of the testing to be commensurate with the test requirements and not to exceed one hour. 8. A list of potential residential receptors to be drawn up and those receptors to be given advance written notification of the time and date of the test. 9. The acoustic design and control of the fixed plant and equipment to meet a criterion of a rating level, measured or calculated at 1m from the façade of the nearest noise sensitive premises, of not more than 5dB(A) above the existing background noise level (L_{A90}). The rating level to be determined as per the guidance provided in BS4142:2014. 10. A report to be commissioned by the applicant, using an appropriately experienced & competent person, to assess the noise from the plant and machinery. The report is to be submitted to, and approved in writing by the Local Planning Authority, and any noise mitigation measures shall be installed before the commencement of the use hereby permitted and permanently retained thereafter.”

APPENDIX 2: RELEVANT POLICIES

This appendix lists all relevant development plan policies and guidance notes pertinent to the determination of this planning application.

1 National Guidance

The National Planning Policy Framework (July 2021) seeks to secure positive growth in a way that effectively balances economic, environmental and social progress for this and future generations. The NPPF is a material consideration and has been taken into account as part of the assessment of these proposals.

2. Development Plan

The Development Plan is comprised of the London Plan 2011, Islington Core Strategy 2011, Development Management Policies 2013, Finsbury Local Plan 2013 and Site Allocations 2013. The following policies of the Development Plan are considered relevant to this application:

A) The London Plan - Spatial Development Strategy for Greater London (March 2021)

- Policy GG1 Building strong and inclusive communities
- Policy GG2 Making the best use of land
- Policy GG3 Creating a healthy city
- Policy GG4 Delivering the homes Londoners need
- Policy GG5 Growing a good economy
- Policy GG6 Increasing efficiency and resilience
- Policy D1 London's form, character and capacity for growth
- Policy D2 Infrastructure requirements for sustainable densities
- Policy D3 Optimising site capacity through the design-led approach
- Policy D4 Delivering good design
- Policy D5 Inclusive design
- Policy D6 Housing quality and standards
- Policy D7 Accessible housing
- Policy D8 Public realm
- Policy D11 Safety, security and resilience to emergency
- Policy D12 Fire safety
- Policy D14 Noise
- Policy H1 Increasing housing supply
- Policy H4 Delivering affordable housing
- Policy H5 Threshold approach to applications
- Policy H6 Affordable housing tenure
- Policy H10 Housing size mix
- Policy S4 Play and informal recreation
- Policy E1 Offices
- Policy E7 Industrial intensification, co-location and substitution
- Policy E11 Skills and opportunities for all
- Policy HC1 Heritage conservation and growth
- Policy G1 Green infrastructure
- Policy G4 Open space
- Policy G5 Urban greening
- Policy G6 Biodiversity and access to nature
- Policy G7 Trees and woodlands
- Policy SI1 Improving air quality
- Policy SI2 Minimising greenhouse gas emissions
- Policy SI3 Energy infrastructure
- Policy SI4 Managing heat risk

Policy SI5 Water infrastructure
Policy SI7 Reducing waste and supporting the circular economy
Policy SI12 Flood risk management
Policy SI13 Sustainable drainage
Policy T1 Strategic approach to transport
Policy T2 Healthy streets
Policy T3 Transport capacity, connectivity and safeguarding
Policy T4 Assessing and mitigating transport impacts
Policy T5 Cycling
Policy T6 Car parking
Policy T6.1 Residential parking
Policy T7 Deliveries, servicing and construction
Policy T8 Funding transport infrastructure through planning

B) Islington Core Strategy 2011

Spatial Strategy	Policy CS13 (Employment Spaces)
Policy CS2 (Finsbury Park)	Policy CS14 (Retail and Services)
Policy CS3 (Nag's Head and Upper Holloway Road)	Policy CS15 (Open Space and Green Infrastructure)
Policy CS4 (Highbury Corner and Holloway Road)	Policy CS16 (Play Space)
Policy CS5 (Angel and Upper Street)	Policy CS17 (Sports and Recreation Provision)
Policy CS6 (King's Cross)	
Policy CS7 (Bunhill and Clerkenwell)	Infrastructure and Implementation
Policy CS8 (Enhancing Islington's Character)	Policy CS18 (Delivery and Infrastructure)
	Policy CS19 (Health Impact Assessments)
Strategic Policies	Policy CS20 (Partnership Working)
Policy CS9 (Protecting and Enhancing Islington's Built and Historic Environment)	
Policy CS10 (Sustainable Design)	
Policy CS11 (Waste)	
Policy CS12 (Meeting the Housing Challenge)	

C) Development Management Policies June 2013

Design and Heritage

- DM2.1 Design
- DM2.2 Inclusive Design
- DM2.3 Heritage

Housing

- DM3.1 Mix of housing sizes
- DM3.2 Existing housing
- DM3.4 Housing standards
- DM3.5 Private outdoor space
- DM3.7 Noise and vibration (residential uses)

Employment

- DM5.1 New business floorspace
- DM5.2 Loss of existing business floorspace
- DM5.4 Size and affordability of workspace

Health and open space

- DM6.1 Healthy development
- DM6.2 New and improved public open space
- DM6.3 Protecting open space
- DM6.4 Sport and recreation
- DM6.5 Landscaping, trees and biodiversity
- DM6.6 Flood prevention

Energy and Environmental Standards

- DM7.1 Sustainable design and construction statements
- DM7.2 Energy efficiency and carbon reduction in minor schemes
- DM7.3 Decentralised energy networks
- DM7.4 Sustainable design standards
- DM7.5 Heating and cooling

Transport

- DM8.1 Movement hierarchy
- DM8.2 Managing transport impacts
- DM8.3 Public transport
- DM8.4 Walking and cycling
- DM8.5 Vehicle parking
- DM8.6 Delivery and servicing for new developments

Infrastructure

- DM9.1 Infrastructure
- DM9.2 Planning obligations
- DM9.3 Implementation

Emerging policy: draft Islington Local Plan 2019

2. The Regulation 19 draft of the Local Plan was approved at Full Council on 27 June 2019 for consultation and subsequent submission to the Secretary of State for Independent Examination. From 5 September 2019 to 18 October 2019, the Council consulted on the Regulation 19 draft of the new Local Plan. Submission took place on 12 February 2020 with the examination process now in progress. As part of the examination consultation on pre-hearing modifications took place between 19 March to and 9 May 2021. The Matters and Issues have now been published and hearings took place between 13 September to 5 October 2021.

In line with the NPPF, Local Planning Authorities may give weight to relevant policies in emerging plans according to:

- the stage of preparation of the emerging plan (the more advanced its preparation, the greater the weight that may be given);

- the extent to which there are unresolved objections to relevant policies (the less significant the unresolved objections, the greater the weight that may be given); and

- the degree of consistency of the relevant policies in the emerging plan to this Framework (the closer the policies in the emerging plan to the policies in the Framework, the greater the weight that may be given).

Emerging policies that are relevant to this application are set out below in Appendix 2. The emerging policies are considered to be consistent with the current policies.

Emerging Islington Local Plan (2019)

Policy PLAN1: Site appraisal, design principles and process
Policy SP2: King's Cross and Pentonville Road
Policy H1: Thriving communities
Policy H2: New and existing conventional housing
Policy H3: Genuinely affordable housing
Policy H4: Delivering high quality housing
Policy H5: Private outdoor space
Policy B1: Delivering business floorspace
Policy B2: New business floorspace
Policy B3: Existing business floorspace
Policy B5: Jobs and training opportunities
Policy G1: Green Infrastructure
Policy G4: Biodiversity, landscape design and trees
Policy G5: Green roofs and vertical greening
Policy S1: Delivering sustainable design
Policy S2: Sustainable design and construction
Policy S3: Sustainable design standards
Policy T1: Enhancing the public realm and sustainable transport
Policy T3: Car-free development
Policy T5: Delivering, servicing and construction
Policy DH4: Basement development

5. Designations

The site has the following designations under the London Plan 2021, Islington Core Strategy 2011, Development Management Policies 2013, Finsbury Local Plan 2013 and Site Allocations 2013:

- Central Activities Zone (CAZ)
- Kings Cross & Pentonville Road Key Area (Core Strategy policy CS9)
- Employment Growth Area (Northdown Street EGA)
- Article 4 Direction – B1a (office) to C3 (residential)
- Article 4 Direction – A1 (Retail) to A2 (Professional and Financial Services)
- Article 4 Direction – B1c (Light Industrial) to C3 (Residential)
- CrossRail 2 Safeguarding Zone
- London Underground Zone of Interest (Tunnels)

6. Supplementary Planning Guidance (SPG) / Document (SPD)

The following SPGs and/or SPDs are relevant:

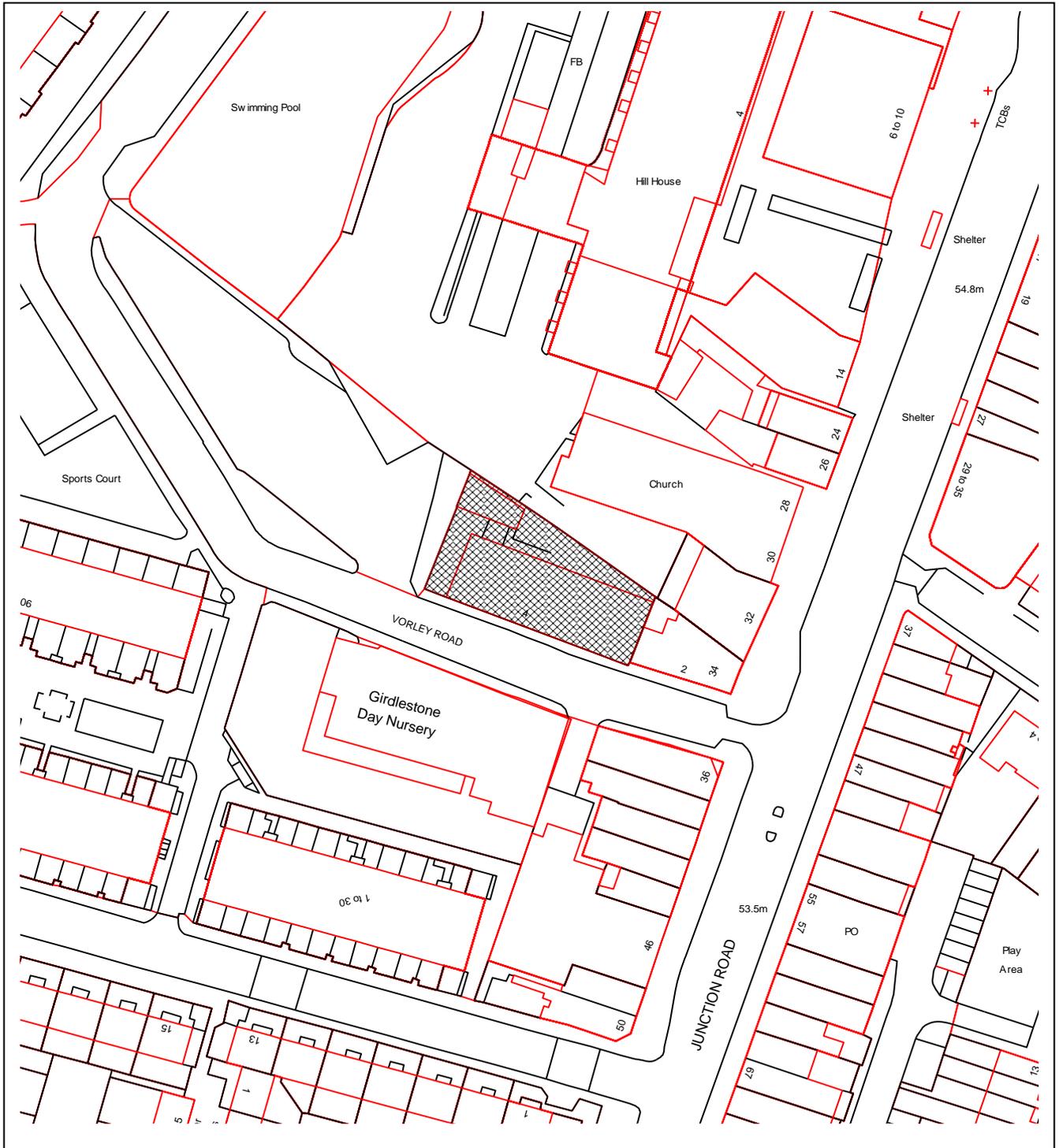
Islington Local Development Plan	London Plan
- Student Bursaries	- Accessible London: Achieving and Inclusive Environment
- Environmental Design	- Housing
- Small Sites Contribution	- Sustainable Design & Construction
- Accessible Housing in Islington	- Providing for Children and Young Peoples
- Conservation Area Design Guidelines	- Play and Informal Recreation
- Inclusive Landscape Design	- Planning for Equality and Diversity in London

- Planning Obligations and S106
- Urban Design Guide

Register of Locally Listed Buildings and Locally Significant Shopfronts (April 2010)

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Islington SE GIS Print Template



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PLANNING COMMITTEE REPORT

 Development Management Service
 Planning and Development Division
 Community Wealth Building Department

PLANNING COMMITTEE		AGENDA ITEM
Date:	6 th December 2022	NON-EXEMPT

Application number	P2022/1847/FUL
Application type	Full Planning Application
Ward	Caledonian Ward
Listed building	N/A
Conservation area	N/A
Development Plan Context	Vale Royal/Brewery Road Local Significant Industrial Site (LSIS); Kings Cross & Pentonville Road Core Strategy Key Area; Local view from Dartmouth Park Hill - LV7; Land Ownership – National Rail Surface - NR Owned; Article 4 Direction A1-A2 (Rest of the borough); Article 4 Direction B1c to C3
Licensing Implications	N/A
Site Address	14 - 22 Vale Royal and 184 York Way, London, N7 9AD
Proposal	Alterations, extensions and partial change of use to create mixed use (Classes B8 and E(g)) buildings arranged over ground, first, second, part third, part fourth and part fifth floors and associated plant, delivered in two phases

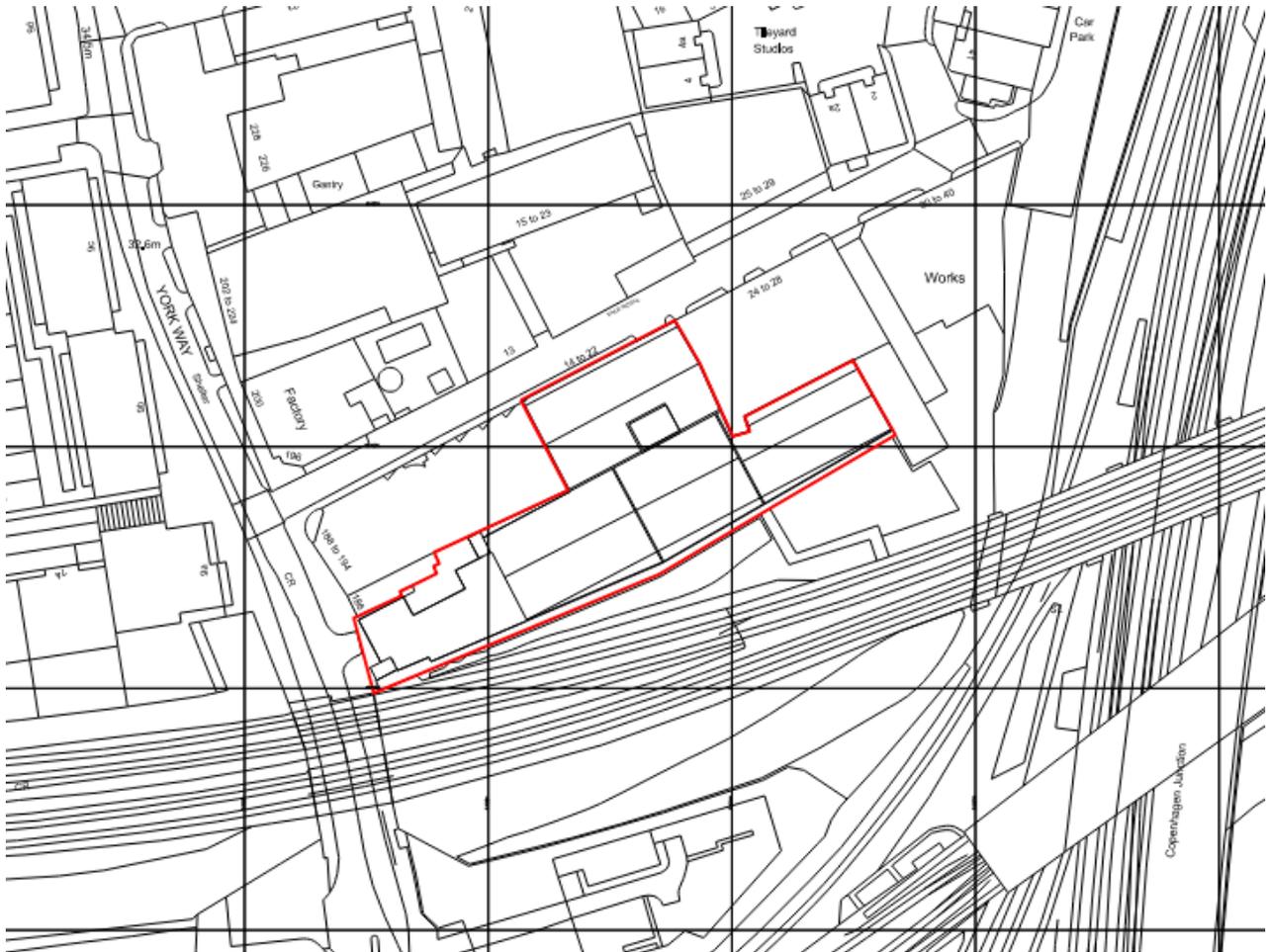
Case Officer	Tom Broomhall
Applicant	Access Self-Storage
Agent	Jon Dingle - Jon Dingle Ltd

1. RECOMMENDATION

The Committee is asked to resolve to GRANT planning permission:

1. subject to the conditions set out in Appendix 1; and
2. conditional on the prior completion of a Deed of Planning Obligation made under section 106 of the Town and Country Planning Act 1990 securing the heads of terms as set out in Appendix 1.

2. SITE PLAN (site outlined in red)



3. PHOTOS OF SITE/STREET



Image 3.1: Aerial View (Site edged red)



Image 3.2: Aerial View - Looking Southeast (Site edged red)

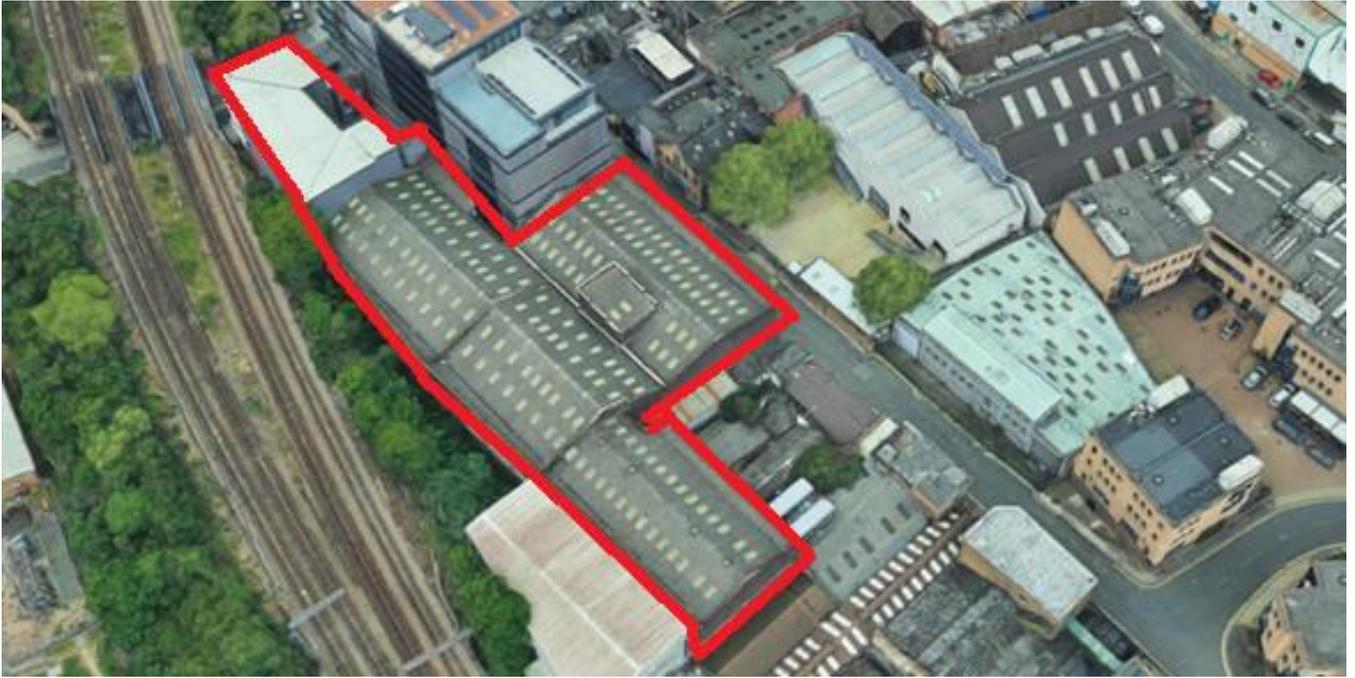


Image 3.3: Aerial View – Looking Northwest



Image 3.4: View along Vale Royal looking Northeast



Image 4 - Street View looking northeast along Vale Royal



Image 5 - Street View looking southwest along Vale Royal

4. SUMMARY

- 4.1 This planning application seeks planning permission for alterations, extensions and partial change of use to create a mixed use (Classes B8 and E(g)) buildings arranged over ground, first, second, third, part fourth and part fifth floors and associated plant equipment.
- 4.2 The site is located within the designated Vale Royal/Brewery Road Locally Significant Industrial Site (LSIS), and the Kings Cross & Pentonville Road Key Area (Core Strategy Policy CS6). The adopted LSIS policy supports the retention and intensification of uses appropriate to the role of the Locally Significant Industrial Site. As such, the principle of the proposed commercial development with provision of an uplift in industrial use floorspace is considered to be acceptable, and accords with the spatial strategies of the Development Plan.
- 4.3 The proposed development is considered to be of an acceptable design that is sensitive to its context. The proposal is not considered to cause harm to the character or appearance of the surrounding area. The proposal is considered to comply with the National Planning Policy Framework 2021, Policies D3, D4 and HC1 of the London Plan 2021, policies CS8 and CS9 of the Islington Core Strategy 2011, and policies DM2.1 and DM2.3 of the Development Management Policies 2013. Consideration has also been taken of policies DH1 (Fostering innovation and conserving and enhancing the historic environment) of the Draft Islington Local Plan.
- 4.4 The proposal would also include energy and sustainability measures that comply with the Development Plan's requirements, to ensure that the proposal would maximise energy efficiency and sustainable design of the site where feasible.
- 4.5 There are no nearby residential properties and the proposed development would not result in an unacceptable impact on nearby commercial properties. The daylight/sunlight assessment shows that the neighbouring properties would not be affected by the development.
- 4.6 All other matters related to highways impacts, transport infrastructure, sustainability, ecology, inclusive design, have been assessed and are considered acceptable and in accordance with planning policy.
- 4.7 Overall, the application is considered to be in accordance with the relevant policies within the Development Plan, and is therefore recommended for approval subject to appropriate conditions and planning obligations as set out in Appendix 1 of this report.

5. SITE AND SURROUNDINGS

- 5.1 The application site comprises of two connected buildings at 14-22 Vale Royal and 184 York Way which form a T-shape and adjoin a number of properties. The existing building at 14-22 Vale Royal is approximately 3 storeys in height fronting on to Vale Royal and comprises of a series of pitched roof buildings with bricked elevations and 2no. roller shutters. The building at 184 York Way comprises of an approximately 5 storey building, with a double height ground floor loading bay behind wooden gates with purple clad floors above and a glazed staircase.
- 5.2 The site has frontages on to the southern side of Vale Royal and the eastern side of York Way. The site adjoins the southern and eastern boundaries of the Fitzpatrick Building at 188-194 York Way, located on the corner of Vale Royal and York Way. The site also adjoins the western and southern boundaries of 24-28 Vale Royal. The southern boundary of the site also adjoins a large double height 'shed' structure known as the Henson Freezer Building. The remaining part of the southern boundary of the site is adjacent to the London Overground railway line separated by trees and greenery at the rear of the site, and a number of other railway lines are located to the south.

- 5.3 Vale Royal is a narrow street ranging in width from approximately 5.8m – 8.2m wide. There is limited and, in some areas, no pavement to its northern edge with pedestrians being mostly reliant on the existing, somewhat narrow and fragmented pavement to the south side of the street.
- 5.4 The site is within the Vale Royal/Brewery Road Locally Significant Industrial Site (LSIS). The site is located in a midpoint within the LSIS. It is sandwiched between the railway embankment associated with major railway infrastructure to its southern edge, and Vale Royal to its north with a minor frontage to York Way to the west.
- 5.5 The northern edge of Vale Royal comprises 2 – 4 storey commercial buildings including the elegantly designed Sir Anthony Gormley's artists' studios complex designed by Sir David Chipperfield Architects.
- 5.6 The site is not located within a Conservation Area, nor are there any listed buildings within, or in close proximity to, the application site.
- 5.7 The existing buildings on the site at 14-22 Vale Royal are currently vacant, and at 184 York Way the building is currently being used by Access for self-storage.
- 5.8 The site is located within the Vale Royal/Brewery Road Locally Significant Industrial Site (LSIS), and the Kings Cross & Pentonville Road Key Area (Core Strategy Policy CS6). All sites within the LSIS are subject to a number of Article 4 directions removing permitted development rights for (i) a change of use from office (B1a) to residential (C3) (ii) shops (A1) to financial and professional services (A2), and (iii) a temporary flexible change of use from A1/B1/D1 to A1, A2, A3 or B1.
- 5.9 The site has a PTAL rating (Public Transport Accessibility Level) of 2. The nearest stations are Caledonian Road (Underground), Camden Road (Overground), Caledonian Road & Barnsbury (Overground), Kings Cross (Underground & National Rail) and St Pancras (Underground, National Rail and Eurostar) which are within 1.3km/15 mins walk. Nearby York Way is the northern end of a signposted north-south cycle route to Copenhagen Street.
- 5.10 The site is not affected by any formal heritage constraints, and whilst several buildings on the site and within the LSIS are attractive Victorian warehouse style buildings, there are no immediately adjacent designated heritage assets or conservation areas. Two locally significant views are identified which cross the LSIS, however neither of these pass directly over the site.

6. PROPOSAL

- 6.1 The proposal which relate to the connected buildings at 14-22 Vale Royal and 184 York Way, would involve alterations, extensions and partial change of use to the existing B8 warehouse buildings to create mixed use (Class B8 and flexible E(g)) use buildings arranged over ground, first, second, third, part fourth and part fifth floors and associated plant equipment.
- 6.2 The proposals increase the extent of the existing B8 storage use and also create Flexible E(g)use floorspace for Office (E(g)i), research and development (E(g)ii) or light industrial (E(g)iii) uses. Flexible E(g)use floorspace would be created fronting onto both Vale Royal and York Way.
- 6.3 The proposals will extend the existing warehouse building fronting on to 14-22 Vale Royal by adding two additional floors at fourth and fifth floor levels, adding a level internally at third floor level within the existing roof structure, and alterations to the façade. This would rationalise the arrangements within the existing space within the ground, first, second and third floors under the existing roofs, in addition to the extension at fourth and fifth floors within the central part of the building.

- 6.4 There are minor changes to the height and massing of the existing building at 184 York Way which will see alterations to the façades and internal alterations. The height of the main part of the building remains the same to the York Way frontage. There is a minor increase in height and massing to the stairwell core to the southernmost edge of the site.
- 6.5 It is proposed to undertake the works in two phases, phase 1 comprising of the extensions and alterations to 14-22 Vale Royal, and phase 2 comprising of the façade alterations and internal alterations to 184 York Way.
- 6.6 On the ground floor, the access into the site from both Vale Royal and York Way frontages will be retained. The façade alterations to the Vale Royal frontage create separate access to the self-storage and to the flexible E(g) floorspace with separate reception for each use, and retention of the existing loading bay. Internally the extent of the loading and unloading area is increased to provide better access and suitable parking with four van spaces, four standard bays and a disabled parking space.
- 6.7 At first floor flexible E(g) use work space is created on the Vale Royal frontage and a void above the extended loading bay at ground floor. The remainder of the building footprint at this level is given over to self-storage.
- 6.8 The second floor matches the layout of the floor below without the loading bay void and a slightly larger footprint of flexible E(g) use work space in the both the Vale Royal and York Way buildings.
- 6.9 On the third floor, internally an additional floor is inserted, and part of the pitched roofing has been retained above part of the warehouse, with the rest either being replaced with a new flat roof or being removed to make way for the proposed extension.
- 6.10 The fourth floor accommodates flexible E(g) use workspace and storage space in the existing top floor of the York Way building and in the extended Vale Royal warehouse building. This additional proposed floor to the warehouse contains further self-storage floorspace and Class E(g) floorspace. The parts of the warehouse either side of this roof extension are retained as existing.
- 6.11 The fifth floor follows the layout of the floor below, containing further self-storage floorspace and Class E(g) floorspace. The height of the proposed roof extension matches the height of the existing 184 York Way buildings.
- 6.12 The main roof area of the proposed extension accommodates bio-solar green roof and plant equipment. Due to the building's structural capacity it is not possible to extend over the remaining parts of the Vale Royal building. The extent of the roof extension is shown below in **Image 9**.
- 6.13 On-site servicing, delivery and waste collection vehicles would be able to provide servicing on-site at the ground floor. Cycle facilities are also proposed at ground level providing 32 cycle parking spaces and a total of 6no. accessible cycle parking spaces combining the service yard and long stay spaces. The building's plant machinery is provided at fifth floor level.
- 6.14 The following shows the uplift in the floorspace created by the development:

Use	Existing (sqm GIA)	Proposed (sqm GIA)	Uplift (sqm GIA)
Nil Use	204	0	-204
Class B8	8,731	9,964	+ 1,233
Flexible Class E(g) (i), (ii), (iii)	0	2,848	+2,848
Total	8,935	12,812	+3,877



Image 6 – Proposed Elevation of 14-22 Vale Royal



Image 7 – Proposed view of roof extensions and façade alterations to 14-22 Vale Royal looking west along Vale Royal

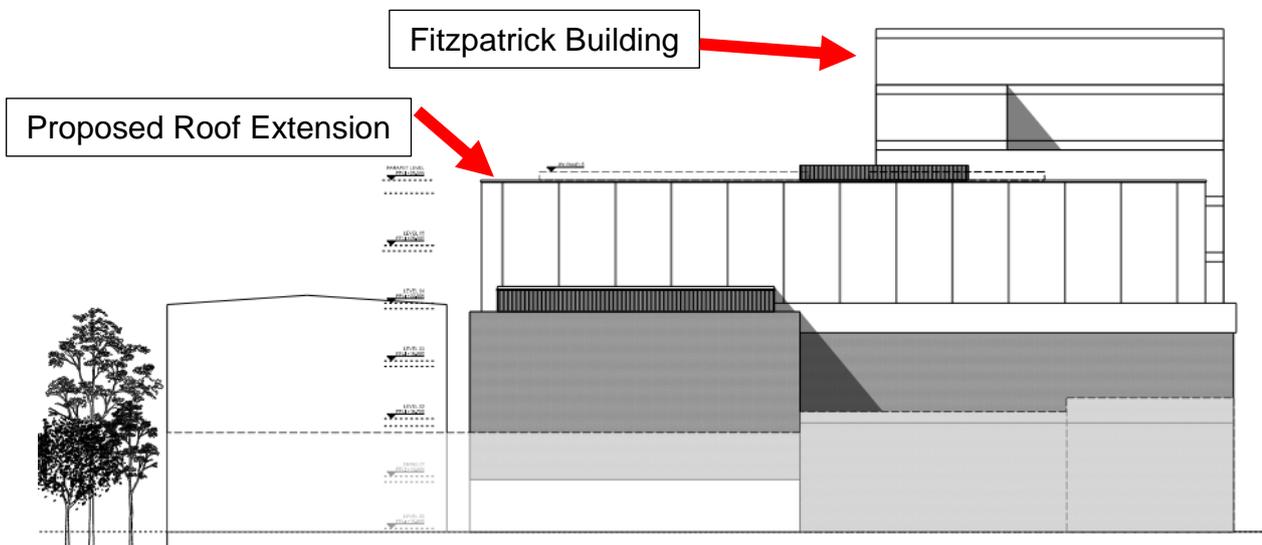


Image 8 – Proposed Eastern Elevation of 14-22 Vale Royal

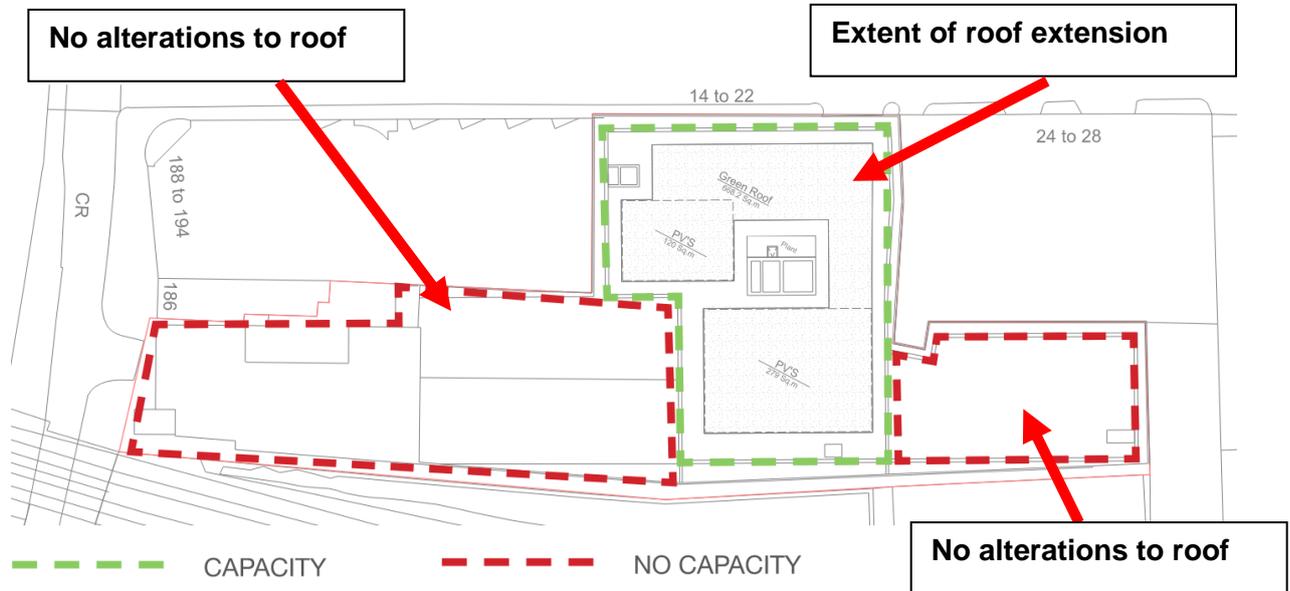


Image 9 – The extent of the proposed roof extension and the building’s structural capacity

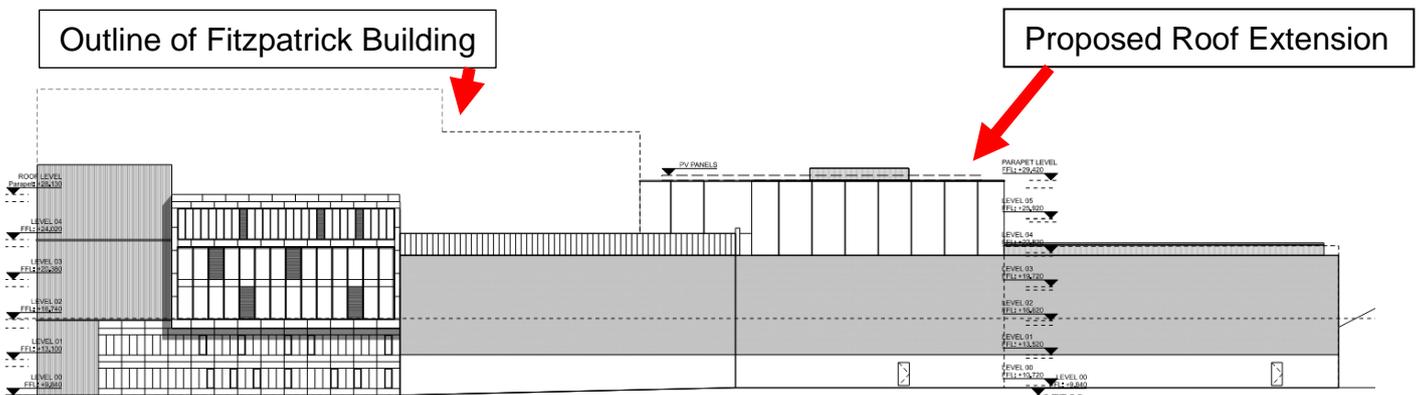


Image 10 – Proposed Southern Elevation of 184 York Way and 14-22 Vale Royal

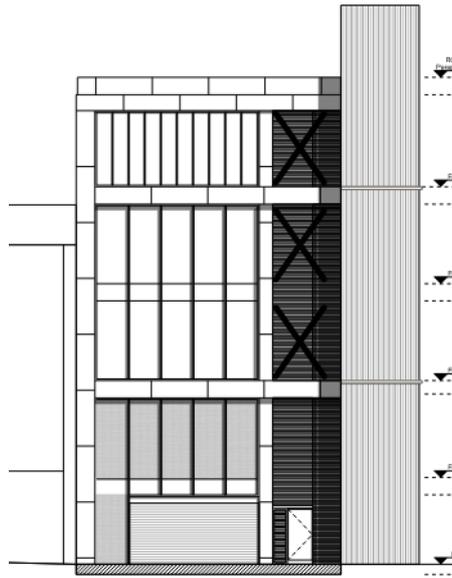


Image 11 – Western Elevation – Proposed frontage to 184 York Way

Revisions:

- 6.15 During the course of the application, additional information has been submitted with regards to the structural capacity of the roof to demonstrate that the extent of bio-solar green roof supporting 131no. PVs has maximised the extent of green roof and an Urban Greening Factor assessment representing the amendments has been submitted.
- 6.16 The applicant has provided additional technical information relating to energy and sustainability to address emerging policy requirements through the submission of a Whole Life Carbon Assessment and Circular Economy Statement.

7. RELEVANT HISTORY:

Application Site

- 7.1 Planning Permission (ref: 840763) for *Use of 204sq.metres on the ground floor as offices for Messrs Abbot Security*. Approved 27/07/1984.

CONDITION: This permission shall operate for the benefit of Messrs Abbott Securities only and shall not ensure for the benefit of the land nor of any other person for the time being having an interest therein.
- 7.2 Planning Permission (ref: 860367) for *Erection of two storey extension to existing warehouse*. Approved 17 November 1986.
- 7.3 Planning Permission (ref: P020012) for *Redevelopment of service yard on York Way frontage to create five storey extension to existing warehouse building*. Approved 13 August 2002.
- 7.4 Advertisement Consent (ref: P2021/1972/ADV) for *Display of internally illuminated fascia signs to west elevation and two internally illuminated totem displays*. Approved 25 October 2021.

- 7.5 Advertisement Consent (ref: P2021/3535/ADV) for *Display of 2 no. internally illuminated fascia signs and 1 no. internally illuminated projecting sign on north and south elevations*. Refused 27 January 2022.

REASON: Due to their overall size, cumulative impact, internal illumination methods and their visual prominence, the proposed signage is considered to have an unacceptable negative impact on the visual appearance on the streetscene and have a negative impact on the amenity of occupiers of adjoining and neighbouring properties. It is therefore contrary to policies DM2.1, DM2.3 and DM2.6 of the Islington Development Management Policies 2013.

Allowed on appeal ref: APP/V5570/Z/22/3295438 dated 20 May 2022.

Neighbouring sites

24-28 Vale Royal

- 7.6 Planning Application (ref: P121813) submitted for the redevelopment of the site involving the erection of a building comprising basement, ground and 5 upper floors to provide 927sqm (GIA) of office floorspace (Class B1), 3,945sqm (GIA) of self-storage floorspace (Class B8), with associated car parking and vehicle loading bays at ground floor level and a new vehicular access from Vale Royal. Refused 09/11/2012.

Dismissed on appeal ref: APP/V5570/A/12/2188520 dated 16 May 2013:

30-40 Vale Royal

- 7.7 Planning Application (ref: P2019/2702/FUL) submitted for Demolition of the existing buildings and the construction of a new part 4, part 6-storey building to provide for 1,982 square metres(GIA) of light industry (Use Class B1(c))/ storage and distribution (Use Class B8) floorspace at ground, mezzanine, first and second floor levels and 3,693 square metres (GIA) of business use floorspace (Use Class B1 (a)/(b)/(c)) at third, fourth and fifth floor levels along with associated access arrangements, plant area, car and cycle parking, refuse storage and ancillary works. [Revised Proposal Description]. Refused on 21/01/2020.

Dismissed on appeal ref: APP/V5570/W/20/3256284 dated 25 March 2021.

25-29 Vale Royal

- 7.8 Planning Permission (ref: P030438) granted for Use of building for B2 and/ or B8, including erection of new gates and fence to front elevation, air conditioning plant to roof. Approved 14/04/2003.

22-23 Tileyard Road

- 7.9 Planning Application (ref: P2018/2355/FUL) submitted for *Demolition of the existing buildings and the construction of a new part 6, part 7, part 8-storey building plus double basement (overall height of 29 metres) to provide for 1,628 square metres(GIA) of light industry floorspace (Use Class B1(c)) at basement and ground floor level along with ancillary café at ground floor level, and 9,111 square metres (GIA) of business use floorspace (Use Class B1 (a)/(b)/(c)), and the construction of a part 6, part 9-storey building plus double basement (overall height of 27.3 metres) consisting of 7,400 square metres (GIA) of self-storage floorspace (Use Class B8) and 870 square metres (GIA) of office floorspace (Use Class B1(a)) along with associated access arrangements, plant area, car and cycle parking, refuse storage and ancillary works.*

Non-determination Appeal Dismissed on 21 August 2019. Ref: APP/V5570/W/19/3224373.

Fitzpatrick Building, 188 - 194 York Way

- 7.10 Planning Permission (ref: P2016/1999/FUL) for *Demolition of the existing office building and redevelopment to provide a part 7/part 8/part 9 storey building to provide office (use class B1a)*

and flexible (Use class B1) floorspace, including basement, ancillary ground floor cafe, cycle parking, plant/storage, landscaping and all other necessary works associated with the development. Approved 30/06/2017.

7.11 13 Vale Royal

Planning Permission (ref: P2016/2706/FUL) for *Refurbishment of existing nightclub 'Egg London', comprising of excavation to create basement; erection of a new single storey extension in existing ground floor garden area and formation of part ground floor mezzanine, first and second floor balcony areas; addition of part third floor extension to replace existing raised second floor flat roof at rear of no. 13 Vale Royal; formation of curved louvre structure and concave motif on Vale Royal elevation; reinstatement of main entrance on Vale Royal and closing of existing York Way entrance; and other associated alterations.* Approved 11/07/2017.

7.12 55 – 61 Brewery Road

Planning Application (ref: P2017/1770/FUL) submitted for demolition of the existing building and the erection of a 6 storey (plus basement) building comprising business and light industrial floorspace. Refused 30 June 2017.

Dismissed on appeal Ref: APP/V5570/W/17/1386143 dated 1 February 2018.

5-10 Brandon Road

7.13 Planning Application (ref: P2019/1856/FUL) submitted for *Demolition of existing buildings and structures and erection of a part 5 and part 6 storey building (with basement) to provide a minimum 2,704sqm (GIA) Use Class B1(c) / B8 floorspace and a maximum 9,983sqm (GIA) flexible Use Class B1 floorspace with service yard, cycle parking, plant, and refuse and recycling facilities.* Refused 11/10/2019.

196-228 York Way

7.14 Planning Permission (ref: P2019/3410/FUL) for *Demolition of the existing buildings and the erection of a part 3 to part 5 storey building plus double basement to provide for a total of 8,268sqm (GIA) of self-storage floorspace (Use Class B8) and 687sqm (GIA) of flexible office floorspace (Use Class B1a) along with associated access arrangements, car and cycle parking, servicing and plant area, refuse storage and ancillary works.* Approved 19 January 2021.

20 Tileyard Road

7.15 Planning Application (ref: P2021/1062/FUL) for *Demolition of the existing building and the erection of a five-storey (plus basement) commercial building comprising industrial (Class E(g)(iii)/B2/B8) and Class E(g) floorspace; and the provision of associated access, cycle parking and refuse storage.* Approved 18 March 2022.

Pre-application Advice:

7.16 The applicant submitted a pre-application advice request (ref: Q2021/1524/MJR) in July 2021 for external and internal alterations of Vale Royal and York Way buildings, extension of Vale Royal buildings to create ground and four upper floors for Class B8 (self-storage) and Class E(g)(i) (office) uses, and Class E(g) uses in York Way building.

7.17 A pre-application meeting was held in July 2021 and the pre-application advice letter was provided in November 2021 following additional information submitted in August and September 2021. Advice was given that the proposed flexible use floorspace (i.e. Class E(g) floorspace) as shown on the plan drawings, would be integral with and support the main use and operation of the site for storage use. As a result, it is understood that the office use would be ancillary to the warehouse business use. On this basis, it would appear that the proposals wouldn't conflict with the aims of the Council's adopted and emerging LSIS land use policies, and should be acceptable, purely in land use terms.

- 7.18 Further advice was given that the principle of extensions and alterations to the existing buildings on site is acceptable. However, support for additional height, scale, bulk and massing is necessarily subject to achieving an acceptable height, mass and form on the site that is contextually responsive. A good standard of architecture is required, particularly given the site's visible location to the long and exposed south-eastern edge, adjacent to a significant expanse of railway infrastructure. The applicant was advised that the extent of the proposed increase in height and mass to the rear (backland) part of the building should be reduced in both height and mass in order to reflect its backland position within the urban structure. This reduction is also required so as not to harm the existing uses – or potential uses – or indeed the redevelopment potential of the site to the front of this module at 24-28 Vale Royal, or the east of it, the site of the Henson Freezer building. As a result, the height of the proposed building, indicated as being 'seven storeys' towards the rear of the site, should be no higher than the York Way frontage height due to the visibility and wider townscape impacts.
- 7.19 Advice was also given that there were issues with the proposed façade alterations as follows:
- Vale Royal:
- 7.20 The materiality and the fenestration shown in the fenestration study is disjointed and unsuccessful. The differences in the seamed metal windows does not provide a harmonious elevation. The applicant is encouraged to create a more harmonious elevation, both in terms of the fenestration and the elevational treatment to the three elements of the elevation I.e. base, middle and top.
- York Way:
- 7.21 The applicant is encouraged to revise the design and materiality to create a more high quality and contextual response that better reflects and capitalises on the qualitative urban transformation underway to York Way and environs.
- 7.22 The initial pre-application advice concluded that the applicant was advised to revise the design of the scheme, taking on board the comments outlined in the design assessment of this response, with particular regard to the height, bulk and massing, elevational treatment and materiality. Further details, information and views would be required as part of a revised follow up submission, in response to these comments, to demonstrate how the proposals achieve an appropriate height, scale, bulk and massing relative to the site's complex constraints.
- 7.23 A revised follow-up pre-application scheme was submitted in December 2021 with a pre-application meeting held in January 2022.
- 7.24 The revised scheme submitted for follow up pre-application advice for alterations, extensions and partial change of use to create mixed use (Classes B8 and E(g)) buildings arranged over ground, first, second, part third, part fourth and part fifth floors and associated plant delivered in two phase
- 7.25 Informal advice was provided that the height, bulk and massing are a major improvement on the earlier iterations and are now considered to be appropriate. Further informal advice was provided relating to the proposed provision of 3,561sqm of flexible E(g) floorspace, raising concerns over the proportion of non-industrial use floorspace in relation to the requirements of the emerging local plan. The provision of Flexible E(g) floorspace has been reduced prior to the submission of the planning application.
- 7.26 Officers engaged in discussions with the applicant on the follow up scheme, however, the follow-up written advice has not been provided in advance of the submission of the current planning application.

8. CONSULTATION

Public Consultation

- 8.1 Letters were sent to occupants of 198 adjoining and nearby properties on Vale Royal, York Way, Tileyard Road, Broadfield Lane on the 16th June 2022.
- 8.2 A site notice and press notice were displayed on 23 June 2022. The public consultation of the application therefore expired on 17 July 2022. However, it is the Council's practice to continue to consider representations made up until the date of a decision.
- 8.3 At the time of the writing of this report three responses had been received from the public with regard to the planning application. The comments made can be summarised as follows:
- References an image on page 50 in the Design and Access statement indicating "Access Self-Storage" signage on the southern elevation towards York Way and cites that this detracts from the proposal and requests that the signage should form a planning condition for further review and the materials should be conditioned. (*Officer comment: The application does not propose signage. A separate application for Advertisement Consent would be required for signage. The submitted proposed elevation drawings do not indicate signage. For the existing site pre-development, signage on the southern elevation has been allowed on appeal ref: APP/V5570/Z/22/3295438 dated 20 May 2022 following the refusal of advertisement consent application ref: P2021/3535/ADV. It is recommended that an informative is attached to any grant of consent confirming that no advertisement consent is granted should these proposals be approved. The details of the proposed materials are to be secured by condition 3.*)
 - Request more information as to what alternations and extension are proposed due to concerns over vibration, dust and noise. (*Officer comment: Officers contacted the customer in July 2022 with a link to the Council's website indicating the application details. No further responses have been received from this customer.*)
 - In July 2022 Antony Gormley's team which own the site opposite the development, initially raised concerns regarding the proposed extension to the loading bay during the construction phase and Access self-storage and HGVs usage of the loading bay, throughout the demolition and construction of the new building, this time consuming frustration will be further amplified to a level that has the potential to seriously disrupt their ability to work from November 2022 to December 2023. However in November 2022 following discussions with the applicant's team, Antony Gormley's team has confirmed that they are satisfied that as long as the applicant agrees to ensure that the scaffolding does not run right up to the corner of the existing loading bay, allowing artic lorries to carry out deliveries, this will maintain the room they need to deliver in and out of their site. (*Officer comment: The concerns raised, related solely to the construction phase of the development and it appears that the applicant has reached an agreement with Antony Gormley's team regarding access arrangements during construction. It is recommended that the final Construction Logistics Plan and Construction Management Plan are secured by condition to ensure that the details are acceptable. See condition 6)*

External Consultees

- 8.4 A number of external statutory and non-statutory consultations were carried out. The following is a summary of the responses received:
- 8.5 **Crossrail 2:** Response received. No objections. (*Officer comment: No conditions are required.*)

8.6 **Network Rail:** Response received. Recommends Approval. Requires the applicant to consult with the Asset Protection Manager at Network Rail, prior to any works commencing on site, and the applicant's agreement to an Asset Protection Agreement. Suggests the following informatives are attached given the proximity of the development to the adjacent railway:

- Environmental pollution (Dust, noise etc.) to the operational railway.
- Proximity of the development to the Network Rail infrastructure and boundary fence and adequate space for future maintenance of the development.
- Encroachment on the boundary fence, interference with sensitive equipment, space for inspection and maintenance of the railway infrastructure.
- Collapse of lifting equipment adjacent to the boundary fence/line.
- Effect of artificial lighting and human factor effects from glare on Train Drivers, Glint/glare from reflective surfaces to signals affecting train movements.
- Effects of development on Biodiversity.
- Potential impact on the adjacent railway infrastructure from the construction activities.
- Collapse of structural temporary works elements on to adjacent Network rail assets and property.
- Ground induced vibration from demolition, general groundworks, causing displacement to impact on NR Track Support Zone.
- Effects of electrical plant or transformers on Network Rail signals or communications systems due to electromagnetic compatibility.

Officer comment: Informatives are attached to meet the requirements of Network Rail.

8.7 **Health & Safety Executive:** Response received. No comment. Building is outside of the scope of a relevant building for review by Health and Safety Executive. *Officer comment: A review of the fire Statement has been undertaken in accordance with the requirements of London Plan D12. See paragraphs 10.243-10.249 and condition 12.*

8.8 **London Fire Brigade:** No response received. *Officer comment: A review of the fire Statement has been undertaken in accordance with the requirements of London Plan D12. See paragraphs 10.243-10.249 and condition 12.*

8.9 **Thames Water:** No objections to the proposal. Thames Water would advise that with regard to the combined waste water network infrastructure capacity, we would not have any objection to the above planning application, based on the information provided, subject to informatives on Waste Water Drainage, proximity to water mains and foul water drainage. *Officer comment: See informatives.*

8.10 **Transport for London:** No objections in principle.

- TfL welcomes that Secure internal cycle storage will be provided in line with the London Plan minimum standards, which a total of 32 standard cycle spaces are provided in the form of Sheffield stands and stacker systems, in addition a further 2 disabled cycle parking spaces and four no. cargo bike spaces within the loading bay are also provided.

Officer comment: Whilst the proposals don't demonstrate full compliance with London Plan policy T5 and Appendix 6 of the Islington Development Management Policies, the applicant's Transport Assessment asserts at sections 4.7 and 4.8:

'A total of 32 cycle parking spaces will be provided within the ground floor of the Vale Royal building. Two of these spaces are positioned to accommodate large cycles such as cargo bikes, tricycles or hand cycles used by disabled people. The provision of cycle parking for the flexible working space in accordance with standards varies between 13 - 42 long stay spaces and 4 and 7 short stay spaces depending on whether office or light industrial parking standards are used. Self-storage facilities generate little demand for cycle parking as they typically employ 2 or 3 staff and customers are generally transporting bulky goods so travel by car or van is the only realistic option. As such a self-storage provision of 4 spaces is usually ample for this use.

The provision of 32 spaces to accommodate long and short stay parking for both elements of the development is considered appropriate given the proposed mix of uses on site. However, use of cycle parking will be monitored and additional stands provided if demand warrants it. The proposed site plan shows where a further 32 bikes could be accommodated within the ground floor of the York Way building.'

Officers recommend that a condition (25) is attached to require details of the monitoring of the cycle usage during the first 6 months following commencement of the use, to be submitted to the Council and the spaces provided should the results of the monitoring indicate an additional demand. Therefore whilst the provision of on-site cycle parking does not meet the London Plan or local plan requirements, consideration is given to the specific nature of the proposed use as self-storage involving the transportation of bulky goods, which would typically necessitate vehicular rather than cycle access. On this basis, the provision of cycle parking as proposed would be acceptable without conflicting with the aims of the relevant London Plan and local plan policies and the requirement for monitoring of the demand is secured by condition (25).

- TfL considers that the proposed parking provision which 10no. vehicle parking spaces including a disabled parking space, is acceptable in light of the proposal's nature. Officer comment: *The provision of parking bays are acceptable due to the self-storage nature of the use requiring the loading and unloading of bulky goods on-site, and therefore do not conflict with the aims of the Council's car free policy.*
- The site is situated in close vicinity to the North London railway, therefore Network Road must be consulted on the construction of the proposal ensuring no adverse impact would be result to the rail infrastructure and London Overground's passenger rail services. Officer comment: *Both Network Rail and London Overground have been consulted and their comments are assessed separately.*
- While the production of a Work Place Travel Plan is welcomed, more assertive targets should be set on walking to increase the proportion of staff who commute by foot where possible. The Final Travel Plan should be secured by condition. Officer comment: *The final travel plan is secured by planning obligation.*
- A Delivery & Servicing Plan should be secured by condition. Officer comment: *See Condition 26.*
- The submission of a Construction Logistics Plan (CLP) is welcomed, TfL requests that the applicant to commit using contractors/ suppliers whom have achieved at least 'Silver Level' accreditation of the FORS scheme to ensure safety and best practice. Officer comment: *See condition (6) requiring the submission of a CLP.*

Subject to the above conditions being met, TfL considers that the proposal is unlikely to give rise to significant highway and transport impact to the Transport for London Road Network and local public transport services.

- 8.11 **London Overground:** No response received.
- 8.12 **Islington Swifts:** Requests that a significant number of integrated swift nestbox bricks reflecting the large scale of this development, be installed near roof level, in accordance with best-practice guidance, which would provide an aesthetically acceptable and zero maintenance way to provide a long-term resource to protect this species and improve the local biodiversity, in line with Islington Council's guidance on this issue (2020-2025 Biodiversity Action Plan and new Local Plan). The CIEEM June 2019 issue of "In Practice" provides best-practice guidance on numbers (<https://cieem.net/resource/the-swift-a-bird-you-need-to-help/>), in line with British Standard BS 42021:2022 for integral nestboxes. Biodiverse roofs, native planting and integrated bat boxes would further enhance biodiversity. *Officer Response: See condition 18.*
- 8.13 **Design out Crime Officer (DOCO):** No response has been received. *Officer Response: The development is required to meet the requirements of Secure By Design Accreditation. See condition 20.*

Internal Consultees

- 8.14 **Planning Policy Officer:** Policy DM5.3 Part A supports the retention and intensification of uses appropriate to the role of the Locally Significant Industrial Site, including B8 use. The principle of intensification of B8 use from 8,731sqm to 9,964sqm (+1,233sqm) is therefore supported.
- 8.15 Policy E4 (part C) of the London Plan states that "the retention, enhancement and provision of additional industrial capacity across the three categories of industrial land set out in Part B should be planned, monitored and managed. Policy E4 Part D also acknowledges situations where retention, enhancement and provision of industrial capacity could be prioritised. London Plan and Local Plan policy is clear that the protection of the industrial function of increasingly diminished industrial land across London is paramount.
- 8.16 The inclusion of an element of E(g) floorspace, in this case 2,848sqm, is acceptable under Part D of Policy DM5.3, however as explained in paragraph 5.22 of Policy DM5.3 the introduction of uses that complement and support the economic role of the LSIS, for example offices, will only be considered acceptable where there is no impact on its primary economic function and the introduction of uses which would have a detrimental impact on the LSIS' economic function and future economic growth will not be allowed. The economic function here is industrial and therefore, the impact of the proposed flexible E(g) floorspace needs to be carefully considered. Whilst the inclusion of new E(g)(i) floorspace is unlikely to impede the primary industrial function of the LSIS on an individual scheme basis, consideration needs to be given to the cumulative impact of E(g)(i) floorspace throughout the LSIS.
- 8.17 The emerging Local Plan policy SP3, part A, is clear that the Vale Royal/Brewery Road Locally Significant Industrial Site (LSIS) will be retained and strengthened as the borough's most significant industrial location.
- 8.18 Supporting this in the emerging local plan is Policy B2 (C), which states that the co-location of industrial use with office and/or research and development uses will be permitted where there would be an intensification of industrial use on the site and it can be demonstrated that the continued industrial function of the LSIS would remain. Although the siting of the E(g) use would be preferably contained to the York Way frontage, the moderate increase in E(g) floorspace is not considered to be demonstrably damaging to the industrial function.

- 8.19 The applicant refers to the E(g) floorspace as flexible workspace. It is true that such workspace could be used in office, research and development or light industrial use, however, it is apparent the applicant intends for this to be used as office floorspace, albeit the justification around the need or relationship between the office and industrial floorspace is lacking. The needs of the different E(g) uses should be considered so that different E(g) uses can occupy the space without significant modification works needed.
- 8.20 In the absence of full weight being able to be applied to the draft Local Plan policies, the proposal from a land use point of view is acceptable.
- 8.21 *Officer comment: The adopted planning policy for the LSIS requires retention of industrial use floorspace and resists the loss of industrial floorspace. The proposals result in the intensification of the existing Class B8 storage use floorspace due to an uplift on the existing provision of B8 floorspace and with an improved offer of accommodation. As a result the proposals exceed the adopted policy requirements and comply with the emerging policy requirements in so far as they result in the intensification of the existing B8 use floorspace. The proposed flexible use floorspace could be used as offices, research and development or light industrial uses. The provision of this floorspace does not conflict with the requirements of the emerging policy in land use terms.*
- 8.22 **Design & Conservation Officer:** No design objections. The proposal sits comfortably within its intensifying context in terms of its height and massing across the site. It also appropriately addresses both adjoining streets that it fronts onto the primary York Way and the tertiary Vale Royal. It creates a legible development that sufficiently annotates the primary entrances and distinguishes between vehicular and pedestrian access which will also enhance safety. The elevations are simple but acceptable for this industrial context with sufficient rhythm and differing use of materials to animate the respective streetscapes. *Officer Response: See paragraphs 10.66 – 10.81 and details of materials are secured by condition 3).*
- 8.23 **Inclusive Design Officer:** The officer initially raised queries relating to provision of accessible parking spaces, accessible cycle parking, provision of electric scooters, entrance/egress, approach, delineation between pedestrian/vehicle spaces within the service yard, accessible WC and shower rooms, and circulation space.
- 8.24 Following responses received from the applicant the officer has confirmed their acceptance of the proposals subject to conditions to secure a small number of outstanding matters and legal agreement regarding accessible parking space.
- Officer Response: See paragraphs 10.86-10.89 and details of the outstanding details to meet the principles of Inclusive Design are secured by conditions 27.*
- 8.25 **Energy Officer:** A number of points were raised in relation to the applicant's energy strategy relating to:
- Calculation of Total (Regulated and Unregulated) Emissions
 - Calculation of a Carbon Offset contribution.
 - The U-values for both the new build and refurbished elements.
 - Details of Thermal Modelling demonstrate that the proposed development is not at risk of overheating and to justify the inclusion of active cooling.
 - Details of futureproofing for connection to a future District Energy Network (DEN) including the requirement to have a communal heating network.

- Details of an investigation into the possibility of forming a shared heating network with neighbouring developments and/or existing buildings.
 - Details of how the 'Be Seen' requirements will be met.
 - Provision of a Draft Green Performance Plan which includes measurable targets for energy consumption, CO2 emissions and water use.
- 8.26 The Energy officer has confirmed that these issues have now been addressed with the exception of a small number of outstanding matters. The officer considers that the inclusion of active cooling has not been justified by the use of thermal modelling of the proposed development without the cooling in place. The officer considers that details of how the proposed development will be futureproofed for connection to a future District Energy Network and the provision of a Draft Green Performance Plan remain outstanding. Officer response: *The application is considered to acceptable subject to condition (14) requiring the outstanding information, and the details of the energy strategy are secured through condition 13.*
- 8.27 **Highways Officer:** No objection to the proposal highways arrangements, during the operation of the development. Raised concerns over the extension of the length of the loading bay during the construction phase of the development. Officer response: *The applicant has subsequently liaised with the owners of the site opposite the development and resolved their objections. Condition 6 is attached requiring the submission of a Construction Logistics Plan and Construction Management Plan prior to commencement of development.*
- 8.28 **Public Protection Officer:** No objections to the proposal subject to the imposition of relevant conditions on plant noise, land contamination and construction management to ensure reference to Islington's Code of Practice for Construction Sites. Officer response: *See conditions attached at 7 and 24.*
- 8.29 **Building Control Officer:** No response received. Officer Response: *An assessment of the revised fire statement has been undertaken against the requirements of the London Plan Policy D12. A revised document has been submitted and this is considered to meet the requirements of the London Plan. See paragraphs 10.243-10.249 and condition 12.*
- 8.30 **Sustainability Officer comments:** No objections to the proposals. The officer raised a number of queries during the course of the application:
- Requests maximisation of green roofs and to retrofit extensive green roofs on existing roof areas where feasible, in addition to providing green roofs on the extension. Officer Response: *The applicant has provided amended plans indicating the provision of a Bio-solar green roof with 131 PVS. These will be secured by condition 17.*
 - Accepts that the development will not be able to achieve a significant reduction in surface water runoff. Officer Response: *This is accepted given the proposal comprises of an extension and alterations to an existing building which occupies the entire footprint of the site.*
 - Requests that the applicant explores incorporating blue roof attenuation storage into the design, combined with the green roofs. Alternatively, if blue roofs are not possible, requests consideration of additional drainage layers under the green roofs. Officer Response: *The applicant has provided a Structural Engineering report stating that: 'A lightweight roofing system is required to minimise the uplift in load.' The structural ability of the roof and would appear to maximise the extent of bio-solar roof based on the constraints of the roof.*
 - Accepts the proposed UGF rating of 0.228 given the ratio of B8 and E uses. The target UGF score of 0.3 does not apply to B8 uses but it does apply to E uses. They have proposed rating is acceptable given the ratio of B8 and E uses, as long as the green roof coverage has been maximised as far as possible. Officer response: *The proposed UGF is acceptable given the*

ratio of B8 and E uses as the general target score of 0.3 does not apply. Details of the Urban Greening Factor are secured through condition 19.

- Requests the scheme incorporate bird boxes, in particular swift bricks as part of the new extension. Officer Response: Details of the Bird and bat boxes are secured by condition 18.
- Requests that the applicant calculate whole life-cycle carbon emissions through a nationally recognised whole life-cycle carbon assessment and demonstrate actions taken to reduce life-cycle carbon emissions. Officer Response: The applicant has provided a Whole Life Carbon Assessment. It is recommended that a condition (29) is attached to secure the outstanding information regarding details of options considered and actions to be taken to reduce Whole Life Carbon emissions with reference to the GLA Guidance. It is also recommended that a condition (30) is attached to require a post-construction Whole Life-Cycle Carbon Assessment in accordance with the GLA guidance.
- Requests that an Adaptive Design Strategy be submitted demonstrating how a circular economy approach has been adopted as part of the building design and construction, and how the building will adapt to change over its lifetime. Officer Response: The applicant has provided a revised Circular Economy Statement. It is recommended that a condition (31) is attached to secure a postconstruction monitoring report should be completed in line with the GLA's Circular Economy Statement Guidance.

8.31 **Ecology Officer**: Response received. No objections. Accepts that there is no need for a further bat survey given that the building was assessed as having “negligible potential”. Requests a condition requiring any lighting scheme to take account of impacts on the nearby SINC. Officer Response: The details of the lighting scheme to protect impacts on the nearby SINC are secured by condition 9.

9. RELEVANT STATUTORY DUTIES & DEVELOPMENT PLAN CONSIDERATIONS AND POLICIES

National Guidance

9.1 Islington Council (Planning Committee), in determining the planning application has the main following statutory duties to perform:

- To have regard to the provisions of the development plan, so far as material to the application and to any other material considerations (Section 70 Town & Country Planning Act 1990);
- To determine the application in accordance with the development plan unless other material considerations indicate otherwise (Section 38(6) of the Planning and Compulsory Purchase Act 2004) (Note: that the relevant Development Plan is the London Plan and Islington's Local Plan, including adopted Supplementary Planning Guidance.)
- As the development is within or adjacent to a conservation area(s), the Council has a statutory duty in that special attention shall be paid to the desirability of preserving or enhancing the character or appearance of the area (s72(1)).

9.2 National Planning Policy Framework 2021 (NPPF): Paragraph 10 states: “at the heart of the NPPF is a presumption in favour of sustainable development.

9.3 The National Planning Policy Framework 2021 seeks to secure positive growth in a way that effectively balances economic, environmental and social progress for this and future generations. The NPPF is a material consideration and has been taken into account as part of the assessment of these proposals

- 9.4 Since March 2014 Planning Practice Guidance for England has been published online.
- 9.5 In considering the planning application account has to be taken of the statutory and policy framework, the documentation accompanying the application, and views of both statutory and non-statutory consultees.
- 9.6 The Human Rights Act 1998 incorporates the key articles of the European Convention on Human Rights into domestic law. These include:
- Article 1 of the First Protocol: Protection of property. Every natural or legal person is entitled to the peaceful enjoyment of his possessions. No one shall be deprived of his possessions except in the public interest and subject to the conditions provided for by law and by the general principles of international law.
 - Article 14: Prohibition of discrimination. The enjoyment of the rights and freedoms set forth in this Convention shall be secured without discrimination on any ground such as sex, race, colour, language, religion, political or other opinion, national or social origin, association with a national minority, property, birth, or other status.
- 9.7 Members of the Planning Committee must be aware of the rights contained in the Convention (particularly those set out above) when making any Planning decisions. However, most Convention rights are not absolute and set out circumstances when an interference with a person's rights is permitted. Any interference with any of the rights contained in the Convention must be sanctioned by law and be aimed at pursuing a legitimate aim and must go no further than is necessary and be proportionate.
- 9.8 The Equality Act 2010 provides protection from discrimination in respect of certain protected characteristics, namely: age, disability, gender reassignment, pregnancy and maternity, race, religion or beliefs and sex and sexual orientation. It places the Council under a legal duty to have due regard to the advancement of equality in the exercise of its powers including planning powers. The Committee must be mindful of this duty inter alia when determining all planning applications. In particular, the Committee must pay due regard to the need to: (1) eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under the Act; (2) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it; and (3) foster good relations between persons who share a relevant protected characteristic and persons who do not share it.
- 9.9 In line with Section 72(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990, in assessing the proposals hereby under consideration, special regard has been given to the desirability of preserving the Conservation Area, its setting and any of its features of special architectural or historic interest.
- 9.10 In line with Section 66(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990, special regard has been given to the desirability of preserving the adjoining listed buildings, their setting and any of their features of special architectural or historic interest.

Development Plan

- 9.11 The Development Plan is comprised of the London Plan 2021, Islington Core Strategy 2011 and the Islington Development Management Policies (2013). The policies of the Development Plan that are considered relevant to this application are listed at Appendix 2 to this report.

Designations

- 9.12 The site has the following designations under the London Plan 2021, Islington Core Strategy 2011, Development Management Policies 2013, and Site Allocations 2013:
- Vale Royal/Brewery Road Locally Significant Industrial Site (LSIS)
 - Kings Cross & Pentonville Road Core Strategy Key Area 6
 - Rail Safeguarding – Crossrail 2
 - Rail Land Ownership – TfL
 - Article 4 Direction A1-A2 (Rest of the borough)
 - Article 4 Direction B1c to C3

Supplementary Planning Guidance (SPG) / Document (SPD)

- 9.13 The SPGs and/or SPDs which are considered relevant are listed in Appendix 2.

Draft Islington Local Plan

- 9.14 The Regulation 19 draft of the Local Plan was approved at Full Council on 27 June 2019 for consultation, with consultation on the Regulation 19 draft taking place from 5 September 2019 to 18 October 2019. The Draft Local Plan was subsequently submitted to the Secretary of State for Independent Examination in February 2020. The Examination Hearings took place between 13 September and 1 October 2021, with consultation on Main Modifications running from 24 June to 30 October 2022.
- 9.15 In line with the NPPF, Local Planning Authorities may give weight to relevant policies in emerging plans according to:
- the stage of preparation of the emerging plan (the more advanced its preparation, the greater the weight that may be given);
 - the extent to which there are unresolved objections to relevant policies (the less significant the unresolved objections, the greater the weight that may be given); and
 - the degree of consistency of the relevant policies in the emerging plan to this Framework (the closer the policies in the emerging plan to the policies in the Framework, the greater the weight that may be given).
- 9.16 Given the advanced stage of the draft Local Plan and the conformity of the emerging policies with the Framework it is considered that policies can be afforded moderate to significant weight depending on the significance of objections to the main modifications.
- 9.17 Emerging policies relevant to this application are set out below:

- Policy SP3: Vale Royal/Brewery Road Locally Significant Industrial Site
- Policy SC3 - Health Impact Assessment
- Policy B1 - Delivering business floorspace
- Policy B2 - New business floorspace
- Policy B3 - Existing business floorspace
- Policy B4 - Affordable workspace
- Policy G3 – New public open space
- Policy G4 – Biodiversity, landscape design and trees
- Policy G5 – Green Roofs and vertical greening
- Policy S1- Delivering sustainable design
- Policy S2 - Sustainable design and construction
- Policy S4 - Minimising greenhouse gas emissions

- Policy S7 – Improving Air Quality
- Policy T2 - Sustainable transport choices
- Policy T4 – Public realm
- Policy T5 - Delivery, servicing and construction
- Policy DH1 - Fostering innovation and conserving and enhancing the historic environment
- Policy DH2 - Heritage assets
- Policy DH3 – Building heights

10. ASSESSMENT

10.1 The main issues arising from this proposal relate to:

- Principle of Development
- Land Use
- Design, Conservation and Heritage
- Accessibility and Inclusive Design
- Neighbouring Amenity
- Energy and Sustainability
- Highways and Transport
- Biodiversity and Landscaping
- Refuse and Recycling
- Fire Safety
- Planning Obligations and CIL

PRINCIPLE OF DEVELOPMENT

10.2 The existing buildings on the site which are currently vacant, comprise of 8,731sqm of B8 storage use and 204sqm of nil use floorspace. There is no objection to extensions and alterations to the existing B8 storage use of the building, in principle land use terms.

10.3 The National Planning Policy Framework has as its economic objective (in paragraph 8) to help build a strong, responsive and competitive economy, by ensuring that sufficient land of the right type is available in the right places and at the right time to support growth, innovation and improved productivity; and by identifying and coordinating the provision of infrastructure. London Plan Policy GG2 states that development proposals should proactively explore the potential to intensify the use of land to support additional workspaces, promoting higher density development, particularly in locations that are well connected to jobs, services, infrastructure and amenities by public transport, walking and cycling.

10.4 The principle of the redevelopment of the site is therefore acceptable and accords with the National Planning Policy Framework's presumption in favour of sustainable development, subject to other material planning considerations discussed below.

LAND USE

10.5 The site is located in the Vale Royal/Brewery Road Locally Significant Industrial Site (LSIS) which is the only designated LSIS in the current Islington Local Plan. The LSIS is the borough's largest concentration of industrial uses and is significant in terms of scale, as well as in terms of its contribution to the local and Central London economies. The LSIS designation recognises the area's important strategic role in providing services for businesses in the Central Activities Zone (CAZ). The boundary of the CAZ is located to the south of the London Overground railway line which abuts the site.

- 10.6 The Vale Royal/Brewery Road Locally Strategic Industrial Site (LSIS), serves a strategic purpose to Central London's businesses and the wider borough economy. Vale Royal/Brewery Road LSIS has a longstanding history of industrial activity dating back to the late 19th Century. It was identified as an Industrial Priority Area in Islington's Development Plan dating back to 1982, where investment on industrial uses were prioritised in the area to support the increasing industrial activity in the area. The current Development Management Policies (DMP, 2013) LSIS designation on the Vale Royal/Brewery Road area was carried over from the Unitary Development Plan (UDP, 2002) which recognised this area as an Industrial and Warehousing Area (IAWA), with policies focusing on safeguarding B2 and B8 uses and land.
- 10.7 The proposal would involve alterations and extensions to the existing B8 storage building resulting in an uplift in the extent of B8 floorspace and the creation of flexible Class E(g) use floorspace.
- 10.8 Whilst there is no express consent authorising a Class B8 storage use of the building, the Planning Permission (ref: 860367) for the erection of a two storey extension to the existing warehouse, approved on 17 November 1986, would indicate that the existing B8 warehouse use has been in use historically and is now likely to be lawful.
- 10.9 The July 1984 permission (840763) was a personal consent for Abbott Securities. The submission states that as a result of Abbott Securities vacation from the building, 204sqm of the ground floor has nil planning use. Officers have received legal advice agreeing that this part of site would have a nil use. When Abbott Securities vacated the building, the permission ceased to be effective as it was personal to them. In the absence of an express condition allowing reversion to the previous lawful use, this effectively means that there is no lawful use which can be carried out on this part of the site (204sqm) without further planning permission and so therefore that part of the site has a nil use.

Use	Existing (sqm GIA)	Proposed (sqm GIA)	Uplift (sqm GIA)
Nil Use	204	0	-204
Class B8	8,731	9,964	+ 1,233
Flexible Use Class E(g)(i) (Office), (ii) (Research and Development), (iii) (Light Industrial)	0	2,848	+ 2,848
Total	8,935	12,812	+3,877

Table 3 - Existing and Proposed (GIA)

Proposed Uses

- 10.10 The proposed development results in 9,964sqm (GIA) of Self-storage (Use Class B8) at part ground to part fifth floor levels and the creation of 2,848 (GIA) of Flexible Class E(g) use floorspace also located at part ground to fifth floor levels.
- 10.11 Therefore the proposed mixed use redevelopment of the building remains B8 storage-led, with 78 percent of the floorspace occupied for a self-storage use and 22 percent occupied by Class

E(g) use floorspace. Therefore, the provision of storage use floorspace is significantly greater than the provision of flexible E(g) use floorspace.

- 10.12 The extensions and alterations to the existing building would result in an uplift of 1,233sqm on the existing B8 use floorspace, in addition to the introduction of 2,848sqm of Flexible E(g) use floorspace.
- 10.13 This amounts to an increase of 14.1% of B8 use floorspace when compared to the existing provision on site. The proposals would therefore bring about the intensification of storage use floorspace on the site and improve the quality of the accommodation.

Land Use Context

- 10.14 The site is located in the Vale Royal/Brewery Road Locally Significant Industrial Site (LSIS) which is the only designated LSIS in the current Islington Local Plan. The LSIS is the borough's largest concentration of industrial uses and is significant in terms of scale, as well as in terms of its contribution to the local and Central London economies. The LSIS designation recognises the area's important strategic role in providing services for businesses in the Central Activities Zone (CAZ).

Adopted Policies

London Plan

- 10.15 The London Plan has been adopted in March 2021, and the relevant policies, relating to Local Significant Industrial Sites continue to apply. Namely, policies E4 (Land for industry, logistics and services to support London's economic function), E6 (Locally Significant Industrial Sites) and E7 (Industrial Intensification, co-location and substitution).
- 10.16 Policy E4 (part A) requires a sufficient supply of land and premises in different parts of London to meet current and future demands for industrial and related functions should be provided and maintained. The policy requires provision for the varied operational requirements including storage and logistics/distribution (Use Class B8) including 'last mile' distribution close to central London and the Northern Isle of Dogs, consolidation centres and collection points.
- 10.17 Policy E4 (part C) seeks the retention, enhancement and provision of additional industrial capacity across the three categories of industrial land which includes LSIS sites. The supporting text of the Policy E4 (para 6.4.5) explains that the floorspace capacity is defined as "either the existing industrial and warehousing floorspace on site or the potential industrial and warehousing floorspace that could be accommodated on site at a 65 per cent plot ratio (whichever is the greater)".
- 10.18 The existing use of the site exceeds the industrial floorspace capacity of 65 per cent plot ratio and the proposed redevelopment would ensure the site continues to exceed the 65 per cent plot ratio by providing 9,964sqm of B8 floorspace, and achieves a plot ratio of 3.4 per cent.
- 10.19 Policy E6 requires London boroughs to designate and define boundaries for Locally Significant Industrial Sites, making clear the range of industrial and related uses acceptable in the LSIS to include, where appropriate, hybrid or flexible space under use classes B1c/B2/B8 that's suitable for SMEs. London Plan encourages all boroughs to explore the potential to intensify industrial activities on industrial land to deliver additional capacity. (para 6.7.1 of the supporting text to policy E7).
- 10.20 Policy E7 instructs both borough local plans and development proposals to be proactive and encourage the intensification of industrial (B1c, B2, B8) uses on all categories on industrial land (meaning SIL, LSIS and non-designated industrial sites) through various measures.

10.21 The latest application is not considered to raise a conflict with the aims of the London Plan 2021, purely in land use terms.

Local Plan

10.22 Islington's Employment Land Study (ELS) (2016) highlights the large losses of industrial land in recent years - 436,000sqm between 2000 and 2012. The recent GLA Industrial Land baseline data demonstrates that the boroughs' stock of industrial land fell from 60ha in 2010 to just 35ha in 2015. Despite this wider context, industrial land within the LSIS has remained relatively stable. The ELS highlights that in the LSIS, severely constrained supply and sustained take-up are combining to maintain rents at a relatively high level. At the time the study estimated the vacancy in the LSIS was between 3-5%, which is lower than the recommended benchmark of 8% 'frictional vacancy', as set out in the SPG.

10.23 Industrial uses play an important role in the borough's economy and help to provide a range of jobs and businesses – a key objective of the borough's Core Strategy. Core Strategy Policy CS13 states that the Vale Royal / Brewery Road LSIS will be retained for industrial / warehousing / employment land.

10.24 Islington Core Strategy Policy CS6 (part D) identifies the Vale Royal/Brewery Road area as the only locally significant concentration of industrial warehousing/employment land in the borough. Policy CS13 (part B) explains that the borough has designated the Vale Royal/Brewery Road LSIS to provide employment space and to retain space that is suitable for industrial/warehousing/employment land. These policies set out the objectives for the Vale Royal/Brewery Road LSIS designation and the appropriate land uses that are suited to industrial and warehousing uses, in line with the primary economic activity of the area; ensuring that new development proposals enhance the area's potential to retain and maximise industrial capacity. Policy CS13(B) sets out that for existing employment space, to protect units which are suitable for SMEs in terms of their type and size.

10.25 Policy DM5.3 is specifically concerned with the Vale Royal/Brewery Road LSIS. Part A of the policy states that the council supports the retention and intensification of uses appropriate to the role of the LSIS (i.e. within the B1(c), B2 and B8 Use Classes). Part B protects the Vale Royal/Brewery Road LSIS from loss or reduction of B1c, B2 and B8 floorspace. Part C states that proposals will be also resisted where they would have a detrimental individual or cumulative impact on the area's primary economic function. Part D resists the introduction of non-business uses (i.e. which fall outside of the B Use Classes) except for services and facilities that are clearly ancillary to, and support the economic and employment function of the Locally Significant Industrial Site.

10.26 The policy supports the retention and intensification of uses in the LSIS within the B1(c), B2 and B8 Use Classes, and proposals which would result in the loss of floorspace for these uses will be refused, as will those which have a detrimental individual or cumulative impact on the area's primary economic function (including by constraining future growth of the primary economic function).

10.27 Supporting text to policy DM5.3 at paragraph 5.22 states:

Proposals for uses that complement and support the economic and employment role of the Locally Significant Industrial Site (for example, cafés or offices) will be considered acceptable where there is no impact on its primary economic function. The introduction of uses which would compromise its economic function and future economic growth (especially residential or student accommodation uses) will not be allowed.

Emerging policy:

- 10.28 Islington's emerging Local Plan policies (Strategic and DM Policies DPD Regulation 19, 2019) and Main Modifications for consultation (June 2022) consider the latest employment and industrial land evidence base both at local and regional levels, in line with national policy. The emerging Local Plan identifies the Vale Royal / Brewery Road LSIS as a strategically important location and recognises the area's contribution to the local and London-wide economies.
- 10.29 Strategic and Development Management Policies (SDMP) policy SP3 focuses on the LSIS.
- 10.30 Part A of the policy seeks to retain and strengthen its role as the borough's most significant industrial location. The principal objective in this area is to retain industrial land and intensify light industrial, B2 general industry and B8 storage and distribution uses, including Sui Generis uses akin to these industrial uses. Light industrial use is now part of Class E and continues to be sought in the LSIS. For proposals involving light industrial floorspace, the Council will use conditions to limit Class E for this specific purpose (consistent with Policy B2: New business floorspace, Part C) and to protect the primary industrial function of the LSIS. To ensure an adequate supply of industrial land and floorspace in Islington industrial uses in the area will be protected in accordance with Policy B3. The encroachment of some non-industrial uses could jeopardise long term sustainability, economic function and future economic growth of the LSIS as an industrial area and will not be allowed.
- 10.31 Part B of the policy encourages the provision of 'hybrid workspace', which may incorporate a mix of the industrial-related uses. Such proposals (including those for refurbishment) should provide a variety of spaces that can accommodate a range of industrial uses, including spaces which are suitable for SMEs.
- 10.32 Part C of the policy notes the LSIS is a successful industrial location which accommodates a wide range of operators, including some existing office use. Proposals for the co-location of industrial use with office and/or research development use will be permitted, where there would be an intensification of industrial use on the site (either through new floorspace or the redevelopment/modernisation of existing floorspace) and it can be demonstrated that the continued industrial function of the LSIS would remain.
- 10.33 Emerging SDMP policy B2 part C reflects the requirements of SP3 and states that in the LSIS, the overriding priority land uses are light industrial, B2 general industrial, B8 storage and distribution and Sui Generis uses which are akin to industrial uses). The retention and intensification of industrial uses will be required as part of any proposal within a LSIS. The light industrial element of Class E continues to be sought in the LSIS and will be secured through condition. The provision of hybrid space is supported. The co-location of industrial uses with office and/or research and development uses will be permitted where there would be an intensification of industrial use on the site and it can be demonstrated that the continued industrial function of the LSIS would remain. The introduction of some non-industrial uses could undermine the primary industrial economic function of LSISs and will therefore only be allowed where proposals comply with the co-location criteria as set out above and/or policy B3: Existing business floorspace, Part D, where relevant.
- 10.34 Policy B2 part G requires all development proposals involving business floorspace to:
- i) Have regard to flexibility for a range of occupiers and provide a range of unit types and sizes
 - ii) Provide a good level of amenity for occupiers, including adequate levels of daylight and sunlight and access to communal/ancillary facilities
 - iii) Incorporate the highest inclusive design standards achievable in context
 - iv) Demonstrate how the proposals individually and cumulatively contribute to providing the range of spaces required to support the primary function/sector of the area in which the site is located.

Proposed Uses Assessment

- 10.35 The proposals result in the retention and intensification of the existing B8 storage use on the site, and therefore accord with the overarching aims of the adopted LSIS development plan policy DM5.3. The introduction of Flexible Class E(g) use floorspace enables the provision of a range of employment uses comprising of office, research and development or light industrial uses. This floorspace is in addition to the intensification of the B8 storage use and all uses fall within the B Use Classes, and are therefore considered to support the economic and employment function of the LSIS. As a result, the proposed uses are considered to accord with the aims of the adopted development plan policy, purely in land use terms.
- 10.36 Officers note the aims of the emerging LSIS policy is also to retain and intensify industrial land and in this regard, the proposals are compliant. The emerging policy does seek to resist the encroachment of non-industrial uses, but it also encourages the provision of 'hybrid workspace', which may incorporate a mix of the industrial-related uses. The policy would also permit the co-location of industrial and storage use with office and/or research development use, where there would be an intensification of B8 storage use on the site. This is achieved by the proposals. The design and layout of the proposed floorspace is considered to meet the aims of emerging Policy B2 part G.
- 10.37 The emerging policy also requires demonstration that the continued industrial function of the LSIS would remain. Officers note that the extent of the proposed flexible Class E(g) floorspace (22 percent) would be ancillary to the B8 storage use floorspace (78 percent) within the scheme and would amount to 70 percent of the proposed floorspace. The proposed intensification of the B8 use floorspace accords with the emerging policy requirements. The extent of flexible Class E(g) uses is secured by condition (10).

SMEs and Affordable Workspace

Adopted Policy

- 10.38 The Core Strategy (2011) highlights the important role that micro and small enterprises play in Islington and how the provision of affordable workspace from new developments helps to accommodate small businesses in the borough (para 3.4.12).
- 10.39 Development Management Policy DM5.4 (part A) requires major development proposals for employment floorspace located within Employment Growth Areas and Town Centres to incorporate an appropriate amount of affordable workspace and/or workspace suitable for occupation by micro and small enterprises.

Emerging Policy

- 10.40 Supporting text paragraph 2.32 within the Schedule of main modifications to the SDMP document sets out:

Office and research and development floorspace will only be considered if they are included as part of a scheme where it can be clearly demonstrated that there would be an intensification of industrial floorspace and the continued industrial function of the LSIS would remain. Non-industrial floorspace as part of any proposals will be required to provide affordable workspace in line with policy B4, part B. Other non-industrial uses which are not considered ancillary to the development could jeopardise the long term sustainability, economic function and future economic growth of the LSIS as an industrial area and will not be allowed

- 10.41 Emerging Policy B4 (B) sets out

B. Within the Vale Royal/Brewery Road Locally Significant Industrial Site (LSIS) and other LSISs, major development proposals involving 3,000sqm or more net additional office and/or research

and development and/or a Sui Generis use akin to office/research and development floorspace must incorporate 10% affordable workspace (as a proportion of proposed office and/or research and development and/or a Sui Generis use akin to office/research and development floorspace GIA) to be leased to the Council at a peppercorn rent for a period of 20 years.

Assessment – Adopted Development Plan

- 10.42 Within the adopted development plan, the requirement for the provision of affordable workspace within major development proposals for employment floorspace, is limited to locations within Employment Growth Areas and Town Centres. Therefore, there is no adopted development plan policy requirement for the provision of Affordable Workspace within this application as it is located within the LSIS. The application does not propose to provide Affordable Workspace as part of the scheme.

Assessment – Emerging Local Plan

- 10.43 The Council is in the process of adopting its emerging new local plan. Currently the Council consulted on modifications to the Local Plan until 30 October 2022.
- 10.44 The proposed provision of 2,848sqm of Flexible Class E(g) floorspace does not engage the requirements of emerging local plan policy B4(B) as it does not exceed 3,00sqm. Therefore there is no requirement for the provision of affordable workspace as part of the scheme.

Class E restrictions

- 10.45 The Town and Country Planning (Use Classes) Regulations were amended on 1st September 2020. The amended Use Class regulations omit the former Use Class B1 and introduces a new Use Class E, which encompasses office use, together with many other town centre use. The application proposes the introduction of Flexible Class E(g) floorspace enabling the use as E(g)(i) Offices, (ii) Research and Development, or as (iii) Light Industrial use.
- 10.46 It is considered that the uses that are encompassed by Class E, would not be an appropriate use of the development given the site's LSIS designation and the focus on industrial uses, without the submission of further details and mitigation measures. As such, it is considered that the proposed Flexible E(g) use floorspace should be restricted from converting to other uses outside of Use Class MA.
- 10.47 As such, conditions (10 and 11) are recommended restricting the use of the Flexible Class E(g) use floorspace, and no other use within Class E of the Use Classes Order 2020. Should any other use be proposed to the building, this would require the submission of an application and appropriate supporting documentation.

Land Use Summary

- 10.48 The site is located within the designated Locally Significant Industrial Site. As such, the principle of its proposed redevelopment with the provision of additional storage B8 use floorspace, is considered to be acceptable, and accords with the aims of the spatial strategies of the Development Plan. The introduction of Flexible Class E(g) use floorspace is also acceptable for the reasons outlined above, provided it is controlled by condition 10. Officers consider that the proposed development, would positively contribute to the industrial character of the LSIS and support the delivery of storage use floorspace.

DESIGN, CONSERVATION AND HERITAGE

Policy context

10.49 The NPPF (2021) addresses the determination of planning applications affecting designated and non-designated heritage assets at paragraphs 194-203 which state, inter alia, that:

'In determining applications, local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on their significance. As a minimum the relevant historic environment record should have been consulted and the heritage assets assessed using appropriate expertise where necessary...'

10.50 Local Planning Authorities should identify and assess the particular significance of any heritage asset that may be affected by a proposal (including by development affecting the setting of a heritage asset) taking account of the available evidence and any necessary expertise. They should take this assessment into account when considering the impact of a proposal on a heritage asset, to avoid or minimise conflict between the heritage asset's conservation and any aspect of the proposal...'

10.51 Paragraph 126 highlights that the creation of high-quality buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities.

10.52 Paragraph 132 states that design quality should be considered throughout the evolution and assessment of individual proposals. Early discussion between applicants, the Local Planning Authority and local community about the design and style of emerging schemes is important for clarifying expectations and reconciling local and commercial interests. Applicants should work closely with those affected by their proposals to evolve designs that take account of the views of the community. Applications that can demonstrate early, proactive and effective engagement with the community should be looked on more favourably than those that cannot.

London Plan

10.53 Policy D3 of the London Plan states that development must make the best use of land by following a design-led approach that optimises the capacity of sites, to ensure that development is of the most appropriate form and land use for the site. The design-led approach requires consideration of design options to determine the most appropriate form of development that responds to a site's context and capacity for growth. It further states that higher density developments should generally be promoted in locations that are well connected to jobs, services, infrastructure and amenities by public transport, walking and cycling.

10.54 In terms of design and heritage considerations, London Plan policy D3, part D states that development proposals should:

- enhance local context by delivering buildings and spaces that positively respond to local distinctiveness through their layout, orientation, scale, appearance and shape, with due regard to existing and emerging street hierarchy, building types, forms and proportions;
- provide active frontages and positive reciprocal relationships between what happens inside the buildings and outside in the public realm to generate liveliness and interest;
- respond to the existing character of a place by identifying the special and valued features and characteristics that are unique to the locality and respect, enhance and utilise the heritage assets and architectural features that contribute towards the local character;

- be of high quality, with architecture that pays attention to detail, and gives thorough consideration to the practicality of use, flexibility, safety and building lifespan through appropriate construction methods and the use of attractive, robust materials which weather and mature well.

10.55 Policy D4 stipulates the importance of design scrutiny of development proposals starting from pre-application stage. It states that the design of development proposals should be thoroughly scrutinised by borough planning, urban design, and conservation officers, utilising analytical tools, local evidence, and expert advice where appropriate. In addition, boroughs and applicants should make use of the design review process to assess and inform design options early in the planning process.

10.56 Policy HC1 reads that development proposals affecting heritage assets, and their settings, should conserve their significance, by being sympathetic to the assets' significance and appreciation within their surroundings. The cumulative impacts of incremental change from development on heritage assets and their settings should also be actively managed. Development proposals should avoid harm and identify enhancement opportunities by integrating heritage considerations early on in this design process.

Local Plan

10.57 Policy CS8 of the Islington Core Strategy sets out the general principles to be followed by new development in the borough. Policy CS9 (Protecting and enhancing Islington's built and historic environment) requires the borough's unique character to be protected by preserving the historic urban fabric, and new buildings should be sympathetic in scale and appearance and to be complementary to the local identity.

10.58 Policy DM2.1 (Design) of the Islington Development Management Policies requires all forms of development to be of a high quality design, incorporate inclusive design principles and make a positive contribution to the local character and distinctiveness of an area, based upon an understanding and evaluation of its defining characteristics. Permission will be refused for development of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions.

10.59 Moreover, the Islington's Urban Design Guide SPD (UDG) sets out the principles of high quality design (Contextual, Connected, Sustainable and Inclusive) and the detailed design guidance such as urban structure, the streetscape, services and facilities, and shopfront design.

Existing Site and Context

10.60 The existing site has a small but prominent frontage to York Way where it forms the end site within the LSIS, adjacent to the overhead railway. It has a long 'body' which connects to the frontage of 14 – 22 Vale Royal, a narrow east to west tertiary route within the industrial estate.

10.61 The existing 14-22 Vale Royal building consists of a 3-storey warehouse. The first two floors have an overhang at a height of 5.5m. The eaves height of the windowless second floor rises from approximately 10.7m on the western boundary to 11.6m on the eastern boundary. The current warehouse building benefits from a goods lift, a passenger lift and a large loading bay fronting Vale Royal. Attached to the Vale Royal warehouse is the 5 storey York Way building with its large double height loading bay onto this primary street.

10.62 The site is adjoined by the former Fitzpatrick Building located on the corner of Vale Royal and York Way which has recently been completed as the Rollingstock Yard building comprising of a relatively tall commercial building, being part 7, 8 & 9 storeys, with a maximum height just below 30m. It is

prominently located onto York Way with a return to Vale Royal, and lies opposite a 19 storey residential tower to the western edge of York Way.

- 10.63 Vale Royal is a narrow street ranging in width from approximately 5.8m to 8.2m wide. There is limited and in some areas no pavement to its northern edge with the pedestrian being mostly reliant on the existing and somewhat narrow and fragmented pavement to the south side of the street.
- 10.64 The northern edge of Vale Royal comprises 2 – 4 storey commercial buildings including the elegantly designed artists' studios complex for Sir Anthony Gormley by Sir David Chipperfield Architects.

Assessment of proposals

Principle of works

- 10.65 The proposal is to retain much of the existing buildings and to fully refurbish them while adding some additional storeys to the Vale Royal frontage and to part of the rear and to create new roof structures. It also includes new façades to the existing buildings, improving their overall appearance and positively contributing to the characteristics of the LSIS.

Height, bulk and mass

14-22 Vale Royal

- 10.66 It is proposed to extend the height of part of the building by two storeys at fourth and fifth floor level by increasing the height from 11m to 18.7m. This increase in massing is located centrally on the building when viewed from Vale Royal. The additional height and massing would remain subservient to the adjoining Fitzpatrick Building which rises to part 7, part 8 and part 9 storeys on the corner with York Way. See **Images 6, 7 and 8**. Therefore, the height of the proposed roof extension on Vale Royal, the narrowest street within the LSIS, at 18.7m is considered appropriate and acceptable.
- 10.67 The proposed increase in heights is most apparent and impactful as applied to the Vale Royal elevation. This has been appropriately designed when viewed from Vale Royal, by setting back the proposed top two floors to match the existing recessed ground (and 1st) floor plane and leaving the middle floors with their existing forward projection. See **Image 7** which shows the profile of the façade as viewed looking west along Vale Royal.
- 10.68 As a result of the details and considered design of the frontage, the impact of the increase in height to the narrow street has been effectively mitigated and remains appropriate in scale to the more prominent Fitzpatrick Building fronting onto York Way.
- 10.69 The eastern most module is sandwiched between the rear of 24-28 Vale Royal to its north and the Hansen Freezer building with the railway line beyond to the south. It is proposed to increase the height only minimally in this location to 13m, primarily to accommodate a different roof form – changing from a valley form to that of a parapeted form. Given the contextual characteristics of this part of the site, this too is considered acceptable and is unlikely to be of such scale and form to jeopardise possible future changes to these adjacent sites.

184 York Way

- 10.70 The height of the building at 184 York Way remains largely the same to the York Way frontage at some 18.3m, save for an increase in height to the stairwell core to the southernmost edge. This is proposed to be increased in height to just over 20m. This minor increase in height and massing to the southernmost part of the building is acceptable given the scale and primacy of York Way. Furthermore, it helps to animate the roofscape of the building as viewed from York Way where it reads as a 'pop up' element, and its increase in height to the return railway edge has no harmful

impacts given the nature and function of this adjacency. This increased height and massing continues for part of the southern return elevation to the railway line after which it drops down to the predominant 18.3m storey height ambient, culminating in the final eastern most module dropping still further in height down to 14m. This proposed minor increase in height and massing is therefore acceptable in design and townscape terms.

Elevational treatment

14-22 Vale Royal

- 10.71 The elevation to Vale Royal has a very immediate relationship with this narrow street. It too has been appropriately considered and reads as three distinct elements – base, middle and top. Each are of equal height, but their architectural detailing and materiality differs considerably coupled with the projection of the middle element which creates an interesting profile to the street.
- 10.72 The base is successfully robust and well animated with its generous fenestration openings, legible entrances for both pedestrian and vehicles, and a further visual enrichment through the application of blue coloured glazed brick piers.
- 10.73 The middle element too is glazed with generous modules set between robust brick piers, emphasising some verticality to the building.
- 10.74 The top module is recessed back to the line of the ground floor plane and contains smaller glazing modules set within a quiet powder coated aluminium ‘frame’. This creates a visually subservient element that is appropriate at upper floor level.
- 10.75 Therefore the treatment of the proposed Vale Royal façade forms an appropriate and contextual response to the streetscene.

184 York Way

- 10.76 The base module to 184 York Way has been rhythmically subdivided and enclosed thereby removing an unsightly gaping service yard to the street edge. It presents a well-structured façade that includes a tall slender element to the southern edge that contains the stairwell, with the top floor element of this core coloured blue to echo the applicant’s branding colour. The changes to the existing western elevation to York Way are therefore considered an enhancement on the existing and are supported.
- 10.77 The long southern return to the railway line is quite visible from some medium range vantage points. It has been well designed with a clearly demarcated façade treatment providing a legible top, middle and base. As the elevation stretches eastwards, it lowers in height and becomes less visible. It then changes its façade language to one of far greater solidity with a small clearstorey element to the top. This is entirely appropriate given the conditions and constraints of the context in this location and is therefore acceptable.

Impact on heritage assets

- 10.78 There are no adjacent heritage assets and therefore there are no impacts of concern.

Design Conclusion

- 10.79 The proposed roof extension to 14-22 Vale Royal sits comfortably within its intensifying context in terms of its height and massing across the site and remains subservient to the Fitzpatrick Building which fronts onto the more prominent York Way. The proposed design and elevational treatment appropriately address both frontages, the primary York Way and the tertiary Vale Royal. The

proposals create a legible development that sufficiently annotates the primary entrances and distinguishes between vehicular and pedestrian access which will also enhance safety.

- 10.80 The elevations are simple but acceptable given the industrial context with sufficient rhythm and differing use of materials to animate the respective streetscapes. The proposals are therefore considered to be acceptable within the intensifying context of the LSIS subject to condition (3) requiring details of the facing materials to be submitted and approved.
- 10.81 Therefore, the proposed development complies with the National Planning Policy Framework 2021, policies D3, D4 and HC1 of the London Plan 2021, policies CS8 and CS9 of the Islington Core Strategy 2011, and policies DM2.1 and DM2.3 of the Development Management Policies 2013. The development also adheres to the guidance in the Islington Urban Design Guide 2017.
- 10.82 Consideration has also been given to the requirements of emerging local plan policies DH1 (Fostering innovation and conserving and enhancing the historic environment) and DH2 (Heritage assets). The proposed development is considered to be of high quality which would contribute to the character of this important part of the borough.

INCLUSIVE DESIGN

- 10.83 Policy GG1 of the London Plan 2021 requires that development must support and promote the creation of a London where all Londoners, including children and young people, older people, disabled people, and people with young children, as well as people with other protected characteristics, can move around with ease and enjoy the opportunities the city provides. Further, it supports and promote the creation of an inclusive London where all Londoners can share in its prosperity, culture and community, minimising the barriers, challenges and inequalities they face.
- 10.84 The Inclusive Design principles are set out within policy D5 of the London Plan which states that development proposals should achieve the highest standards of accessible and inclusive design. It should:
1. be designed taking into account London's diverse population;
 2. provide high quality people focused spaces that are designed to facilitate social interaction and inclusion;
 3. be convenient and welcoming with no disabling barriers, providing independent access without additional undue effort, separation or special treatment;
 4. be able to be entered, used and exited safely, easily and with dignity for all 5) be designed to incorporate safe and dignified emergency evacuation for all building users. In all developments where lifts are installed, as a minimum at least one lift per core (or more subject to capacity assessments) should be a suitably sized fire evacuation lift suitable to be used to evacuate people who require level access from the building.
- 10.85 At a local level, Islington's Development Management Policy DM2.2 requires all new developments to demonstrate that they: i) provide for ease of and versatility in use; ii) deliver safe, legible and logical environments; iii) produce places and spaces that are convenient and enjoyable to use for everyone, and iv) bring together the design and management of a development from the outset and over its lifetime
- 10.86 The Council's Inclusive Design SPD further sets out detailed guidelines for the appropriate design and layout of existing and proposed new buildings.

- 10.87 The Council's Inclusive Design Officer has been consulted on the inclusive design principles of the proposal. The officer initially raised queries relating to the provision of accessible parking spaces, accessible cycle parking, provision of electric scooters, entrance/egress, approach, delineation between pedestrian/vehicle spaces within the service yard, accessible WC and shower rooms, and circulation space.
- 10.88 Following responses received from the applicant, the officer has confirmed their acceptance of the proposals subject to condition (27) to secure points of clarification to comply with the principles of Inclusive Design.
- 10.89 Overall, the proposed development is welcomed in terms of accessible design and is a significant improvement upon the existing building and its layout.
- 10.90 In conclusion, the proposed development would comply with the relevant policies in delivering an inclusive environment that is safe, convenient and inclusive for all future users.

NEIGHBOURING AMENITY

- 10.91 Paragraph 130 of the National Planning Policy Framework (July 2021) states that planning decisions should ensure that developments would have a high standard of amenity for existing and future users. All new developments are subject to an assessment of their impact on neighbouring amenity in terms of loss of daylight, sunlight, privacy and an increased sense of enclosure. A development's likely impact in terms of air quality, dust, safety, security, noise and disturbance is also assessed.
- 10.92 Part D of policy D3 of the London Plan (2021) states that development proposals should deliver appropriate outlook, privacy and amenity, the design of the development should also help prevent or mitigate the impacts of noise and poor air quality.
- 10.93 Policy DM2.1 of the Development Management Policies Document (2013) identifies that satisfactory consideration shall be given to noise and the impact of disturbance, vibration, as well as overshadowing, overlooking, privacy, direct sunlight and daylight receipt, over-dominance, sense of enclosure and outlook.
- 10.94 All the immediate neighbouring properties are in commercial occupancy and thus impacts on them would not result in a loss of residential amenity. Noting there are residential properties on the western side of York Way, there is no change to the built form adjacent to York Way, with changes to height and massing located adjacent to Vale Royal and away from the York Way frontage.
- 10.95 Given the location of the building and the extent of the development, it is considered that the Antony Gormley Studio at 15-23 Vale Royal has the potential to be impacted given the reasonable expectation of daylight due to the land use of this site as an artist's studio. The daylight impacts on the non-residential buildings at 186 York Way and the Fitzpatrick Building at 188-194 York Way, both of which adjoin part of the site, have also been assessed.

Daylight, Sunlight and Overshadowing

- 10.96 To assess the sunlight and daylight impact of new development on existing buildings, Building Research Establishment (BRE) criteria is adopted. In accordance with both local and national policies, consideration has to be given to the context of the site, the more efficient and effective use of valuable urban land and the degree of material impact on neighbours.
- 10.97 The starting point must be an assessment against the BRE guidelines and from there a real understanding of impacts can be gained. Knowing very clearly what the actual impacts are in the

first instance is consistent with the judgement made in 'Rainbird vs Tower Hamlets [2018]'. Once the transgressions against the BRE guidelines are highlighted, consideration of other matters can take place.

10.98 The 'Effective Use of Land' section in the Government's Planning Practice Guidance (PPG), confirms that consideration is to be given to whether a proposed development would have an unreasonable impact on the daylight and sunlight levels enjoyed by neighbouring occupiers, setting out that all development should maintain acceptable living standards, although what will be appropriate will depend to some extent on the context. The Guidance cites city centre locations where tall modern buildings predominate as an area where lower daylight levels at some windows may be appropriate if new development is to be in keeping with the general form of its surroundings.

BRE Guidance: Daylight to existing buildings

10.99 The BRE Guidelines stipulate that... "the diffuse daylighting of the existing building may be adversely affected if either:

- The VSC [Vertical Sky Component] measured at the centre of an existing main window is less than 27%, and less than 0.8 times its former value; and
- The area of the working plane in a room which can receive direct skylight is reduced to less than 0.8 times its former value." (No Sky Line / Daylight Distribution).

10.100 At paragraph 2.2.7 of the BRE Guidelines it states: "*If this VSC is greater than 27% then enough skylight should still be reaching the window of the existing building... any reduction below this level should be kept to a minimum. If the VSC, with the development in place is both less than 27% and less than 0.8 times its former value, occupants of the existing building will notice the reduction in the amount of skylight. The area of lit by the window is likely to appear more gloomy, and electric lighting will be needed more of the time.*" The BRE Guidelines state (paragraph 2.1.4) that the maximum VSC value is almost 40% for a completely unobstructed vertical wall.

10.101 At paragraph 2.2.10 the BRE Guidelines state: "*Where room layouts are known, the impact on the daylighting distribution in the existing building can be found by plotting the 'no sky line' in each of the main rooms. For houses this would include living rooms, dining rooms and kitchens. Bedrooms should also be analysed although they are less important... The no sky line divides points on the working plane which can and cannot see the sky... Areas beyond the no sky line, since they receive no direct daylight, usually look dark and gloomy compared with the rest of the room, however bright it is outside*".

10.102 Paragraph 2.2.13 states: "*Existing windows with balconies above them typically receive less daylight. Because the balcony cuts out light from the top part of the sky, even a modest obstruction may result in a large relative impact on the VSC, and on the area receiving direct skylight.*" The paragraph goes on to recommend the testing of VSC with and without the balconies in place to test if it the development or the balcony itself causing the most significant impact.

10.103 The BRE Guidelines at Appendix F give advice on setting alternative target values for access to skylight and sunlight. Appendix F states that the numerical targets widely given are purely advisory and different targets may be used based on the special requirements of the proposed development or its location. An example given is "in a mews development within a historic city centre where a typical obstruction angle from ground floor window level might be close to 40 degrees. This would correspond to a VSC of 18% which could be used as a target value for development in that street if new development is to match the existing layout".

BRE Guidance: Sunlight to existing buildings

10.104 The BRE Guidelines (2022) state in relation to sunlight at paragraph 3.2.11: “If a living room of an existing dwelling has a main window facing within 90 degrees of due south, and any part of a new development subtends an angle of more than 25 degrees to the horizontal measured from the centre of the window in a vertical section perpendicular to the window, then the sunlighting of the existing dwelling may be adversely affected”.

10.105 This will be the case if the centre of the window:

- Receives less than 25% of annual probable sunlight hours, or less than 5% of annual probable sunlight hours during the winter months between 21 September and 21 March and;
- Receives less than 0.8 times its former sunlight hours during either period and;
- Has a reduction in sunlight received over the whole year greater than 4% of annual probable sunlight hours.

10.106 The BRE Guidelines state at paragraph 3.1.6 in relation to orientation:

“A south-facing window will, receive most sunlight, while a north-facing one will only receive it on a handful of occasions (early morning and late evening in summer). East and west-facing windows will receive sunlight only at certain times of the day. A dwelling with no main window wall within 90 degrees of due south is likely to be perceived as insufficiently sunlit.”

10.107 The guidelines go on to state (paragraph 3.2.3):

“... it is suggested that all main living rooms of dwellings, and conservatories, should be checked if they have a window facing within 90 degrees of due south. Kitchens and bedrooms are less important, although care should be taken not to block too much sun”

10.108 Where these guidelines are exceeded then sunlighting and/or daylighting may be adversely affected. The BRE Guidelines provide numerical guidelines, the document though emphasises that advice given is not mandatory and the guide should not be seen as an instrument of planning policy, these (numerical guidelines) are to be interpreted flexibly since natural lighting is only one of many factors in site layout design.

BRE Guidance: Overshadowing

10.109 The BRE Guidelines state that it is good practice to check the sunlighting of open spaces where it will be required and would normally include: gardens to existing buildings (usually the back garden of a house), parks and playing fields and children’s playgrounds, outdoor swimming pools and paddling pools, sitting out areas such as those between non-domestic buildings and in public squares, focal points for views such as a group of monuments or fountains.

10.110 At paragraph 3.3.17 it states: “It is recommended that for it to appear adequately sunlit throughout the year, at least half of a garden or amenity area should receive at least two hours of sunlight on 21 March. If as a result of new development an existing garden or amenity area does not meet the above, and the area which can receive two hours of sun on 21 March is less than 0.8 times its former value, then the loss of sunlight is likely to be noticeable. If a detailed calculation cannot be carried out, it is recommended that the centre of the area should receive at least two hours of sunlight on 21 March.”

Assessment

- 10.111 No 186 York Way adjoins the northern elevation of no. 184 York Way. The ground floor of the property has most recently been used as an estate agents with previous uses including a taxi booking company. Historic decisions indicate ancillary storage and offices have been permitted on the upper floors of this property.
- 10.112 The Fitzpatrick Building at 188-194 York Way, is a part 7/part 8/part 9 storey modern office development consented in 2017 and recently completed.
- 10.113 No. 15-23 Vale Royal is located to the north of the site and is the studio of the artist Sir Anthony Gormley. The studio receives daylight from rooflights in its saw-tooth roof. The BRE Guidelines can also be applied to non-domestic buildings where occupants have a reasonable expectation of daylight, which would include artists' studios. This site has been assessed regards to the impact of the proposals on daylight.
- 10.114 The applicant has submitted a Daylight and Sunlight report prepared by Point 2 Surveyors dated March 2022 Version V1. The report and appendices consider the impacts of the proposed development on the on buildings with a reasonable expectation of daylight, in accordance with the Building Research Establishment (BRE) guidelines.
- 10.115 The report considers the impact on properties relevant for assessment as either registered with a residential component or have a certain requirement for natural light given the building usage are as follows:
- 186 York Way (Commercial building of 4 storeys)
 - Fitzpatrick Building at 188-194 York Way (Office building of part 7, part 8 part 9 storeys)
 - Antony Gormley Studios, 15-23 Vale Royal (Artists studio of 2 to 3 storeys).

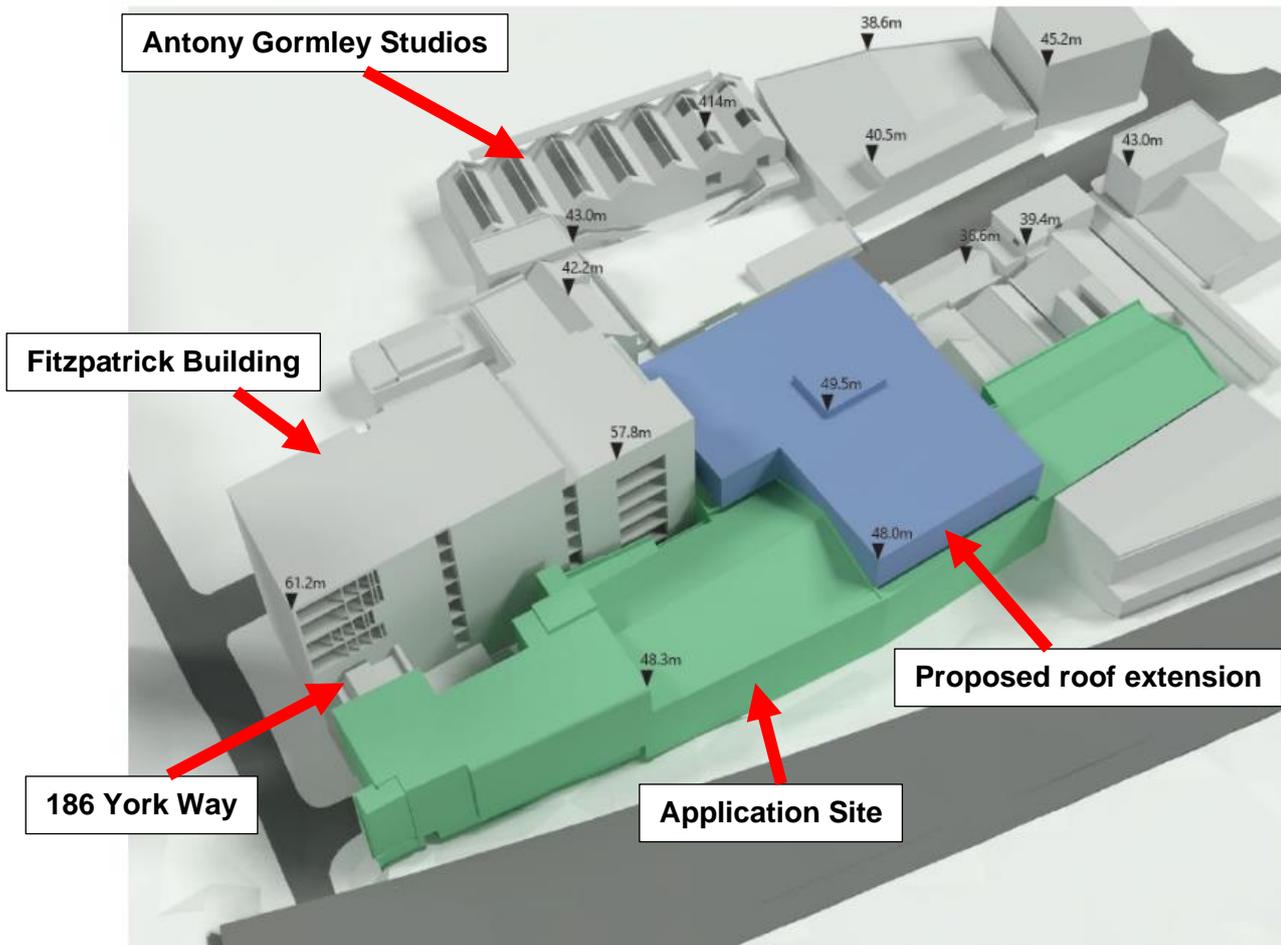


Image 12 – eb7 Daylight Report – 3D view of the proposed development and context (additional massing shown in blue, retained building shown in green)

Impacts to Daylight

10.116 The report shows the losses of daylight to the tested windows and rooms of the three commercial properties which have been tested, would not result in any transgressions of the VSC and NSL Tests.

Impacts to Sunlight

10.117 The report shows any losses of sunlight to the tested windows and rooms of the two commercial properties which have been tested, would not result in any transgressions of the APSH Tests. The property at 186 York Way does not fall within 90 degrees of due south of the property and so is not required to be tested for sunlight impacts.

Overshadowing

10.118 The report includes an overshadowing technical analysis, of the Sun Hours on Ground assessment for the outside yard space to the Antony Gormley Studio at 15-23 Vale Royal.

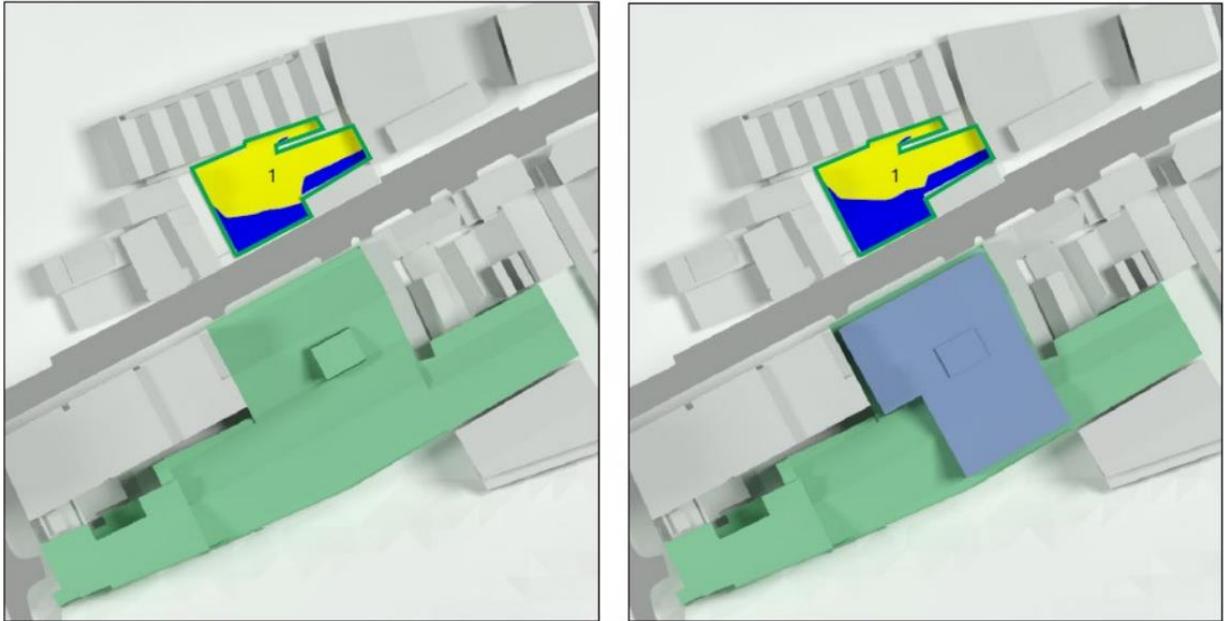


Image 13 – Existing Scenario March 21st vs Proposed Scenario March 21st

10.119 The report demonstrates that 60% of the forecourt area assessed receives at least two hours of sun on the 21st of March with the proposed development in place. This is a very minor increase in overshadowing in the yard space with more than 2 hours of direct sunlight being reduced from 71% to 60% of the yard space as at 21st March. This reduction is within the BRE guidance which considers that a reduction of up to 0.8 times its former value, is not likely to create a noticeable loss of sunlight. The report therefore demonstrates that impact on the amenity of the space, is in compliance with the BRE guidance.

Daylight and Sunlight Summary

10.120 It is noted in the BRE Guidelines and London Plan policies that a level of flexibility is required when assessing daylight and sunlight, with a consideration of the site context and circumstances. In this case, the site is not located within close proximity to neighbouring residential buildings. There are no transgressions in terms of loss of daylight to the nearest residential properties, nor to those commercial buildings with a reasonable expectation of daylight or sunlight.

10.121 The latest application is considered to have demonstrated that an acceptable level of amenity would be maintained to the properties at 186 York Way, Fitzpatrick Building at 188-194 York Way and Antony Gormley Studios at 15-23 Vale Royal, with respect to levels of daylight and overshadowing.

Privacy

10.122 The supporting text to policy DM2.1 states at paragraph 2.14 that *'to protect privacy for residential developments and existing residential properties, there should be a minimum distance of 18 metres between windows of habitable rooms. This does not apply across the public highway, overlooking across a public highway does not constitute an unacceptable loss of privacy'*. In the application of this guidance, consideration has to be given also to the nature of views between windows of the development and neighbouring habitable rooms. For instance, where the views between habitable rooms are oblique as a result of angles or height difference between windows, there may be no or little harm.'

10.123 Paragraph 2.3.36 of the Mayor of London's Housing SPG states that such minimum distances "*can still be useful yardsticks for visual privacy, but adhering rigidly to these measures can limit the variety of urban spaces and housing types in the city, and can sometimes unnecessarily restrict density*". This is noted, and there have indeed been instances where window-to-window distances of less than 18m have been accepted where exceptional circumstances apply, however the Mayor's guidance does not override Islington's Development Management Policies, and there remains a need to ensure that proposed developments maintain adequate levels of privacy for neighbouring residents.

10.124 The proposed development includes no residential accommodation or habitable rooms, therefore the 18m requirement does not apply to itself. Nevertheless, there is potential for light industrial or office windows to adversely affect the privacy of neighbouring residential properties.

10.125 The nearest neighbouring residential properties to the application site are located on the opposite side of York Way, and thus privacy impacts strictly speaking do not apply. As such it is not considered that loss of privacy or increased overlooking should be considered to have an adverse impact.

Outlook

10.126 Given the separation distance from the closest residential occupiers is in located on the opposite side of York Way and the additional massing is located behind the taller building at the Fitzpatrick Building at 188-194 York Way, the proposals are not considered to result in materially harmful impact on outlook to residential amenity.

10.127 There are some adjacent commercial properties, with the closest windows positioned on the north side of Vale Royal at Tileyard Studios. Given the uses and proximity to the site, the impacts on outlook to these non-residential properties is not considered to have a materially adverse effect.

Mechanical Noise

10.128 Policy DM2.1 also states development should not have an adverse impact on amenity in respect to noise and disturbance.

10.129 There is mechanical plant proposed at rooftop level at sixth floor level. A Noise and Vibration Impact Assessment report has been submitted to support the proposed development in terms of noise in general, including noise from plant. The Acoustic officer commented that the report proposes ASHPs and has no combustion sources listed. However there is some separation from this area and the nearest residential receptor. Therefore the noise emitted from the proposed plant is not considered to result in an adverse impact on residential properties.

10.130 However the noise assessment has been reviewed by the Council's Public Protection Officer and considered to be acceptable subject to condition (7) to limit the noise emitted from the plant to at least 5dB(A) below the background noise levels, and for the submission of a post-installation report.

General Noise and Disturbance

10.131 Noise and disturbance are likely to be generated from the proposed construction works, as well as the commercial operations proposed under this application. However the Council's Acoustic Officer has noted the site's location on an industrial estate with reasonable distance separation and the railway lines to the south and east between that and the nearest residential uses. The officer has given regard to these circumstances, and not raised any objections.

Construction Impacts

- 10.132 The construction works proposed under this application would unavoidably cause some degree of noise and disruption which could affect neighbouring properties although no residential properties are located in the immediate vicinity of the site. The submission includes a Construction Logistics Plan (CLP). The EPPP officer notes it fails to reference the Council's Code of Practice for Construction Sites (CoPCS), but does include a site noise risk assessment. The office requests that it is advised that a final CLP is required by condition (6) or required via S106 obligation.
- 10.133 The Air Quality report classifies the site as "low" risk. Having reviewed the report, the EPPP officer notes that with no combustion sources or additional parking and as the site isn't in an area of high exposure or next to a busy road, there is no requirement for a further air quality assessment.
- 10.134 It is worth noting that outside planning control there are further controls applicable to construction, including Environmental Health legislation and regulations that would further protect the amenities of neighbouring occupiers during the construction period.
- 10.135 The EPPP officer notes the submission includes a contaminated land report which advises that some further investigation is required particularly with the potential for a UST (which may or may not be removed) in the south of the site. Therefore, a land contamination condition is imposed (24).
- 10.136 The transportation and highways impact during the construction stage is further discussed in the Highways and Transport section below.

Air Quality Impacts

- 10.137 The London Plan Policy SI1 sets out requirements for developments to be air quality neutral. The purpose of the London Plan's requirement that development proposals be 'air quality neutral' is to prevent the gradual deterioration of air quality throughout Greater London.
- 10.138 An air quality assessment has been submitted. The Air Quality report classifies the site as "low" risk and the Council's EPPP officer has accepted these findings and not requested any further details.

Neighbouring Amenity Summary

- 10.139 Subject to the conditions set out in this report, it is considered that the proposed development would not give rise to unacceptable impacts on neighbouring residential amenity.

ECOLOGY

- 10.140 The southern boundary of the site is located close to a Sites of Importance for Nature Conservation (SINC) (Grade Borough 1) (Copenhagen Junction) by railway lines and other buildings. As a result of the proposed development and the additional height and massing, it is recommended that a condition is attached requiring details of the lighting strategy to ensure no adverse impacts on the adjacent SINC. These details are secured through condition 9.
- 10.141 A Preliminary Bat Roost Assessment has been submitted with the application. A full biological record centre desktop study was not undertaken as part of this assessment. This was not considered necessary given the limited scale of the proposed development, the nature of the on-site and surrounding habitats and limited potential for impacts to arise within or outside of the Site. The Preliminary Bat Roost Assessment details that the building appeared to be well-sealed and in good condition and no evidence of bats or potential roost features have been recorded with negligible potential to support roosting bats.

Light Pollution

10.142 The proposal would not alter the commercial nature of the site; however, the proposal raises the possibility of night time light pollution occurring, given the additional glazing proposed and the additional height of the building, should office staff need to work outside normal office hours; due to the proposed intensification of commercial use on the site, the cumulative impact is likely to be greater than existing and therefore, it is considered that adequate measures would need to be in place to mitigate any adverse light pollution impact.

10.143 To address this, condition 9 is recommended for details of measures to adequately mitigate light pollution. The measures which could be used include automated roller blinds, lighting strategies that reduce the output of luminaires closer to the façades or light fittings controlled through the use of sensors.

10.144 It is considered that any proposed measures would need to ensure the extent of light being used within the building is reduced and would help minimise any impact on neighbouring properties, and address any light pollution concerns.

BIODIVERSITY, LANDSCAPING AND TREES

10.145 London Plan Policy G1 states that development proposals should incorporate appropriate elements of green infrastructure that are integrated into London's wider green infrastructure network. Policy G5 further states that major development proposals should contribute to the greening of London by including urban greening as a fundamental element of site and building design, and by incorporating measures such as high-quality landscaping (including trees), green roofs, green walls and nature-based sustainable drainage.

10.146 Policy CS15 of the Islington Core Strategy and policy DM6.5 of the Islington Development Management Policies reads that the council will seek to maximise opportunities to 'green' the borough through planting, green roofs, and green corridors to encourage and connect green spaces across the borough; development proposals are required to maximise the provision of soft landscaping, including trees, shrubs and other vegetation, and maximise biodiversity benefits. Part C of the policy requires new-build developments, and all major applications, to use all available roof space for green roofs, subject to other planning considerations.

10.147 The site is approximately 0.29 hectares in size and is entirely covered by built form with no existing soft landscaping within the site.

10.148 During the course of the application additional information has been submitted to demonstrate the bio-solar green roof has been maximised given the structural capacity of the existing roof. The extent of bio-solar green roof and 131 PVs will be secured by condition (17).

Urban Greening Factor

10.149 London Plan Policy G5 requires major development proposals to include urban greening as a fundamental element of site and recommends a target score of 0.3 for commercial development excluding B2 and B8 uses.

10.150 An Urban Greening Factor assessment has been submitted with the application which indicates a UGF of 0.228. Given the ratio of B8 (78 percent) and E(g) uses (22 percent), the Council's Sustainability officer has accepted the score of 0.228 as the target score of 0.3 excludes B8 uses, as long as the green roof coverage has been maximised as far as possible. Given the subsequent structural assessment which demonstrates the structural capacity of the existing roof has been maximised, the extent of green roof is considered to have been maximised. The proposals are therefore acceptable in accordance with the aims of London Plan Policy G5.

10.151 In summary, the proposal would enhance biodiversity on the site through the provision of green roofs. Given the nature of the use, the site is already entirely built form and the site's constraints, the proposed green roofs are acceptable and the details of the green roofs are secured by condition (16).

ENERGY AND SUSTAINABILITY

10.152 The NPPF confirms that the purpose of the planning system is to contribute to the achievement of sustainable development, and standards relevant to sustainability are set out throughout the NPPF. Paragraph 152, under section 14. '*Meeting the challenge of climate change, flooding and coastal change*', highlights that the planning system should support the transition to a low carbon future in a changing climate, taking full account of flood risk and coastal change. It should help to: shape places in ways that contribute to radical reductions in greenhouse gas emissions, minimise vulnerability and improve resilience; encourage the reuse of existing resources, including the conversion of existing buildings; and support renewable and low carbon energy and associated infrastructure.

10.153 The NPPF para 157 states that in determining planning applications, LPAs should expect new development to comply with any development plan policies on local requirements for decentralised energy supply unless it can be demonstrated by the applicant, having regard to the type of development involved and its design, that this is not feasible or viable; and take account of landform, layout, building orientation, massing and landscaping to minimise energy consumption.

10.154 London Plan policy GG6 seeks to make London a more efficient and resilient city, in which development must seek to improve energy efficiency and support the move towards a low carbon circular economy, contributing towards London becoming a zero carbon city by 2050. Proposals must ensure that buildings are designed to adapt to a changing climate, making efficient use of water, reducing impacts from natural hazards like flooding and heatwaves, while mitigating and avoiding contributing to the urban heat island effect.

10.155 Policy SI 2, in support of the strategic objectives set out in Policy GG6 above, stipulates for new developments to aim to be zero carbon with a requirement for a detailed energy strategy to demonstrate how the zero-carbon target will be met within the framework of the energy hierarchy. It requires all major development proposals to contribute towards climate change mitigation by reducing carbon dioxide emissions by 35% through the use of less energy (be lean), energy efficient design (be clean) and the incorporation of renewable energy (be green). Moreover, where it is clearly demonstrated that the zero carbon figure cannot be achieved then any shortfall should be provided through a cash contribution towards the Council's carbon offset fund.

10.156 In regard to Energy Infrastructure, policy SI 3 part D states that all major development proposals within Heat Network Priority Areas should have a communal low-temperature heating system, which should be selected in accordance with the following heating hierarchy:

- connect to local existing or planned heat networks
- use zero-emission or local secondary heat sources (in conjunction with heat pump, if required)
- use low-emission combined heat and power (CHP) (only where there is a case for CHP to enable the delivery of an area-wide heat network, meet the development's electricity demand and provide demand response to the local electricity network)
- use ultra-low NOx gas boilers

- 10.157 Where a heat network is planned but not yet in existence the development should be designed to allow for the cost-effective connection at a later date.
- 10.158 Policy SI 4 (Managing Heat Risk) of the London Plan requires for development proposals to minimise adverse impacts on the urban heat island through design, layout, orientation, materials and the incorporation of green infrastructure.
- 10.159 Policy CS10 of the Islington Core Strategy requires that development proposals are designed to minimise onsite carbon dioxide emissions by maximising energy efficiency, supplying energy efficiently and using onsite renewable energy generation. Developments should achieve a total (regulated and unregulated) CO2 emissions reduction of at least 27% relative to total emissions from a building which complies with Building Regulations 2013 (39% where connection to a Decentralised Heating Network is possible). Typically, all remaining CO2 emissions should be offset through a financial contribution towards measures which reduce CO2 emissions from the existing building stock.
- 10.160 Policy DM7.1 of the Islington Development Management Policies requires development proposals to integrate best practice sustainable design standards and states that the council will support the development of renewable energy technologies, subject to meeting wider policy requirements. Details are provided within Islington's Environmental Design SPD, which is underpinned by the Mayor's Sustainable Design and Construction Statement SPG.
- 10.161 The applicant has submitted an Energy Statement prepared by Energy Statement Rev 1.0, prepared by Thornton Reynolds, dated July 2022 with the purpose of addressing the policy requirements referenced above.

Carbon Emissions

- 10.162 The London Plan sets out a CO2 reduction target, for regulated emissions only, of 40% against Building Regulations 2010 and 35% against Building Regulations 2013.
- 10.163 At local level, the council requires onsite total CO2 reduction targets (regulated and unregulated) against Building Regulations 2010 of 40% where connection to a decentralised energy network is possible, and 30% where not possible. These targets have been adjusted for Building Regulations 2013 to 39% where connection to a decentralised energy network is possible, and 27% where not possible.
- 10.164 The Energy and Sustainability Strategy states that the proposed development achieves a reduction of 39.8% in total (regulated and unregulated) emissions against a Part L 2013 baseline. The Energy and Sustainability Strategy addendum shows the tonnes of Regulated Emissions and percentage savings at each stage of the energy hierarchy. This shows a saving of 31.98% in total emissions. This is based on SAP10 carbon factors, used to calculate Total (Regulated and Unregulated) emissions. This exceeds the requirement is Islington Policy for developments not able to connect to a DEN to achieve a 27% reduction.

Zero Carbon Policy

- 10.165 As noted earlier, policy SI 2 of the London Plan stipulates development proposals to aim to be zero carbon, this is supported by Islington Core Strategy Policy CS10 which states that development will need to promote zero carbon development by minimising on-site carbon dioxide emissions, promoting decentralised energy networks and by requiring development to offset all remaining CO2 emissions associated with the building through a financial contribution towards measures which reduce CO2 emissions from the existing building stock.

10.166 The Council's Environmental Design SPD states that "after minimising CO2 emissions onsite, developments are required to offset all remaining CO2 emissions (Policy CS10) through a financial contribution", this includes both regulated and unregulated emissions. The SPD further states that the calculation of the amount of CO2 to be offset, and the resulting financial contribution, shall be specified in the submitted Energy Statement.

10.167 The Carbon Offset contribution of £119,700.00 has been agreed and confirmed by the Council's Energy Officer and is secured via S106.

10.168 Part A of policy DM7.1 states "*Development proposals are required to integrate best practice sustainable design standards (as set out in the Environmental Design SPD), during design, construction and operation of the development.*" It further states that "*developments are required to demonstrate how the proposed design has maximised incorporation of passive design measures to control heat gain and to deliver passive cooling, following the sequential cooling hierarchy*".

10.169 The Energy and Sustainability Strategy shows the proposed development achieving a 21.3% reduction in Regulated Emissions over a Part L 2013 baseline meeting the requirement in the London Plan for non-domestic buildings to achieve a 15% reduction at this stage. LED lighting and suitable controls have been specified throughout.

Overheating and Cooling

10.170 Part A of policy DM7.5 of the Islington Development Management Policies requires developments to demonstrate that the proposed design has maximised passive design measures to control heat gain and deliver passive cooling, in order to avoid increased vulnerability against rising temperatures whilst minimising energy intensive cooling. Part B of the policy supports this approach, stating that the use of mechanical cooling shall not be supported unless evidence is provided to demonstrate that passive design measures cannot deliver sufficient heat control. Part C of the policy requires applicants to demonstrate that overheating has been effectively addressed by meeting standards in the latest CIBSE (Chartered Institute of Building Service Engineers) guidance.

BE LEAN- Reduce Energy Demand

10.171 The Energy and Sustainability Statement details how the Cooling Hierarchy has been incorporated into the design of the building.

10.172 Overheating Report provided as Appendix C. Thermal Modelling has been undertaken and London DSY for 2020 & 2050 used to compare results against the criteria of CIBSE TM52. This shows that all areas pass using both DSYs against TM52 however, all modelling has been undertaken with active cooling in place. It is therefore not clear with active cooling is required to prevent overheating and its inclusion justified.

10.173 The inclusion of active cooling has not been justified by the use of thermal modelling of the proposed development without the cooling in place. Further details to demonstrate the inclusion of active cooling are secured by condition (14).

BE CLEAN- Low Carbon Energy Supply

10.174 In respect to energy (heating and cooling) supply, it is proposed that such would be provided by an Air Source Heat Pump (ASHP). These will be on an individual basis and will not form a communal system.

10.175 Part C of policy DM7.3 of the Islington Development Management Policies states “*major developments located within 500 metres of a planned future DEN, which is considered by the council likely to be operational within 3 years of a grant of planning permission, will be required to provide a means to connect to that network and developers shall provide a reasonable financial contribution for the future cost of connection and a commitment to connect via a legal agreement or contract, unless a feasibility assessment demonstrates that connection is not reasonably possible.*”

10.176 The Energy and Sustainability Strategy includes a map from Association of Decentralised Energy showing that there is no planned or existing DEN within 500m of the proposed development and therefore no feasibility assessment of connection is required.

Shared Energy Networks

10.177 Part D of policy DM7.4 states “*Where connection to an existing or future DEN is not possible, major developments should develop and/or connect to a Shared Heating Network (SHN) linking neighbouring developments and/or existing buildings, unless it can be demonstrated that this is not reasonably possible.*”

10.178 The Energy and Sustainability Statement does not include any details of how the proposed development will be futureproofed for connection to a future DEN. The applicant is required to provide details of how the proposed development will be futureproofed for connection to a future DEN as required in Islington DM3.7a. Further details of requirements are set out in Appendix 1 of Islington’s Environmental Design SPD as well as Islington’s Guidelines for Connecting to Heat Networks Parts 1 & 2. These include the requirement to have a communal heating network. This is secured by condition (14).

CHP/CCHP or Alternative Low Carbon On Site Plant

10.179 In accordance with the London Plan hierarchy, where connection to district heating or cooling networks are not viable, on-site low carbon heating plant should be proposed and CHP/CCHP prioritised (this may also form the basis of the alternative strategy, where the primary strategy is for connection to a district heating or cooling network if found viable through further investigation).

10.180 The Council’s Environmental Design Guide (page 12) states “Combined Heat and Power (CHP) should be incorporated wherever technically feasible and viable. Large schemes of 50 units or more, or 10,000sqm floorspace or more, should provide detailed evidence in the form of an hourly heating profile (and details of electrical baseload) where the applicant considers that CHP is not viable; simpler evidence will be accepted on smaller schemes.”

10.181 The applicant has provided reasoning as to why an investigation into the possibility of forming a shared heating network with neighbouring developments and/or existing buildings might not be feasible. This response has been accepted by the Council’s Energy Officer.

BE GREEN- Renewable Energy Supply

10.182 The Mayor’s SD&C and SPD reads “*although the final element of the Mayor’s energy hierarchy, major developments should make a further reduction in their carbon dioxide emissions through the incorporation of renewable energy technologies to minimise overall carbon dioxide emissions, where feasible.*”

10.183 The Council’s Environmental Design SPD (page 12) states “use of renewable energy should be maximised to enable achievement of relevant CO2 reduction targets.”

10.184 The Energy and Sustainability Strategy includes a review of various renewable technologies. As well as the ASHP mentioned above a 77kWp/131 Solar PV array has been proposed. This is supported and secured by condition (17).

BREEAM - Sustainable Design Standards

10.185 Part A of policy DM7.4 of the Islington Development Management Policies states “*Major non-residential developments are required to achieve Excellent under the relevant BREEAM or equivalent scheme and make reasonable endeavours to achieve Outstanding.*”

10.186 The Council’s Environmental Design Guide states “*Schemes are required to demonstrate that they will achieve the required level of the CSH/BREEAM via a pre-assessment as part of any application and subsequently via certification*”.

10.187 BREEAM pre-assessments have been submitted for both the New Build and Refurbished elements of the proposed development. These score 76.19% and 73.54% respectively. These are both equivalent to ‘Excellent’ rating and meet the requirements of Islington policy DM7.4(A).

10.188 A condition (15) is recommended to secure compliance with the A BREEAM pre-assessment which achieves an BREEAM ‘Excellent’ rating which meets the policy requirement.

Be Seen

10.189 The London Plan 2021 policy SI 2 (A4): states that developments must “be seen: monitor, verify and report on energy performance” and that “The move towards zero-carbon development requires comprehensive monitoring of energy demand and carbon emissions to ensure that planning commitments are being delivered. Major developments are required to monitor and report on energy performance, such as by displaying a Display Energy Certificate (DEC), and reporting to the Mayor for at least five years via an online portal to enable the GLA to identify good practice and report on the operational performance of new development in London.” The Council will also seek to secure this via Section 106 Agreement, based on the template wording used by the GLA.

10.190 Details of how the proposal will meet the GLA’s ‘Be Seen’ requirements have been provided and the Energy Officer has accepted this response.

Draft Green Performance Plan

10.191 Policy DM7.1 of the Islington Development Management Policies and the Environmental Design SPD (8.0.12 – 8.0.18) states “applications for major developments are required to include a Green Performance Plan (GPP) detailing measurable outputs for the occupied building, particularly for energy consumption, CO2 emissions and water use, and should set out arrangements for monitoring the progress of the plan over the first years of occupancy.” The council’s Environmental Design SPD provides detailed guidance and a contents check-list for a Green Performance Plan.

10.192 Draft Local Plan policy S4 requires developments to submit a Green Performance Plan (GPP) detailing the actual measurable outputs for the occupied building in relation to energy consumption and carbon emissions, based on the commitments in the SDCS.

10.193 The applicant is required to provide a Draft Green Performance Plan which includes measurable targets for energy consumption, CO2 emissions and water use. Further details of requirements can be found in DM 7.1 D, Environmental Design SPD 8.0.12-8.0.18 and Appendix 3. This requirement is secured by condition(14).

Economy

- 10.194 Policy SI 7 of the London Plan 2021 states that resource conservation, waste reduction, increases in material reuse and recycling, and reductions in waste going for disposal will be achieved by the Mayor, waste planning authorities and industry working in collaboration to promote a more circular economy that improves resource efficiency and innovation to keep products and materials at their highest use for as long as possible.
- 10.195 Emerging policy S10 of the Islington Local Plan states that all developments must adopt a circular economy approach to building design and construction in order to keep products and materials in use for as long as possible to minimise construction waste.
- 10.196 The proposal comprises of substantial building works, including the extensions and alterations to the existing buildings on site. It is required to demonstrate that materials extracted from demolition can be re-used where possible, and that the building will adapt to change over its lifetime. The development also needs to minimise the environmental impact of materials through the use of sustainably sourced, low impact and recycled materials.
- 10.197 Officers note that the requirements for the submission of an Adaptive Design Strategy and whole life-cycle carbon (WLC) assessment are requirements of emerging policies S4 and S10. Whilst not a requirement of currently adopted policy, officers have sought confirmation from the applicant of the actions taken to reduce life-cycle carbon emissions and the approach to adaptive design/circular economy.
- 10.198 The applicant has provided both a Whole Life Carbon Assessment and a Circular Economy Statement. Whilst much of the required information has been provided, there remains outstanding information regarding options considered and actions to be taken to reduce Whole Life Carbon emissions. This information is secured by conditions (29, 30 and 31).

Sustainable Drainage

- 10.199 Policy SI 5 states that in order to minimise the use of mains water, water supplies and resources should be protected and conserved in a sustainable manner. Commercial development proposals should achieve at least the BREEAM excellent standard for the 'Wat 01' water category or equivalent, and incorporate measures such as smart metering, water saving and recycling measures, including retrofitting, to help to achieve lower water consumption rates and to maximise future-proofing.
- 10.200 Policy CS10 of the Islington Core Strategy requires all development to demonstrate that it is designed to be adapted to climate change, particularly through design which minimises overheating and incorporates sustainable drainage systems. Policy DM6.6 of the Islington Development Management Policies is concerned with flood prevention and requires that schemes must be designed to reduce surface water runoff to a 'greenfield rate', where feasible.
- 10.201 The sustainability officer has accepted that due to the nature of the proposed development and the constrained site the scheme will not be able to achieve a significant reduction in surface water runoff. The officer requested consideration of blue roof attenuation storage into the design, combined with the green roofs or additional drainage layers under the green roofs. In response to these comments, during the course of the application the applicant has provided a structural report indicating that the capacity of the existing roof has been maximised.

HIGHWAYS AND TRANSPORT

- 10.202 Paragraph 108 of the NPPF states that applications should ensure that appropriate opportunities to promote sustainable transport modes can be or have been taken up, given the type of development and its location. Development proposals should also ensure that any significant

impacts from the development on the transport network or on highway safety, can be cost effectively mitigated to an acceptable degree.

10.203 Policy T4 of the London Plan 2021 states that development proposals should reflect and be integrated with current and planned transport access, capacity and connectivity. A Transport Statement should be submitted with development proposals to ensure that impacts on the capacity of the transport network are fully assessed. Furthermore, part C of this policy states that where appropriate, mitigation, either through direct provision of public transport, walking and cycling facilities and highways improvements or through financial contributions, will be required to address adverse transport impacts that are identified.

10.204 Policy DM8.1 of the Islington Development Management Policies states that the design of the development is required to prioritise the transport needs of pedestrians, public users and cyclists above those of motor vehicles. Further, Policy DM8.2 states that proposals are required to meet the transport needs of the development and address its transport impacts in a sustainable manner and in accordance with best practice. Where the council considers that a development is likely to have a significant negative impact on the operation of transport infrastructure, this impact must be satisfactorily mitigated.

10.205 The site has poor access to public transport and the Public Transport Accessibility Level is 2 which is amongst the lowest rating. There are a small number of bus routes on York Way providing connection to central London. Five stations are within 1.3km/15 minutes' walk of the site: Caledonian Road (Underground), Camden Road (Overground), Caledonian Road & Barnsbury (Overground), Kings Cross (Underground, National Rail) and St Pancras (Underground, National Rail and Eurostar). York Way is the northern end of a signposted north-south cycle route to Copenhagen Street. Whilst there are options for public transport within the wider surroundings, the site does not benefit from good access to these and would not normally be considered a sustainable location for high density development.

Vehicle Parking

10.206 Given the nature of the self-storage use and existing on site loading bay, 9no. vehicle parking spaces are proposed on-site, as well as a disabled parking space, which is considered acceptable and in accordance with policy CS10 of the Islington Core Strategy and policy DM8.5 of the Islington Development Management Policies.

10.207 In the circumstances of the existing and expanded storage use of the site, in providing vehicle parking spaces for this self-storage use, there is no conflict with the requirements of these policies requiring development to be car free.

10.208 In regard to disabled parking, 1no. disabled parking space is proposed on site, however, it is anticipated that the need for disabled parking provision would increase as a result of the development. In accordance with Policy DM8.5 and the guidance with the Planning Obligation SPD, a financial contribution of £10,000 is required to secure additional on-street blue badge parking bays, (assuming 1no. bay is provided on-site), or alternative accessibility improvements to be agreed by the Council's highway officers. The financial contribution is to be secured by the S106 legal agreement.

Cycling

10.209 In terms of cycling, policy T5 of the London Plan states that development proposals should help remove barriers to cycling and create a healthy environment in which people choose to cycle. It should also secure appropriate levels of cycle parking which should be fit for purpose, secure and well-located.

- 10.210 The London Plan requires 1 long stay cycle parking space per 75sqm of office floorspace, and 1 short stay cycle parking space per 500sqm. For B2-B8 general industrial, storage or distribution uses the London Plan requires 1 long stay space per 500 sqm (GEA) and 1 short stay space per 1000 sqm (GEA).
- 10.211 Based on the London Plan (2021) requirements, when applied to the entire site, the site should provide 58 long stay spaces and 16 short stay cycle spaces, totalling 74 spaces.
- 10.212 Appendix 6 of the adopted Development Management Policies sets out that 1 space per 80sqm of Office use floorspace (Now Class E(g)(i) use, 1 space per 400sqm of B8 storage and distribution use floorspace should be provided (area relates to Gross Internal Area for the purpose of calculations).
- 10.213 Based on the requirements of the adopted local plan, the site should provide 25 cycle parking spaces for the 9,964sqm of B8 warehouse use floorspace, and 36 cycle parking spaces for the 2,848sqm of Flexible Class E(g) use floorspace which totals 61 spaces.
- 10.214 The Council's emerging local plan matches the cycle parking requirements of the London Plan.
- 10.215 The proposals amount to extensions and alterations to the existing developed site. A cycle parking facility would be located at ground floor level and would provide for an initial 32 cycle parking spaces and 2 no. accessible cycle parking spaces for long stay usage.
- 10.216 Whilst the proposals don't demonstrate full compliance with London Plan policy T5 and Appendix 6 of the Islington Development Management Policies, the applicant's Transport Assessment asserts at sections 4.7 and 4.8:

'A total of 32 cycle parking spaces will be provided within the ground floor of the Vale Royal building. Two of these spaces are positioned to accommodate large cycles such as cargo bikes, tricycles or hand cycles used by disabled people. The provision of cycle parking for the flexible working space in accordance with standards varies between 13 - 42 long stay spaces and 4 and 7 short stay spaces depending on whether office or light industrial parking standards are used. Self-storage facilities generate little demand for cycle parking as they typically employ 2 or 3 staff and customers are generally transporting bulky goods so travel by car or van is the only realistic option. As such a self-storage provision of 4 spaces is usually ample for this use.

The provision of 32 spaces to accommodate long and short stay parking for both elements of the development is considered appropriate given the proposed mix of uses on site. However, use of cycle parking will be monitored and additional stands provided if demand warrants it. The proposed site plan shows where a further 32 bikes could be accommodated within the ground floor of the York Way building.'

- 10.217 As a result officers recommend that a condition (25) is attached to require details of the monitoring of the cycle usage during the first 6 months following commencement of the use, to be submitted to the Council and the spaces provided, should the results of the monitoring indicate an additional demand. Therefore, whilst the provision of on-site cycle parking does not meet the London Plan or local plan requirements, consideration is given to the specific nature of the proposed use as self-storage involving the transportation of bulky goods, which would typically necessitate vehicular rather than cycle access. On this basis, the provision of cycle parking as proposed would be acceptable without conflicting with the aims of the relevant London Plan and local plan policies and the requirement for monitoring of the demand is secured by condition (25).
- 10.218 Therefore, whilst the provision of on-site cycle parking does not meet the London Plan or local plan requirements, consideration is given to the specific nature of the proposed use as self-storage

involving the transportation of bulky goods, which would necessitate vehicular rather than cycle access. On this basis, the provision of cycle parking as proposed would be acceptable without conflicting with the aims of the relevant London Plan and local plan policies and is secured by condition (25).

10.219 Given the industrial nature of the proposed uses and the location, and the narrow width of the adjacent pavement, 32 spaces and 2 accessible spaces for long stay use and 4no. accessible cycle spaces is proposed for visitors and is secured by condition (21).

10.220 Mobility scooter charging points are provided adjacent to the cycle parking. The ground floor level provides the required provision of end-of-trip facilities.

10.221 Overall, the proposal would provide an acceptable level of cycle facilities to support the development and to encourage use of alternative transport modes, which complies with the objectives of LP Policy T5, and Development Management Policy DM8.4.

Servicing and Waste Management

Adopted Policy

10.222 Part A of policy DM8.6 (Delivery and Servicing for New Developments) states that for commercial developments over 200 square metres, delivery/servicing vehicles should be accommodated on-site, with adequate space to enable vehicles to enter and exit the site in forward gear (demonstrated by a swept path analysis). Where servicing/delivery vehicles are proposed on street, Policy DM8.6 (Delivery and servicing for new developments), Part B, requires details to be submitted to demonstrate that onsite provision is not practical, and show that the on-street arrangements will be safe and will not cause a traffic obstruction/nuisance.

Emerging Policy

10.223 Part G of emerging policy SP3 requires all development proposals in the LSIS must (individually and cumulatively) consider the layout, orientation, access, servicing and delivery arrangements in order to minimise conflict and to avoid potential negative impacts on highways safety and amenity. Proposals for B8 space should consider the potential provision of yard space.

Assessment

On-site service area

10.224 The proposals retain the existing on-site vehicular servicing area at 14-22 Vale Royal by retaining access into the site from the Vale Royal frontage using the existing loading bay, providing direct access to the internal servicing yard. The on-site servicing area is extended to provide 10 vehicular parking spaces, including one accessible parking space, for use by customers in connection with the self-storage use.

10.225 Details have been provided within the submitted Transport Assessment to demonstrate the proposed loading and unloading arrangements in connection with the operation of the self-storage use, using swept paths. The swept paths indicate that the servicing area can accommodate cars and 3.5t transit vans for access and egress, entering and exiting the site in forward gear, for each of the 10no. vehicular spaces, should the other spaces be occupied.

10.226 The Council's Highways officer queried the safety of the swept paths given the close proximity of the some of the swept path manoeuvres shown in the submission. The applicant has confirmed that there are existing columns in the servicing area that mean that to access some bays, drivers may need to manoeuvre backwards and forwards rather than manoeuvre in one smooth turn. However, the manoeuvres shown are not considered to be particularly onerous and the yard should be well lit and brightly painted to ensure that columns are highly visible to drivers. The response has been accepted by the Council's Highways officer.

10.227 The proposals also remove the existing vehicular access into 184 York Way as there is no opportunity for vehicles to exit in forward gear with all vehicular access and delivery and servicing arrangements undertaken via 14-22 Vale Royal. The 184 York Way building will retain pedestrian access only. It is recommended that these arrangements are secured by condition (28).

On-street arrangements

10.228 The existing arrangements for refuse vehicles and heavy goods vehicles would remain as existing where they would stop on street on Vale Royal immediately adjacent to the site frontage using the existing on-street loading bay. A refuse storage area will be provided at ground floor level within the Vale Royal building close to the site frontage. The footprint of the existing site is entirely developed and the proposals extend the existing B8 use of the site.

10.229 The assessment presented in the Transport Assessment indicates that 2 HGVs per day could visit the flexible workspace and that 7 HGVs could visit the extended self-storage facility. The survey undertaken on Vale Royal and Tileyard Road shows that the loading bay on Vale Royal typically operates with a significant amount of spare capacity, with 8 of the 11 hourly beat counts showing at least 10 metres of the 17 metre bay being vacant. The closure of the access to the existing on-site parking area enables the loading bay to be increased to 25 metres in length.

10.230 The TA indicates the approved highway works plans for Vale Royal loading bay in connection with an approved extension to the width of the loading bay to 2.5m. The TA considers that this additional capacity will ensure that HGVs visiting the site will be able to stop on street without detriment to the operation of the local road network. The carriageway width of Vale Royal outside the is 5.5 metres, which the TA sets out is wide enough to allow an HGV to pass the widened loading bay.

10.231 In terms of refuse and recycling, officers have had regard to the council's refuse and recycling storage requirements. The scheme proposes 6no. 1100L Eurobins, to be located at ground floor adjacent to the vehicular entrance. Officers consider that the proposed arrangements are acceptable for the proposed uses.

10.232 Overall, given the existing situation and the proposed extension to the existing building retaining the existing use, the proposed continuation of both on-site servicing and delivery and on-street refuse collection arrangements are acceptable.

Construction Impacts

10.233 The proposed construction works would inevitably have some impact to the local area during the construction period. As such, a Construction Management Plan would need to be submitted and agreed by the Council prior to any construction work commencing on site; this is echoed by the Council's Public Protection Team who have recommended submission of a final version of a CMP (*condition 6*).

10.234 The submitted Construction Logistics Plan (Including Site Waste Management) proposed to extend the existing on-street loading bay in front of the site, eastwards so that when vehicles are loading/unloading at the site, a 17 metre loading bay is retained for other people to use. The proposed increase in the length of the loading bay was only proposed during the construction phase. However, this extension to the on-street loading bay would impact on access to the site at 15-22 Vale Royal opposite the site which serves as the artist studio of Sir Antony Gormley. During the course of the application, the applicant's team liaised with Sir Antony Gormley's office regarding the proposed construction arrangements and have reached an agreement, as confirmed by Sir Antony Gormley's office.

10.235 As a result it is proposed that a Construction Management Plan should be required to be submitted by condition, outlining measures for the routing, accommodation, loading and unloading of

construction vehicles during the entirety of the construction phase. A construction programme should also be provided within the CLP once a contractor has been appointed. This is secured by condition (6) in the event of planning permission being granted, to ensure that the proposal would make all reasonable efforts to avoid unacceptable impacts to neighbouring amenity, the wider environment, or the safe and efficient operation of the highway network.

10.236 The Council's Highways Team has recommended that the applicant would need to cover any cost to repair any damages to the public footway/carrageway caused by the development. This would be secured under section 106 agreement with a figure to be confirmed.

10.237 In the interest of protecting neighbouring amenity during the construction phase of the development (having regard to impacts such as noise and dust) the applicant is also required to comply with the Council's code of construction practice. Compliance would need to be secured as part of a section 106 agreement together with a payment of £8,162 towards monitoring. This payment is considered an acceptable level of contribution having regard to the scale of the development, the proximity of other properties, and likely duration of the construction project.

Highways Summary

10.238 Overall, it is considered that the proposal would have adequate provision for servicing, waste storage, accessibility, cycling, collections and deliveries, subject to conditions and legal agreement. The proposal would then be acceptable and would comply with policies T5 and T6 of the London Plan 2021, policies CS10, CS11 and CS13 of the Islington Core Strategy 2011; and DM8.2, DM8.4, DM8.5 and DM8.6 of the Islington Development Management Policies (2013) and policies S1, S2, S3 and S4 of the emerging Strategic and Development Management Policies.

SAFETY AND SECURITY

10.239 London Plan Policy D11 requires development proposals to include measures to design out crime.

10.240 The Met Police's Design out Crime Officer (DOCO) has been consulted on the application. No response has been received.

10.241 It is recommended that this application, if granted, is conditioned (*condition 20*) to achieve Secured by Design accreditation.

FIRE SAFETY

10.242 Policy D12 of the London Plan states that in the interests of fire safety and to ensure the safety of all building users, all development proposals must achieve the highest standards of fire safety. All major development proposals should be submitted with a Fire Statement, which is an independent fire strategy, produced by a third party, suitably qualified assessor. Such statements should contain: the building's construction; means of escape for all building users; features that reduce the risk to life; access for fire service personnel and equipment; provision for fire appliances; and future modifications to the building.

10.243 The Fire Statement submitted with the application, has been prepared by Bureau Veritas Group, and a suitably qualified Chartered Engineer with experience in the fire engineering consultancy industry for 3.5 years, developing fire strategies and apply fire engineering principles for different types of construction in various locations in the UK, has been involved in the preparation of the document.

10.244 Both the HSE and the London Fire Brigade have been consulted on the development. No response has been received from the London Fire Brigade. The HSE has not raised any objections to the application.

10.245 Officers have undertaken a review of the Fire Statement in accordance with the requirements of policy D12(B) and this is set out in the following table:

10.246 In response to queries relating to the requirements of the London Plan policy D12(B), a revised document has been submitted and amended dated 8th November 2022.

London Plan policy D12(B) requires the following detail:	Response:
1. The building's construction: methods, products and materials used, including manufacturers' details.	All methods, products and materials used will be in accordance with The Construction (Design and Management) Regulations 2015 and accompanied with relevant testing data for their application and suitability. This will be applicable to all external and internal alterations of the building.
2. The means of escape for all building users: suitably designed stair cores, escape for building users who are disabled or require level access, and associated evacuation strategy approach	<p>The proposed evacuation strategy is based on a single stage simultaneous evacuation. This means that on the activation of a detector or manual call point all occupants of the building start their evacuation.</p> <p>The fire statement includes annotated plans indicating the escape points from each floor of the proposed development, from ground floor up to fifth floor indicating final exit, storey exit and specific signage requirements.</p> <p>Due to the adopted evacuation strategy (simultaneous), all building occupants must be evacuated in case of fire, and therefore a sufficient space outside the building should be chosen. The design team has confirmed that two assembly point will be located at: 1. Outside King's Cross Vehicle Testing Centre, 7 York Way and 2. Outside Reception, Tileyard Studios, 21 Tileyard Road with an aerial image indicating the locations of the assembly points.</p>
3. Features which reduce the risk to life: fire alarm systems, passive and active fire safety measures and associated management and maintenance plans	<p>The following fire safety systems are provided within the building:</p> <ul style="list-style-type: none"> - Fire detection and alarm system designed to BS5839-1. The provision of an enhanced L2 category fire detection and alarm system is proposed for all the office spaces and an L5 category system for the corridors of the warehouse spaces. - Wayfinding signage for the fire service. - Emergency lighting to be provided in accordance with BS 5266. - Structural fire resistance: 90 min. Higher risk areas to be separated by 60 minutes fire resistance with FD60s doors. - External surfaces of the walls must be constructed in order to adequately resist the spread of fire across the façade of the building. The building is less than 18m high with facades greater than 1m from any relevant boundary. Any façade less than 1m from a relevant boundary requires

	<p>achieve Class B-S3, d2 or better (European class). Profiled or flat steel sheet at least 0.5 mm thick with an organic coating of no more than 0.2 mm thickness is also acceptable.</p> <ul style="list-style-type: none"> - Fire-fighting shafts comprising fire-fighting stairs and fire-fighting lobbies inclusive of a fire main will be provided due to the height and risk profile of the building. - Fire mains will be provided within the fire-fighting lobbies of the building having inlets on ground floor level and outlets on all upper floor levels. - With regards to internal fire spread the surface finishes will satisfy the following classifications shown in table below, when tested under the European Classifications (in accordance with BS EN 13501-1:2018). <table border="1" data-bbox="512 562 1193 806"> <thead> <tr> <th>Location</th> <th>European Class</th> </tr> </thead> <tbody> <tr> <td>Small rooms not more than 4m² in a residential building and 30m² in a non-residential building</td> <td>D-s3, d2</td> </tr> <tr> <td>Circulation spaces within dwellings</td> <td>C-s3, d2</td> </tr> <tr> <td>Other circulation spaces including common areas</td> <td>B-s3, d2</td> </tr> </tbody> </table>	Location	European Class	Small rooms not more than 4m ² in a residential building and 30m ² in a non-residential building	D-s3, d2	Circulation spaces within dwellings	C-s3, d2	Other circulation spaces including common areas	B-s3, d2								
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Other circulation spaces including common areas	B-s3, d2																
<p>4. Access for fire service personnel and equipment: how this will be achieved in an evacuation situation, water supplies, provision and positioning of equipment, firefighting lifts, stairs and lobbies, any fire suppression and smoke ventilation systems proposed, and the ongoing maintenance and monitoring of these</p>	<p>The building has been characterised with an A3 risk profile and the top occupied storey height is more than 7.5. Therefore according to BS9999:2017, the provision of fire-fighting shafts is required. Fire mains should be provided within the fire-fighting lobbies. It should be noted that Fire Service Vehicle Access is within 18m from the dry riser inlet and therefore satisfies the requirement of the Building Regulations.</p> <p>A new private or an existing fire hydrant should be provided no more than 90m from a dry riser of the building and no more than 90m from any other hydrant, in accordance with BS9990. Design team has confirmed that new private fire hydrants will be provided, if the existing ones are more than 90m away from the dry risers' location.</p> <p>The fire statement includes annotated plans of the ground and first floors with the positions of the dry riser inlet, dry riser outlet, firefighting entrance, firefighting lobby and firefighting stairs.</p>																
<p>5. How provision will be made within the curtilage of the site to enable fire appliances to gain access to the building</p>	<p>Firefighting access to the building is via Vale Road and York Way. BS9999:2017 requires provision of access for a pumping appliance to within 18m of the dry riser inlets.</p> <p>The fire service access requirements are outlined within the table below.</p> <table border="1" data-bbox="467 1794 1437 1912"> <thead> <tr> <th rowspan="2">Appliance</th> <th rowspan="2">Min. Width of Road between Kerbs</th> <th rowspan="2">Min. Gateway Width</th> <th rowspan="2">Min. Clearance Height</th> <th rowspan="2">Min. Carrying Capacity</th> <th colspan="2">Min. Turning Circle</th> </tr> <tr> <th>Kerb to Kerb</th> <th>Wall to Wall</th> </tr> </thead> <tbody> <tr> <td>Pump</td> <td>3.7m</td> <td>3.1m</td> <td>3.7m</td> <td>12.5 tonnes</td> <td>16.8m</td> <td>19.2m</td> </tr> </tbody> </table>	Appliance	Min. Width of Road between Kerbs	Min. Gateway Width	Min. Clearance Height	Min. Carrying Capacity	Min. Turning Circle		Kerb to Kerb	Wall to Wall	Pump	3.7m	3.1m	3.7m	12.5 tonnes	16.8m	19.2m
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	<p>Existing roadways allow access for fire appliances, and entry points to buildings will be readily identifiable to the Fire and Rescue Service. The document includes a plan indicating the access routes for fire appliances into the site.</p> <p>The fire statement includes annotated plans of the ground and first floors with the positions of the dry riser inlet, dry riser outlet, firefighting entrance, firefighting lobby and firefighting stairs.</p>
<p>6. Ensuring that any potential future modifications to the building will take into account and not compromise the base build fire safety/protection measures.</p>	<p>Details of the suggested contents of the fire safety manual are provided in Section 19 of the Approved Document B and H of BS 9999 Code of practice for fire safety in the design management and use of buildings.</p> <p>The document sets out that the proposed development follows the current recommendations of BS 9999 and other associated British Standards to comply with the Building Regulations. Any potential future modification to the building needs to follow a new fire strategy that includes all the appropriate details considering all the modifications. The fire strategy report and its recommendations is based on the current design and cannot be used if any changes / modifications will be provided in the future.</p> <p>The document states that there is an on-going management obligation to ensure that not only the active and passive fire protection facilities are correctly maintained, and that there are appropriate management procedures in place to facilitate a safe evacuation in the event of a fire.</p>

10.247 The submitted information is specific and relevant to the proposal and the Fire Statement references BS9999:2017.

10.248 It is noted that the author of the submitted Fire Statement is a qualified person with expertise in fire safety and engineered solutions, and as such, the applicant has used the relevant expertise to consider the fire safety of the development as part of the overall scheme.

10.249 A condition is recommended (*condition 12*) requiring an updated Fire Statement to be submitted in the event that there are any changes to the details.

PLANNING OBLIGATIONS AND CIL

10.250 There is a requirement that planning obligations under Section 106 must meet 3 statutory tests, i.e. that they are (i) necessary to make the development acceptable in planning terms, (ii) directly related to the development, and (iii) fairly and reasonably related in scale and kind to the development. Under the terms of the Planning Act 2008 (as amended) and Community Infrastructure Levy Regulations 2010 (as amended), the Mayor of London's and Islington's Community Infrastructure Levy (CIL) would be chargeable on the proposed development on grant of planning permission. This is calculated in accordance with the Mayor's adopted Community Infrastructure Levy Charging Schedule 2019 and the Islington adopted Community Infrastructure Levy Charging Schedule 2014.

10.251 Islington's CIL Regulation 123 infrastructure list specifically excludes measures that are required in order to mitigate the direct impacts of a particular development and if specific off-site measures are required to make the development acceptable these should be secured through a S106 agreement.

10.252 Policy CS18 (Delivery and Infrastructure) of the Islington Core Strategy 2011 states that the council will work with its partners to deliver the infrastructure required to support development, and will require contributions from new development to ensure that the infrastructure needs are provided for and that the impacts of the development are mitigated. As mentioned in the previous section in the report, the proposed development would be subject to S106 obligations to ensure that appropriate education and training opportunities arise from the development, which would require a local employment and training contribution and a construction training placement during the construction period. Further details of planning obligations are set out in the relevant sections of this report, and as a full list in Appendix 1.

10.253 In order for the development to mitigate its own direct impacts, and to be acceptable in planning terms the following heads of terms are recommended to be secured by a S106 agreement.

- A bond/deposit to cover costs of repairs to the footway and for repairs to the highway (total to be confirmed by LBI Highway). This ensures funds are available for the repair and reinstatement of the footways and highways adjoining the development (paid for by the developer). The bond must be paid before commencement of works. Any reinstatement works will be carried out by LBI Highways (and the cost met by the developer or from the bond). Conditions surveys may be required. If this bond/ deposit exceeds the cost of the works as finally determined, the balance will be refunded to the developer. Conversely, where the deposit is insufficient to meet costs then the developer will be required to pay the amount of the shortfall to the Council;
- Compliance with the Code of Employment and Training.
- Facilitation of 2 work placements during the construction phase of the development, lasting a minimum of 26 weeks, or a fee of £10,000 to be paid to LBI. Developer / contractor to pay wages (must meet national minimum wage). London Borough of Islington Construction Works Team to recruit for and monitor placements.
- Compliance with the Code of Local Procurement.
- Compliance with the Code of Construction Practice, including a monitoring fee of £7,754 and submission of a site-specific response document to the Code of Construction Practice for the approval of LBI Public Protection. This shall be submitted prior to any works commencing on site.
- The provision of 5 additional accessible parking bays or a contribution of £10,000 towards provision of on-street bays or other accessible transport initiatives.
- Payment towards offsetting projected residual CO2 emissions of the development of £119,700.
- Connection to a local energy network, if technically and economically viable (burden of proof will be with the developer to show inability to connect). In the event that a local energy network is not available or connection to it is not economically viable, the developer should develop an on-site solution and/or connect to a neighbouring site (a Shared Heating Network) and future proof any on-site solution so that in all cases (whether or not an on-site solution has been provided), the development can be connected to a local energy network if a viable opportunity arises in the future.
- Submission of Green Performance Plan.

- Submission of a draft framework Travel Plan with the planning application, of a draft full Travel Plan for Council approval prior to occupation, and of a full Travel Plan for Council approval 6 months from first occupation of the development or phase (provision of travel plan required subject to thresholds shown in Table 7.1 of the Planning Obligations SPD).
- Payment towards employment and training for local residents of a commuted sum of £31,191
- Payment of Council's legal fees in preparing the S106 Agreement and officer's fees for the preparation, monitoring and implementation of the S106 Agreement.

11. CONCLUSION

- 11.1 The proposed development is considered acceptable in principle and in land use terms. In this regard, the scheme is considered to be compliant with the adopted development plan policies comprising of the London Plan Policies SD4, SD5 and E1, Islington Core Strategy CS6 and CS13, Islington Development Management Policy DM5.3, which all encourage the intensification of industrial use floorspace at this location, subject to the acceptability of other material considerations. In this regard the scheme exceeds the requirements of the adopted plan. The proposed uplift in B8 storage floorspace is also considered to achieve compliance with the requirements of the emerging local plan policy SP3.
- 11.2 The scheme would comply with policies relating to design, energy, sustainability, accessibility and transportation.
- 11.3 There are no amenity impacts to adjacent properties and no residential properties are located close to the site.
- 11.4 The proposal is considered to be a sustainable form of development,
- 11.5 The proposal results in an increase in biodiversity on site with a consequent uplift in the site's Urban Greening Factor, whilst not meeting the target UGF, the score of 0.228 is acceptable given the ratio of B8 and E uses, the green roof coverage has been maximised as far as possible in the structural capacity of the existing roof.
- 11.6 It should be recognised that the scheme involves the following benefits:
- Uplift in class B8 storage floorspace (1,233sqm GIA) within the LSIS involving the creation of modern and inclusive floorspace;
 - Increase in employment at the site through the creation of 2,848 sqm of Flexible Class E(g) use floorspace,
 - Jobs and training contributions;
 - Improvements to the energy efficiency of the operation of the new building.
- 11.7 It is recommended that planning permission is granted subject to conditions and s106 legal agreement heads of terms as set out in Appendix 1 - **RECOMMENDATIONS**.

APPENDIX 1 – RECOMMENDATIONS

RECOMMENDATION A

That planning permission be granted subject to the prior completion of a Deed of Planning Obligation made under section 106 of the Town and Country Planning Act 1990 between the Council and all persons with an interest in the land (including mortgagees) in order to secure the following planning obligations to the satisfaction of the Head of Law and Public Services and the Service Director, Planning and Development / Head of Service – Development Management or, in their absence, the Deputy Head of Service:

- A bond/deposit to cover costs of repairs to the footway and for repairs to the highway (total to be confirmed by LBI Highway). This ensures funds are available for the repair and reinstatement of the footways and highways adjoining the development (paid for by the developer). The bond must be paid before commencement of works. Any reinstatement works will be carried out by LBI Highways (and the cost met by the developer or from the bond). Conditions surveys may be required. If this bond/ deposit exceeds the cost of the works as finally determined, the balance will be refunded to the developer. Conversely, where the deposit is insufficient to meet costs then the developer will be required to pay the amount of the shortfall to the Council;
- Compliance with the Code of Employment and Training.
- Facilitation of 2 work placements during the construction phase of the development, lasting a minimum of 26 weeks, or a fee of £10,000 to be paid to LBI. Developer / contractor to pay wages (must meet national minimum wage). London Borough of Islington Construction Works Team to recruit for and monitor placements.
- Compliance with the Code of Local Procurement.
- Compliance with the Code of Construction Practice, including a monitoring fee of £7,754 and submission of a site-specific response document to the Code of Construction Practice for the approval of LBI Public Protection. This shall be submitted prior to any works commencing on site.
- The provision of 5 additional accessible parking bays or a contribution of £10,000 towards provision of on-street bays or other accessible transport initiatives.
- Payment towards offsetting projected residual CO2 emissions of the development of £119,700.
- Connection to a local energy network, if technically and economically viable (burden of proof will be with the developer to show inability to connect). In the event that a local energy network is not available or connection to it is not economically viable, the developer should develop an on-site solution and/or connect to a neighbouring site (a Shared Heating Network) and future proof any on-site solution so that in all cases (whether or not an on-site solution has been provided), the development can be connected to a local energy network if a viable opportunity arises in the future.
- Submission of Green Performance Plan.
- Submission of a draft framework Travel Plan with the planning application, of a draft full Travel Plan for Council approval prior to occupation, and of a full Travel Plan for Council approval 6 months from first occupation of the development or phase (provision of travel plan required subject to thresholds shown in Table 7.1 of the Planning Obligations SPD).
- Payment towards employment and training for local residents of a commuted sum of £31,191;

- Payment of Council's legal fees in preparing the S106 Agreement and officer's fees for the preparation, monitoring and implementation of the S106 Agreement.

That, should the Section 106 Deed of Planning Obligation not be completed within 13 weeks from the date when the application was made valid or within the agreed extension of time, the Service Director, Planning and Development / Head of Service – Development Management or, in their absence, the Deputy Head of Service may refuse the application on the grounds that the proposed development, in the absence of a Deed of Planning Obligation is not acceptable in planning terms.

ALTERNATIVELY, should this application be refused (including refusals on the direction of The Secretary of State or The Mayor) and appealed to the Secretary of State, the Service Director, Planning and Development / Head of Service – Development Management or, in their absence, the Deputy Head of Service be authorised to enter into a Deed of Planning Obligation under section 106 of the Town and Country Planning Act 1990 to secure to the heads of terms as set out in this report to Committee.

RECOMMENDATION B

That the grant of planning permission be subject to **conditions** to secure the following, and that there is delegated to each of the following: the Head of Development Management the Team Leader Major Applications and the Team Leader Planning Applications to make minor changes (additions removals or amendments) to the conditions:

List of Conditions:

1	Commencement
	<p>CONDITION: The development hereby permitted shall be begun not later than the expiration of three years from the date of this permission.</p> <p>REASON: To comply with the provisions of Section 91(1)(a) of the Town and Country Planning Act 1990 as amended by the Planning and Compulsory Purchase Act 2004 (Chapter 5).</p>
2	Approved Plans List
	<p>DRAWING AND DOCUMENT NUMBERS: The development hereby approved shall be carried out in accordance with the following approved plans:</p> <p>AC114-MAA-XX-DR-A-00001 P03 Location Plan; AC114-MAA-XX-DR-A-00002 P02 Existing Site Plan; AC114-MAA-XX-DR-A-00003 P02 Existing Ground and First Floor Plans; AC114-MAA-XX-DR-A-00004 P02 Existing Second and Third Floor Plans; AC114-MAA-XX-DR-A-00005 P02 Existing Fourth Floor Plan; AC114-MAA-XX-DR-A-00006 P02 Existing Roof Plan; AC114-MAA-XX-DR-A-00007 P02 Existing Elevations; AC114-MAA-XX-DR-A-00008 P02 Existing Sections; AC114-MAA-XX-DR-A-01001 P02 Proposed Site Plan; AC114-MAA-XX-DR-A-01002 P01 Proposed Phasing Plan; AC114-MAA-XX-DR-A-02001-P11 - Proposed Ground Floor Plan; AC114-MAA-XX-DR-A-02002-P08 - Proposed First Floor Plan; AC114-MAA-XX-DR-A-02003-P08 - Proposed Second Floor Plan; AC114-MAA-XX-DR-A-02004-P06 - Proposed Third Floor Plan; AC114-MAA-XX-DR-A-02005-P06 - Proposed Fourth Floor Plan; AC114-MAA-XX-DR-A-02006-P06 - Proposed Fifth Floor Plan; AC114-MAA-XX-DR-A-02007-P08 - Proposed Roof Plan; AC114-MAA-XX-DR-A-02011 P03 Proposed York Way and Warehouse 3 Ground Floor Plan; AC114-MAA-XX-DR-A-02012 P03 Proposed Warehouses 1, 2 and 4 Ground Floor Plan; AC114-MAA-XX-DR-A-02013-P04 - Proposed</p>

	<p>York Way and Wh 3 First Floor Plan; AC114-MAA-XX-DR-A-02014-P04 - Proposed Wh 1 2 4 First Floor Plan; AC114-MAA-XX-DR-A-02015-P04 - Proposed York Way and Wh 3 Second Floor Plan; AC114-MAA-XX-DR-A-02016-P04 - Proposed Wh 1 2 4 Second Floor Plan; AC114-MAA-XX-DR-A-02017-P04 - Proposed York Way and Wh 3 Third Floor Plan; AC114-MAA-XX-DR-A-02018-P04 - Proposed Wh 1 2 4 Third Floor Plan; AC114-MAA-XX-DR-A-02019-P04 - Proposed York Way and Wh 3 Fourth Floor Plan; AC114-MAA-XX-DR-A-02020-P04 - Proposed Wh 1 2 4 Fourth Floor Plan; AC114-MAA-XX-DR-A-02021-P04 - Proposed York Way and Wh 3 Roof Plan; AC114-MAA-XX-DR-A-02022-P04 - Proposed Wh 1 Roof Plan and Wh 2 and 4 Fifth Floor Plan; AC114-MAA-XX-DR-A-02024-P04 - Proposed Wh 2 4 Roof Plan; AC114-MAA-XX-DR-A-04001-P06 - Proposed North Elevation; AC114-MAA-XX-DR-A-04002 P03 Proposed East Elevation; AC114-MAA-XX-DR-A-04003 P03 Proposed West Elevation; AC114-MAA-XX-DR-A-04004 P03 Proposed Part South Elevation; AC114-MAA-XX-DR-A-04005 P04 Proposed Part South Elevation; AC114-MAA-XX-DR-A-04010 P02 Proposed North and South Elevations; AC114-MAA-XX-DR-A-05001 P04 Proposed Sections AA and BB; AC114-MAA-XX-DR-A-05002 P04 Proposed Section CC; AC114-MAA-XX-DR-A-05010 P03 Proposed Site Sections; AC114-MAA-XX-DR-A-06001-P02 - Ground Floor Inclusive Design Plan; AC114-MAA-XX-DR-A-06002-P02 - Second Floor Typical Upper Inclusive Design Plan;</p> <p>Air Quality Assessment Rev V1.0 dated 25/03/2022; BREEAM New Construction Pre-Assessment - Version 1 Rev C dated 11.04.2022; BREEAM Refurbishment Pre-Assessment - Version 1 Rev C dated 11.04.2022; Construction Logistics Plan (Including Site Waste Management May 2022; Contaminated Land Assessment – ref: J21100 Rev1 dated 21 July 2021; Circular Economy Statement Version 1B dated 21.11.22; Daylight and Sunlight Report ref: 4845 dated 19 April 2022; Design and Access Statement Revision P03 April 2022; Drainage Statement ref: 13675 dated Rev P1 May 2022; Economic Regeneration Statement - August 2022; Energy and Sustainability Statement Version 1 Rev A dated 29.03.22; Energy and Sustainability Addendum - October 2022; Fire Statement Version 02- 08.11.2022; Flood Risk Assessment ref: 13675 dated Rev P2 May 2022; Health Impact Assessment dated 29th April 2022; Letter from Jon Dingle dated 29th April 2022; Noise and Vibration Impact Assessment Rev 2 dated 22/04/2022; Planning Statement - May 2022; Planning Statement Addendum - August 2022; Preliminary Bat Roost Assessment Version 1 dated 11/02/22; Statement of Community Involvement – April 2022; Structural Engineering Methodology Report Rev P2 dated 21/10/22; Transport Assessment - May 2022; Travel Plan - May 2022; Townscape, Visual Impact and Heritage Assessment dated 13 April 2022; Whole Life Carbon Assessment Version 1A dated 30/09.22; Vale Royal GLA Spreadsheet; AC114_Vale Royal_Urban Greening Factor - June 2022;</p> <p>REASON: To comply with Section 70(1)(a) of the Town and Country Act 1990 as amended and the Reason for Grant and also for the avoidance of doubt and in the interest of proper planning.</p>
3	Materials (Details)
	<p>CONDITION: Details and samples of all facing materials shall be submitted to and approved in writing by the Local Planning Authority prior to any superstructure work commencing on site. The details and samples shall include:</p> <ol style="list-style-type: none"> a) solid brickwork (including brick panels and mortar courses) b) render (including colour, texture and method of application); c) window treatment (including sections and reveals); d) roofing materials; e) green procurement plan; f) any other materials to be used.

	<p>The development shall be carried out strictly in accordance with the details so approved and shall be maintained as such thereafter.</p> <p>REASON: In the interest of securing sustainable development and to ensure that the resulting appearance and construction of the development is of a high standard.</p>
4	Plumbing (No pipes to outside of building)
	<p>CONDITION: Notwithstanding the plans hereby approved, no plumbing, down pipes, rainwater pipes or foul pipes other than those shown on the approved plans shall be located to the external elevations of buildings hereby approved without obtaining express planning consent unless submitted to and approved in writing by the local planning authority as part of discharging this condition.</p> <p>REASON: The Local Planning Authority considers that such plumbing and pipes would potentially detract from the appearance of the building and undermine the current assessment of the application.</p>
5	Roof Level Structures (Details)
	<p>CONDITION: Details of any roof-top structures/enclosures shall be submitted to and approved in writing by the Local Planning Authority prior to relevant works commencing on site. The details shall include the location, height above roof level, specifications and cladding and shall relate to:</p> <p>a) roof-top plant; b) ancillary enclosures/structure; and c) lift overrun</p> <p>The development shall be carried out strictly in accordance with the details so approved and shall be maintained as such thereafter.</p> <p>REASON: In the interest of good design and also to ensure that the Authority may be satisfied that any roof-top plant, ancillary enclosure/structure and/or the lift overruns do not have a harmful impact on the surrounding streetscene.</p>
6	Construction Management Plan (Details and Compliance)
	<p>CONDITION: A Construction Management Plan (CMP) and a Construction Logistics Plan (CLP) shall be submitted to and approved in writing by the Local Planning Authority, in consultation with Transport for London (TfL), as follows:</p> <p>a) Prior to demolition works: a Demolition Management and Logistics Plan b) Prior to construction works: a Construction Management and Logistics Plan</p> <p>The reports shall assess the impacts during the demolition construction phases of the development on surrounding streets, including TfL red routes, and other occupiers together with means of mitigating any identified impacts. The CMP must refer to the new LBI Code of Practice for Construction Sites.</p> <p>The CMP should ensure that all relevant mitigation measures are included in line with the Air Quality report “medium” risk requirements, including continuous monitoring.</p> <p>The development shall be carried out strictly in accordance with the approved documents throughout the demolition and construction periods.</p>

	REASON: In the interests of neighbouring amenity, highway safety, and the free flow of traffic on streets, and to mitigate the impacts of the development.
7	Plant Noise (Compliance)
	<p>CONDITION: The design and installation of new items of fixed plant shall be such that when operating the cumulative noise level $L_{Aeq Tr}$ arising from the proposed plant, measured or predicted at 1m from the facade of the nearest noise sensitive premises, shall be a rating level of at least 5dB(A) below the background noise level $L_{AF90 Tbg}$. The measurement and/or prediction of the noise should be carried out in accordance with the methodology contained within BS 4142: 2014+A1:2019.</p> <p>REASON: To ensure the protection of neighbouring amenity in respect to noise and vibration.</p>
8	Refuse, Delivery & Servicing (Details)
	<p>CONDITION: Details of the delivery & servicing strategy and site-wide waste strategy for the development, shall be submitted to and approved in writing by the Local Planning Authority prior to occupation of the development hereby approved</p> <p>The development shall be carried out and operated strictly in accordance with the details and waste management strategy so approved.</p> <p>REASON: To secure the necessary physical waste enclosures to support the development and to ensure that responsible waste management practices are adhered to.</p>
9	Lighting (Details and Compliance)
	<p>CONDITION: Details of measures to adequately mitigate light pollution affecting the neighbouring Site of Importance for Nature Conservation (SINC) shall be submitted to and approved in writing by the Local Planning Authority prior to superstructure works commencing on site and subsequently implemented prior to first occupation of the development hereby permitted. These measures might include:</p> <ul style="list-style-type: none"> • Automated roller blinds; • Lighting strategies that reduce the output of luminaires closer to the façades; • Light fittings controlled through the use of sensors. <p>The blinds are to be activated via an automated light sensor system and shall remain closed between dusk and sunrise each day, and shall cover the full extent of the windows</p> <p>The details shall include the location and full specification of: all lamps; light levels/spill lamps, floodlights, support structures, and hours of operation. The lighting measures shall be carried out strictly in accordance with the details so approved, shall be installed prior to occupation of the development and shall be maintained as such thereafter.</p> <p>The approved mitigation measures shall be implemented strictly in accordance with the approved details and shall be permanently maintained thereafter.</p> <p>REASON: To ensure the adjacent SINC will not be adversely affected by light pollution.</p>
10	Restriction on Class E uses (Compliance)
	<p>CONDITION: Operation of Section 55(2)(f) of the Town and Country Planning Act 1990 is precluded with regard to the Class E uses on the ground to fifth floor levels, except the permitted use(s) hereby approved:</p>

	<p>A) Part ground floor to part fifth floor Flexible Class E(g)(i), (ii) and (iii) Uses – as shown on plan no’s AC114-MAA-XX-DR-A-02001-P11 - Proposed Ground Floor Plan; AC114-MAA-XX-DR-A-02002-P08 - Proposed First Floor Plan; AC114-MAA-XX-DR-A-02003-P08 - Proposed Second Floor Plan; AC114-MAA-XX-DR-A-02004-P06 - Proposed Third Floor Plan; AC114-MAA-XX-DR-A-02005-P06 - Proposed Fourth Floor Plan; AC114-MAA-XX-DR-A-02006-P06 - Proposed Fifth Floor Plan;</p> <p>and for no other purpose, including any purpose falling solely under Class E of the Schedule 2 of the Town and Country Planning (Use Classes) Order 1987 (as amended) and subsequent Town and Country Planning (Use Classes) (Amendment) (England) Regulations 2020) or any provision equivalent to that Class in any statutory instrument revoking and re-enacting that Order with or without modification.</p> <p>REASON: For the avoidance of doubt and to ensure that the Local Planning Authority can restrict the use of the building to this specific commercial use(s) only and retains control over the change of use of the building in the future. This is relevant due to the specific amenity implications of some of the uses that fall within Class E that necessitate additional noise/odour/acoustic/operational hours and other considerations. The assessment has found the above uses and locations to be acceptable subject to further details to be approved. A move away from the above permitted uses in identified locations within the building could give rise to unacceptable amenity or other impacts.</p>
11	<p>Restriction of PD Rights - Class E to residential (Compliance)</p>
	<p>Notwithstanding the provisions of Schedule 2, Part 3, Class MA the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modifications), no change of use from Class E (commercial, business and service) to a use falling within Class C3 (dwellinghouses) shall take place without obtaining the express planning permission from the Local Planning Authority.</p> <p>REASON: For the avoidance of doubt and to ensure that the Local Planning Authority can restrict the use of the building to this specific use only, in order to protect the supply of office and commercial floorspace in this location.</p>
12	<p>Fire Safety Strategy (Details/Compliance)</p>
	<p>CONDITION: The details and measures set out in the Fire Statement by Bureau Veritas Group Issue 02 dated 08.11.2022, shall be implemented in accordance with the approved document, unless otherwise agreed in writing by the Local Planning Authority.</p> <p>Should any subsequent change(s) be required to secure compliance with the submitted Fire Safety Strategy, a revised Fire Statement would need to be submitted to and approved by the Local Planning Authority.</p> <p>The development shall be carried out in accordance with the Fire Safety Strategy under this condition and shall be maintained as such thereafter.</p> <p>REASON: To ensure that the development incorporates the necessary fire safety measures in accordance with the Mayor’s London Plan Policy D12</p>
13	<p>Energy Strategy (Compliance)</p>
	<p>CONDITION: The energy efficiency measures/features and renewable energy technology, which shall provide for a reduction in carbon emissions of no less than 39.8% in total (total</p>

	<p>emissions) and 31.98% (regulated emissions) as detailed within the Energy & Sustainability Statement prepared by SRE Version 1 Rev A dated 29.03.2022, and the Energy & Sustainability Addendum, prepared by SRE, Version 2 Rev C dated 19.10.2022, shall be installed and operational prior to the first occupation of the development.</p> <p>Should there be any change to the energy features/measures within the approved Energy Statement and Sustainable Design & Construction Statement, a revised strategy shall be submitted to and agreed in writing by the Local Planning Authority prior to the occupation of the development.</p> <p>REASON: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO₂ emission reduction targets by energy efficient measures/features and renewable energy are met.</p>
14	Energy (Details)
	<p>CONDITION: Prior to occupation of the development hereby approved, the following information is required to address outstanding Energy matters:</p> <ul style="list-style-type: none"> - Details of a protected pipe route to the site boundary and confirmation that adequate plant room space will be provided for necessary heat exchangers and plant for connection to a future District Energy Network, shall be submitted to and approved in writing by the Local Planning Authority. - Justification for the inclusion of active cooling by the use of thermal modelling of the proposed development without the cooling in place. - Provision of a Draft Green Performance Plan <p>REASON: To ensure the development is designed to be able to connect to a future Decentralised Energy Network.</p>
15	BREEAM (Compliance)
	<p>CONDITION: The development shall achieve a BREEAM New Construction rating (2018) of no less than 'Excellent' and shall use reasonable endeavours to achieve a rating of 'Outstanding'.</p> <p>No building shall be occupied until a final Certificate has been issued certifying that the highest feasible BREEAM (or any such equivalent national measure of sustainable building which replaces that scheme) rating has been achieved for this development unless the Local Planning Authority agrees in writing to an extension of the period by which a Certificate is issued.</p> <p>REASON: To ensure that the development achieves the highest feasible BREEAM rating level to ensure that the development contributes to mitigating and adapting to climate change and to meeting targets to reduce carbon dioxide emissions.</p>
16	Green Roof (Details and Compliance)
	<p>CONDITION: Notwithstanding the plans hereby approved, the bio-solar green roof shall be maximised across the development. Details shall be submitted to and approved in writing to the Local Planning Authority prior to practical completion of the development hereby approved, demonstrating the following:</p> <ul style="list-style-type: none"> a) how the extent of green/brown roof has been maximised b) that the green/brown roofs are biodiversity based with extensive substrate base (depth 80 -150mm); and

	<p>c) planted/seeded with a mix of species within the first planting season following the practical completion of the building works (the seed mix shall be focused on wildflower planting, and shall contain no more than a maximum of 25% sedum).</p> <p>d) Submission of a green roof maintenance plan.</p> <p>The bio-solar green roof shall not be used as an amenity or sitting out space of any kind whatsoever and shall only be accessed for the purpose of essential maintenance or repair, or escape in case of emergency.</p> <p>The bio-solar green roofs shall be installed strictly in accordance with the details as approved, shall be laid out within 3 months or the next available appropriate planting season after completion of the external development works / first occupation, and shall be maintained as such thereafter.</p> <p>REASON: In order to ensure the development maximises opportunities to help boost biodiversity and minimise water run-off.</p>
17	Solar PVs (Details and Compliance)
	<p>CONDITION: Prior to the commencement of relevant works on site, details of the proposed 131no. Solar Photovoltaic Panels at the site shall be submitted to and approved in writing by the Local Planning Authority. These details shall include but not be limited to:</p> <ul style="list-style-type: none"> - Location; - Area of panels; - Design (including elevation plans); and - How the design of the PVs would not adversely affect the provisions of green roofs on site. <p>The solar photovoltaic panels as approved shall be installed prior to the first occupation of the development and retained as such permanently thereafter.</p> <p>REASON: In the interest of addressing climate change and to secure sustainable development and to secure high quality design in the resultant development.</p>
18	Bird & Bat Boxes (Details)
	<p>CONDITION: Notwithstanding the approved plans, prior to commencement of relevant works, details of bird and bat boxes (including swift boxes) shall be submitted to and approved in writing by the Local Planning Authority.</p> <p>The details approved shall be installed prior to the first occupation of the building, and shall be maintained as such thereafter.</p> <p>REASON: To ensure the development provides the maximum possible provision towards creation of habitats and valuable areas for biodiversity</p>
19	Biodiversity measures (Compliance)
	<p>CONDITION: The biodiversity measures and ecological enhancements identified within the AC114_Vale Royal_Urban Greening Factor - June 2022; shall be implemented prior to the occupation of the development hereby approved.</p> <p>The details hereby approved shall be maintained as such thereafter.</p> <p>REASON: In the interest of biodiversity.</p>

20	Secured by Design (Compliance)
	<p>CONDITION: Notwithstanding the approved plans and documents, prior to superstructure works commencing of the development hereby approved, the development shall achieve Secured by Design – Commercial Development accreditation. The development shall be carried out strictly in accordance with the details set out in the accreditation and shall be maintained as such thereafter.</p> <p>REASON: In the interests of safety and security.</p>
21	Cycle Parking (Details and Compliance)
	<p>CONDITION: Notwithstanding the plans hereby approved, details of bicycle storage areas, including cycle parking product specification, which shall be secure and provide for no less than 32 cycle spaces and a total of 6 no. accessible cycle parking spaces shall be submitted and approved in writing by the Local Planning Authority.</p> <p>The approved cycle storage shall be provided prior to the first occupation of the development hereby approved, shall be maintained as such thereafter and no change therefrom shall take place unless otherwise agreed in writing by the Local Planning Authority.</p> <p>REASON: To ensure adequate and suitable bicycle parking is available and easily accessible on site and to promote sustainable modes of transport.</p>
22	Lifts (Compliance)
	<p>CONDITION: All lifts hereby approved shall be installed and operational prior to the first occupation of the floorspace hereby approved. The lifts should be maintained throughout the lifetime of the development.</p> <p>REASON: To ensure that inclusive and accessible routes are provided throughout the floorspace at all floors and also accessible routes through the site are provided to ensure no one is excluded from full use and enjoyment of the site.</p>
23	No mechanical plant equipment (Compliance)
	<p>CONDITION: Notwithstanding the plans hereby approved, no additional mechanical plant equipment shall be installed without express consent from the Local Planning Authority.</p> <p>REASON: To protect the character and appearance of the surrounding area.</p>
24	Contaminated Land (Details and Compliance)
	<p>CONDITION: "Prior to the commencement of development the following assessment in response to the NPPF and in accordance with Land Contamination Risk Management (LCRM) guidance (Environment Agency as updated 2021) and BS10175:2011+A2:2017 shall be submitted to and approved in writing by the Local Planning Authority:</p> <p>a) A land contamination investigation. The investigation shall be based upon and target the risks identified in the approved preliminary risk assessment and shall provide provisions for, where relevant, the sampling of soil, soil vapour, ground gas, surface and groundwater. All works must be carried out in compliance with and by a competent person who conforms to CLR 11: Model Procedures for the Management of Land Contamination or the current UK requirements for sampling and testing.</p> <p>Following the agreement to details relating to point a); details of the following works shall be submitted to and approved in writing by the Local Planning Authority prior to any superstructure works commencing on site:</p>

	<p>b) A remediation method statement of any necessary land contamination remediation works arising from the land contamination investigation. This statement shall detail any required remediation works and shall be designed to mitigate any remaining risks identified in the approved site investigation. The development shall be carried out strictly in accordance with the investigation and any scheme of remedial works so approved and no change therefrom shall take place without the prior written approval of the Local Planning Authority.</p> <p>If, during development, contamination not previously identified is found to be present at the site, the Council is to be informed immediately and no further development (unless otherwise agreed in writing by the Council) shall be carried out until a report indicating the nature of the contamination and how it is to be dealt with is submitted to, and agreed in writing by, the Council. All works must be carried out in compliance with and by a competent person who conforms to CLR 11: Model Procedures for the Management of Land Contamination (Defra 2004) or the current UK requirements for sampling and testing.</p> <p>c) Following completion of measures identified in the approved remediation scheme a verification report, that demonstrates the effectiveness of the remediation carried out, must be produced which is subject to the approval in writing of the Local Planning Authority in accordance with part b). This report shall include: details of the remediation works carried out; results of any verification sampling, testing or monitoring including the analysis of any imported soil; all waste management documentation showing the classification of waste, its treatment, movement and disposal; and the validation of gas membrane placement. All works must be carried out in compliance with and by a competent person who conforms to CLR 11: Model Procedures for the Management of Land Contamination or the current UK requirements for sampling and testing.</p> <p>REASON: In order to minimise the impacts of land contamination and due to the contaminated land report advising further investigation due to the potential for an Underground Storage Tank (which may or may not be removed) in the south of the site.</p>
25	<p>Cycle Parking Monitoring of usage and demand (Details)</p>
	<p>CONDITION: During the first six month period following the commencement of the use of the self-storage development hereby approved, the applicant shall monitor the usage of the cycle parking within the development. The details of the usage shall be submitted to the Local Planning Authority. Should the demand indicate that further cycle parking facilities are required, the applicant shall take measures to provide the additional 32 cycle parking spaces located within 184 York Way as indicated on plan AC114-MAA-XX-DR-A-02001-P11 - Proposed Ground Floor Plan.</p> <p>REASON: To ensure adequate and suitable bicycle parking is available and easily accessible on site and to promote sustainable modes of transport.</p>
26	<p>Refuse, Delivery & Servicing (Details)</p>
	<p>CONDITION: Details of the delivery & servicing strategy and site-wide waste strategy for the development, shall be submitted to and approved in writing by the Local Planning Authority prior to occupation of the development hereby approved</p> <p>The development shall be carried out and operated strictly in accordance with the details and waste management strategy so approved. The physical enclosures shall be provided/erected prior to the first occupation of the development and shall be maintained as such thereafter.</p>

	<p>REASON: To secure the necessary physical waste enclosures to support the development and to ensure that responsible waste management practices are adhered to.</p>
27	<p>Inclusive Design Principles (Details and Compliance)</p> <p>CONDITION: Prior to occupation of the development, to ensure compliance with the Inclusive Design SPD and Local Plan inclusive design policies, the following amendments/details shall be provided unless otherwise agreed in writing:</p> <ul style="list-style-type: none"> a) Details regarding the location of the end of trip facilities including lockers at each cycle parking location; b) Details of accessible internal signage regarding the location of accessible parking, the loading bays, and cycle storage. c) Details of the layout of the service yard indicating clear delineation between pedestrian/vehicle spaces. d) Details of the rail design and surface materials associated with the ramp compliant with Inclusive Design SPD 4.3. e) Details of the turning space for the accessible WCs of at least 1500mm diameter and compliance with Inclusive Design SPD 4.8. f) Details of the turning space in the ground floor WC and compliance with Inclusive Design SPD 4.8. g) Details of the Upper floor shower to comply with the principles of inclusive design, provided within the same room as the WC to facilitate access to those facilities. h) Details of furniture layouts and the kitchen countertop within the breakout areas to include a low worktop, knee space as well as turning space. i) Details of the dimensions of the fourth floor landing to meet the minimum Building Regulations Part M Other than dwellings requirements of 1200mm length. <p>The internal service yard can be used as drop-off space when the gate is open within the hours of 8am to 7pm Monday to Sunday.</p> <p>The internal dimensions of all lifts are to be a minimum of 1100mm x 1400mm.</p> <p>REASON: In order to accommodate for needs of employees and visitors with characteristics protected by Equality Act 2010.</p>
28	<p>Removal of vehicular access to 184 York Way (Compliance)</p> <p>CONDITION: Following completion of the works hereby approved, there shall be no vehicular access into the site at 184 York Way throughout the lifetime of the development. All vehicles and deliveries in connection with the development hereby approved, shall be undertaken via 14-22 Vale Royal in accordance with the approved delivery and servicing arrangements.</p> <p>REASON: To maintain highway safety.</p>
29	<p>Whole Life Carbon Assessment (Details)</p> <p>CONDITION: Prior to the occupation of the development, an updated Whole Life Carbon Assessment shall be submitted including further details regarding the process for the options considered and actions to be taken to reduce Whole Life Carbon emissions with reference to table 2.2 of the GLA guidance.</p> <p>The details shall include identification and commitments to undertake actions as to how the reduction in emissions is going to be achieved.</p> <p>REASON: In the interests of sustainable development and to maximise on-site carbon dioxide savings.</p>

30	Post-construction Whole Life-Cycle Carbon Assessment (Details)
	<p>CONDITION: Prior to the occupation of the development the post-construction tab of the GLA's Whole Life-Cycle Carbon Assessment template should be completed in line with the GLA's Whole Life-Cycle Carbon Assessment Guidance.</p> <p>The post-construction assessment should be submitted to, and approved in writing by, the local planning authority, prior to occupation of the development.</p> <p>REASON: In the interests of sustainable development and to maximise on-site carbon dioxide savings.</p>
31	Post-construction Monitoring
	<p>CONDITION: Prior to the occupation of each phase of development, a postconstruction monitoring report should be completed in line with the GLA's Circular Economy Statement Guidance. The post-construction monitoring report shall be submitted along with any supporting evidence as per the guidance to, and approved in writing by, the local planning authority, prior to occupation of each phase of development.</p> <p>REASON: In the interests of sustainable waste management and in order to maximise the re-use of materials.</p>

List of Informatives:

1	S106
	<p>SECTION 106 AGREEMENT</p> <p>You are advised that this permission has been granted subject to a legal agreement under Section 106 of the Town and Country Planning Act 1990.</p>
2	Definition of 'Superstructure' and 'Practical Completion'
	<p>A number of conditions attached to this permission have the time restrictions 'prior to superstructure works commencing on site' and/or 'following practical completion'. The council considers the definition of 'superstructure' as having its normal or dictionary meaning, which is: the part of a building above its foundations. The council considers the definition of 'practical completion' to be: when the work reaches a state of readiness for use or occupation even though there may be outstanding works/matters to be carried out.</p>
4	Rooftop Plant
	<p>The applicant is advised that any additional roof top plant not shown on the approved plans will require a separate planning application.</p>
5	Construction Works
	<p>Noise from demolition and construction works is subject to control under the Control of Pollution Act 1974. You must carry out any building works that can be heard at the boundary of the site only between 08.00 and 18.00 hours Monday to Friday and 08.00 to 13.00 on Saturday and not at all on Sundays and Public Holidays. You are advised to consult the Pollution Team, Islington Council, 222 Upper Street London N1 1XR (Tel. No. 020 7527 3258</p>

	<p>or by email pollution@islington.gov.uk) or seek prior approval under Section 61 of the Act if you anticipate any difficulty in carrying out construction other than within the hours stated above.</p>
6	<p>Highway Requirements</p> <p>Compliance with sections 168 to 175 and of the Highways Act, 1980, relating to “Precautions to be taken in doing certain works in or near streets or highways”. This relates, to scaffolding, hoarding and so on. All licenses can be acquired through streetworks@islington.gov.uk. All agreements relating to the above need to be in place prior to works commencing.</p> <p>Compliance with section 174 of the Highways Act, 1980 - “Precautions to be taken by persons executing works in streets.” Should a company/individual request to work on the public highway a Section 50 license is required. Can be gained through streetworks@islington.gov.uk. Section 50 license must be agreed prior to any works commencing. Compliance with section 140A of the Highways Act, 1980 – “Builders skips: charge for occupation of highway. Licenses can be gained through streetworks@islington.gov.uk.</p> <p>Compliance with sections 59 and 60 of the Highway Act, 1980 – “Recovery by highways authorities etc. of certain expenses incurred in maintaining highways”. Haulage route to be agreed with streetworks officer. Contact streetworks@islington.gov.uk. Joint condition survey required between Islington Council Highways and interested parties before commencement of building works to catalogue condition of streets and drainage gullies. Contact highways.maintenance@islington.gov.uk.</p>
7	<p>Highways Requirements (2)</p> <p>Joint condition survey required between Islington Council Highways and interested parties before commencement of building works to catalogue condition of streets and drainage gullies. Contact highways.maintenance@islington.gov.uk Approval of highways required and copy of findings and condition survey document to be sent to planning case officer for development in question.</p> <p>Temporary crossover licenses to be acquired from streetworks@islington.gov.uk. Heavy duty vehicles will not be permitted to access the site unless a temporary heavy duty crossover is in place.</p> <p>Highways re-instatement costing to be provided to recover expenses incurred for damage to the public highway directly by the build in accordance with sections 131 and 133 of the Highways Act, 1980.</p> <p>Before works commence on the public highway planning applicant must provide Islington Council’s Highways Service with six months’ notice to meet the requirements of the Traffic Management Act, 2004.</p> <p>Development will ensure that all new statutory services are complete prior to footway and/or carriageway works commencing. Works to the public highway will not commence until hoarding around the development has been removed. This is in accordance with current Health and Safety initiatives within contractual agreements with Islington Council’s Highways contractors.</p>
8	<p>Highways Requirement (3)</p>

	<p>Alterations to road markings or parking layouts to be agreed with Islington Council Highways Service. Costs for the alterations of traffic management orders (TMO's) to be borne by developer.</p> <p>All lighting works to be conducted by Islington Council Highways Lighting. Any proposed changes to lighting layout must meet the approval of Islington Council Highways Lighting. NOTE: All lighting works are to be undertaken by the PFI contractor not a nominee of the developer. Consideration should be taken to protect the existing lighting equipment within and around the development site.</p> <p>Any costs for repairing or replacing damaged equipment as a result of construction works will be the responsibility of the developer, remedial works will be implemented by Islington's public lighting at cost to the developer. Contact streetlights@islington.gov.uk Any damage or blockages to drainage will be repaired at the cost of the developer.</p> <p>Works to be undertaken by Islington Council Highways Service. Section 100, Highways Act 1980. Water will not be permitted to flow onto the public highway in accordance with Section 163, Highways Act 1980 Public highway footway cross falls will not be permitted to drain water onto private land or private drainage</p>
9	Community Infrastructure Levy (CIL)
	<p>Under the terms of the Planning Act 2008 (as amended) and Community Infrastructure Levy Regulations 2010 (as amended), this development is liable to pay the London Borough of Islington Community Infrastructure Levy (CIL) and the Mayor of London's Community Infrastructure Levy (CIL).</p> <p>The Council will issue a CIL Liability Notice stating the CIL amount that will be payable on the commencement of the development. Failure to pay CIL liabilities when due will result in the Council imposing surcharges and late payment interest.</p> <p>Further information and all CIL forms are available on the Planning Portal at www.planningportal.gov.uk/cil, and the Islington Council website at www.islington.gov.uk/cil. CIL guidance is available on the GOV.UK website at www.gov.uk/guidance/community-infrastructure-levy.</p>
10	Thames Water
	<p>The proposed development is located within 15m of our underground water assets and as such we would like the following informative attached to any approval granted. The proposed development is located within 15m of Thames Waters underground assets, as such the development could cause the assets to fail if appropriate measures are not taken. Please read our guide 'working near our assets' to ensure your workings are in line with the necessary processes you need to follow if you're considering working above or near our pipes or other structures.</p> <p>https://developers.thameswater.co.uk/Developing-a-large-site/Planning-your-development/Working-near-or-diverting-ourpipes.</p> <p>Should you require further information please contact Thames Water. Email: developer.services@thameswater.co.uk</p> <p>Please read our guide 'working near our assets' to ensure your workings will be in line with the necessary processes you need to follow if you're considering working above or near our pipes or other structures. https://www.thameswater.co.uk/developers/larger-scale-developments/planning-your-development/working-near-our-pipes Should you require further information please contact Thames Water. Email: developer.services@thameswater.co.uk</p>

Phone: 0800 009 3921 (Monday to Friday, 8am to 5pm) Write to: Thames Water Developer Services, Clearwater Court, Vastern Road, Reading, Berkshire RG1 8DB

There are public sewers crossing or close to your development. If you're planning significant work near our sewers, it's important that you minimize the risk of damage. We'll need to check that your development doesn't limit repair or maintenance activities, or inhibit the services we provide in any other way. The applicant is advised to read our guide working near or diverting our pipes. <https://www.thameswater.co.uk/developers/larger-scale-developments/planning-your-development/working-near-our-pipes>

With regard to SURFACE WATER drainage, Thames Water would advise that if the developer follows the sequential approach to the disposal of surface water we would have no objection. Management of surface water from new developments should follow Policy SI 13 Sustainable drainage of the London Plan 2021. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. Should you require further information please refer to our website. <https://www.thameswater.co.uk/developers/larger-scale-developments/planning-your-development/working-near-our-pipes>

As required by Building regulations part H paragraph 2.36, Thames Water requests that the Applicant should incorporate within their proposal, protection to the property to prevent sewage flooding, by installing a positive pumped device (or equivalent reflecting technological advances), on the assumption that the sewerage network may surcharge to ground level during storm conditions. If as part of the basement development there is a proposal to discharge ground water to the public network, this would require a Groundwater Risk Management Permit from Thames Water. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. We would expect the developer to demonstrate what measures will be undertaken to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 02035779483 or by emailing trade.effluent@thameswater.co.uk . Application forms should be completed on line via www.thameswater.co.uk. Please refer to the Wholesale; Business customers; Groundwater discharges section.

Thames Water would advise that with regard to WASTE WATER NETWORK and SEWAGE TREATMENT WORKS infrastructure capacity, we would not have any objection to the above planning application, based on the information provided.

The proposed development is located within 15 metres of our underground waste water assets and as such we would like the following informative attached to any approval granted. "The proposed development is located within 15 metres of Thames Waters underground assets and as such, the development could cause the assets to fail if appropriate measures are not taken. Please read our guide 'working near our assets' to ensure your workings are in line with the necessary processes you need to follow if you're considering working above or near our pipes or other structures.

<https://developers.thameswater.co.uk/Developing-a-large-site/Planning-your-development/Working-near-or-diverting-our-pipes>

Should you require further information please contact Thames Water. Email: developer.services@thameswater.co.uk Phone: 0800 009 3921 (Monday to Friday, 8am to 5pm) Write to: Thames Water, Developer Services, Clearwater Court, Vastern Road, Reading, Berkshire RG1 8DB

As the proposed refurbishment scheme of the existing warehouse, with alterations, extensions and partial change of use to create mixed use (Classes B8 and E(g)) buildings arranged over ground, first, second, part third, part fourth and part fifth floors and associated plant delivered in two phases and not major redevelopment or demolition. And Based on the Foul Water Drainage Strategy; The foul drainage system will utilise the existing network. The foul water drainage strategy will involve the collection of drainage stacks through laterals and connect to the existing foul water network and continue to use the existing outlet into the Thames Water public combined sewer in Vale Royal and York Way.

And based on The development does not propose to alter the existing drainage network as there is limited opportunity to include SuDS into the development with the entire site occupied by the existing building which is being retained. The surface water run-off rate of the proposed development will however be lower than that of the existing building due to the inclusion of green roof.

The discharge wizard from Microdrainage indicates that the existing brownfield run-off volume of a 6 hour storm with 100-year return period for the site is 164.45m³ (See Appendix D). The proposed greenfield run-off volume will be approximately reduced by 3-5% due to the inclusion of a green roof.

We would expect the developer to demonstrate what measures will be undertaken to minimise groundwater discharges into the public sewer. Groundwater discharges typically result from construction site dewatering, deep excavations, basement infiltration, borehole installation, testing and site remediation. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. Should the Local Planning Authority be minded to approve the planning application, Thames Water would like the following informative attached to the planning permission: "A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. We would expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 020 3577 9483 or by emailing trade.effluent@thameswater.co.uk . Application forms should be completed on line via www.thameswater.co.uk. Please refer to the Wholesale; Business customers; Groundwater discharges section.

Water

Comments

If you are planning on using mains water for construction purposes, it's important you let Thames Water know before you start using it, to avoid potential fines for improper usage. More information and how to apply can be found online at thameswater.co.uk/buildingwater.

On the basis of information provided, Thames Water would advise that with regard to water network and water treatment infrastructure capacity, we would not have any objection to the above planning application. Thames Water recommends the following informative be attached to this planning permission. Thames Water will aim to provide customers with a minimum pressure of 10m head (approx. 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.

A Trade Effluent Consent will be required for any Effluent discharge other than a 'Domestic Discharge'. Any discharge without this consent is illegal and may result in prosecution. (Domestic usage for example includes - toilets, showers, washbasins, baths, private swimming pools and canteens). Typical Trade Effluent processes include: -

	<p>Laundrette/Laundry, PCB manufacture, commercial swimming pools, photographic/printing, food preparation, abattoir, farm wastes, vehicle washing, metal plating/finishing, cattle market wash down, chemical manufacture, treated cooling water and any other process which produces contaminated water. Pre-treatment, separate metering, sampling access etc may be required before the Company can give its consent. Applications should be made at https://wholesale.thameswater.co.uk/Wholesale-services/Business-customers/Trade-effluent or alternatively to Waste Water Quality, Crossness STW, Belvedere Road, Abbeywood, London. SE2 9AQ. Telephone: 020 3577 9200.</p> <p>As per Building regulations part H paragraph 2.21, Drainage serving kitchens in commercial hot food premises should be fitted with a grease separator complying with BS EN 1825-:2004 and designed in accordance with BS EN 1825-2:2002 or other effective means of grease removal. Thames Water further recommend, in line with best practice for the disposal of Fats, Oils and Grease, the collection of waste oil by a contractor, particularly to recycle for the production of bio diesel.</p> <p>Failure to implement these recommendations may result in this and other properties suffering blocked drains, sewage flooding and pollution to local watercourses. Please refer to our website for further information : www.thameswater.co.uk/help</p> <p><u>Water Comments</u></p> <p>There are water mains crossing or close to your development. Thames Water do NOT permit the building over or construction within 3m of water mains. If you're planning significant works near our mains (within 3m) we'll need to check that your development doesn't reduce capacity, limit repair or maintenance activities during and after construction, or inhibit the services we provide in any other way. The applicant is advised to read our guide working near or diverting our pipes. https://www.thameswater.co.uk/developers/larger-scale-developments/planning-your-development/working-near-our-pipes</p> <p>On the basis of information provided, Thames Water would advise that with regard to water network infrastructure capacity, we would not have any objection to the above planning application. Thames Water recommend the following informative be attached to this planning permission.</p>
11	Fire Safety
	<p>It is recommended that you obtain technical advice regarding compliance with the Building Regulations (and/including matters relating to fire safety and evacuation) prior to any further design work commencing and prior to the selection of materials. In particular, you should seek further guidance regarding the design of the external fabric (including windows) to limit the potential for spread of fire to other buildings.</p> <p>Islington's Building Control team has extensive experience in working with clients on a wide range of projects. Should you wish to discuss your project and how Islington Building Control may best advise you regarding compliance with relevant (building control) regulations, please contact Building Control on 020 7527 5999 or by email on Building Control@islington.gov.uk.</p>
13	Network Rail Informatives
	<p>Item 1. Issues - Environmental pollution (Dust, noise etc.) to the operational railway. Reasons/Mitigations: Any designs, construction and locations of installations / equipment on the outside party land should take into account possible effects of noise, vibration and generation of airborne dust in regard to the operational railway. Contractors are expected to use the 'best practical means' for controlling pollution and environmental nuisance complying all</p>

current standards and regulations. Design and construction methodologies should consider mitigation measures to minimise the generation of airborne dust, noise and vibration in regard to the operational railway. Demolition work shall be carried out behind hoardings and dust suppression systems are to be employed to risk to the operational line.

Item 2. Issues - Proximity of the development to the Network Rail infrastructure and boundary fence and adequate space for future maintenance of the development.

Reasons/Mitigations:

It is recommended that all works be situated at least 3 metres from NR boundary fence or wall, to allow construction and any future maintenance work to be carried out without involving entry or encroachment onto Network Rail's land. Where trees exist on Network Rail land, design of any foundations close to the boundary must take into account the effects of root penetration in accordance with the relevant guidelines.

Item 3. Issues - Encroachment on the boundary fence, interference with sensitive equipment, space for inspection and maintenance of the railway infrastructure.

Reasons/Mitigations:

The developer / contractor must ensure that the construction foot print is secured by a new fence, set back from the existing Network Rail boundary fence line to achieve sufficient clearance for inspection and maintenance of Network Rail fence line as well as provide access for inspection and maintenance of the site in future without importing risks to the operational railway. This would normally be minimum 3.0m – 5.0m from the boundary fence depending on the adjacent NR assets.

Item 4. Issues - Collapse of lifting equipment adjacent to the boundary fence/line.

Reasons/Mitigations:

Operation of mobile cranes shall comply with CPA Good Practice Guide 'Requirements for Mobile Cranes Alongside Railways Controlled by Network Rail'. Operation of Tower Crane shall also comply with CPA Good Practice Guide 'Requirements for Tower Cranes Alongside Railways Controlled by Network Rail'. Operation of Piling Rig shall comply with Network Rail standard 'NR-L3-CIV-0063 regarding piling, crane operations and elevated platforms adjacent to the railway'. Collapse radius of equipment (including compound collapse radius) shall not fall within 4m from the railway boundary unless possession and isolation of NR lines have been arranged or agreed with Network Rail prior to works commencing.

Item 5. Issues - Effect of artificial lighting and human factor effects from glare on Train Drivers, Glint/glare from reflective surfaces to signals affecting train movements.

Reasons/Mitigations:

Lightings associated with the proposed works must not interfere with sighting of signalling apparatus and/or train drivers' line of sight. The location and colour of lights must not give rise to the potential for confusion with the signalling arrangements on the railway. A Glint & Glare Assessment will be required for submission to Network Rail following for which the developer shall obtain Network Rail's Asset Protection Engineer's consent following acceptance reviews of the assessment report.

Item 6. Issues - Effects of development on Biodiversity

Reasons/Mitigations:

The outside party shall consider the effects of their proposed works on the environment in close proximity to Network Rail land, such as effects on protected birds, invasive plants and protected trees.

	<p>Item 7. Issues - Potential impact on the adjacent railway infrastructure from the construction activities. Reasons/Mitigations: The developer /contractor shall provide applicable construction methodologies relating to the works that may import risks to the railway and potential disruption to railway services and infrastructure for acceptance reviews prior to works commencing. All works shall be risk assessed so that necessary risk control and mitigation measures are put in place to avoid adverse effect to adjacent NR infrastructure. Railway infrastructure including embankment and bridges shall not be loaded with additional surcharge from the proposed development. Increased surcharge on railway embankment and / cutting imports risks of ground instability which can cause settlement on Network Rail infrastructure (Overhead Line Equipment / gantries, track, embankment, boundary fence, etc.). Where applicable, works, both temporary and permanent, shall be designed and constructed, subject to acceptance reviews of the designs so that they will have no influence on stability of any adjacent NR asset.</p> <p>Item 8. Issues - Collapse of structural temporary works elements on to adjacent Network rail assets and property Reasons/Mitigations: Where structural collapse of temporary works including scaffolding and access towers could result in any element falling within minimum 4.0m of the railway boundary or a NR asset.</p> <p>Item 9. Issues - Ground induced vibration from demolition, general groundworks, causing displacement to impact on NR Track Support Zone. Reasons/Mitigations: The contractor shall be aware that any proposal which may require works to be carried out adjacent to NR's track support zone and other NR assets at risk of displacement from ground induced vibration as a consequence of the construction equipment and works shall be identified so that required measures to safeguard NR assets in accordance with Network Rail's standard; NR/L2/CIV/177 – Monitoring Track over or adjacent to building or civil engineering works are adopted so that risks to the operational railway are properly addressed.</p> <p>Item 10. Issues - Effects of electrical plant or transformers on Network Rail signals or communications systems due to electromagnetic compatibility. Reasons/Mitigations: The contractor will be required to undertake an Electromagnetic Compatibility (EMC) risk assessments to determine potential impact the project may have on Network Rail assets, where there is a proposal to install (or upgrade existing) transmitter / telecoms equipment located within 100.0m of the operational railway. In addition, any project that will be within 20m (buffer zone) of the operational railway (with OLE) is urged by NR to undertake an EMC (Electro Magnetic Compatibility) and EMF (Electromagnetic Frequency) assessments, carried out in accordance with Network Rail standards 'NR/L1/RSE/30040 & 'NR/L1/RSE/30041' and NR/L2/TEL/30066' for safety assurance in regard to occupants of the proposed developments.</p>
14	<p>No Advertisement Consent granted for signage</p> <p>Notwithstanding the details shown on the drawings and documents hereby approved, no advertisement consent is granted for signage on the application site as a result of the approval of the proposed development. A separate application for Advertisement Consent would be required for the display of any signage in accordance with the requirements of the Town and Country Planning (Control of Advertisements) Regulations (As Amended).</p>

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APPENDIX 2: RELEVANT POLICIES

This appendix lists all relevant development plan policies and guidance notes pertinent to the determination of this planning application.

1 National Guidance

The National Planning Policy Framework 2021 seeks to secure positive growth in a way that effectively balances economic, environmental and social progress for this and future generations. The NPPF is a material consideration and has been taken into account as part of the assessment of these proposals.

- National Planning Policy Framework 2021
- National Planning Practice Guidance (on-line and regularly updated)

2. Development Plan

The Development Plan is comprised of the London Plan 2021, Islington Core Strategy 2011, Development Management Policies 2013, Finsbury Local Plan 2013 and Site Allocations 2013. The following policies of the Development Plan are considered relevant to this application:

A) The London Plan 2021 – The Spatial Development Strategy for Greater London

- Policy GC2- Making the best use of land
- Policy D1- London's form, character and capacity for growth
- Policy D4- Delivering good design
- Policy D5- Inclusive design
- Policy D14- Noise
- Policy E1- Offices
- Policy E2- Providing suitable business space
- Policy HC1- Heritage conservation and growth
- Policy SI 2- Minimising greenhouse gas emission

B) Islington Core Strategy 2011

- Policy CS6 Kings Cross and Pentonville Road
- Policy CS8 Enhancing Islington's character
- Policy CS9 Protecting and enhancing Islington's built and historic environment
- Policy CS10 Sustainable design
- Policy CS11 Waste
- Policy CS13 Employment spaces
- Policy CS18 Delivery and infrastructure

C) Islington Development Management Policies 2013

Design and Heritage

- Policy DM2.1- Design
- Policy DM2.2- Inclusive Design
- Policy DM2.3- Heritage

Employment

- Policy DM5.1- New business floorspace
- Policy DM5.3 - Vale Royal/Brewery Road Locally Significant Industrial Site
- Policy DM5.4 - Size and affordability of floorspace

Energy and Environmental standards

- Policy DM7.1- Sustainable design and construction
- Policy DM7.3 – Decentralised Energy Networks
- Policy DM7.4 – Sustainable Design Standards
- Policy DM7.5 – Heating and Cooling

Transport

- Policy DM8.4- Walking and cycling
- Policy DM8.5- Vehicle parking
- Policy DM8.6 – Delivery and servicing for new developments

Supplementary Planning Guidance (SPG) / Document (SPD)

The following SPGs and/or SPDs are relevant:

London Plan

- Accessible London 2016
- Character and Context 2014
- Sustainable Design and Construction 2014

Islington SPG/SPD

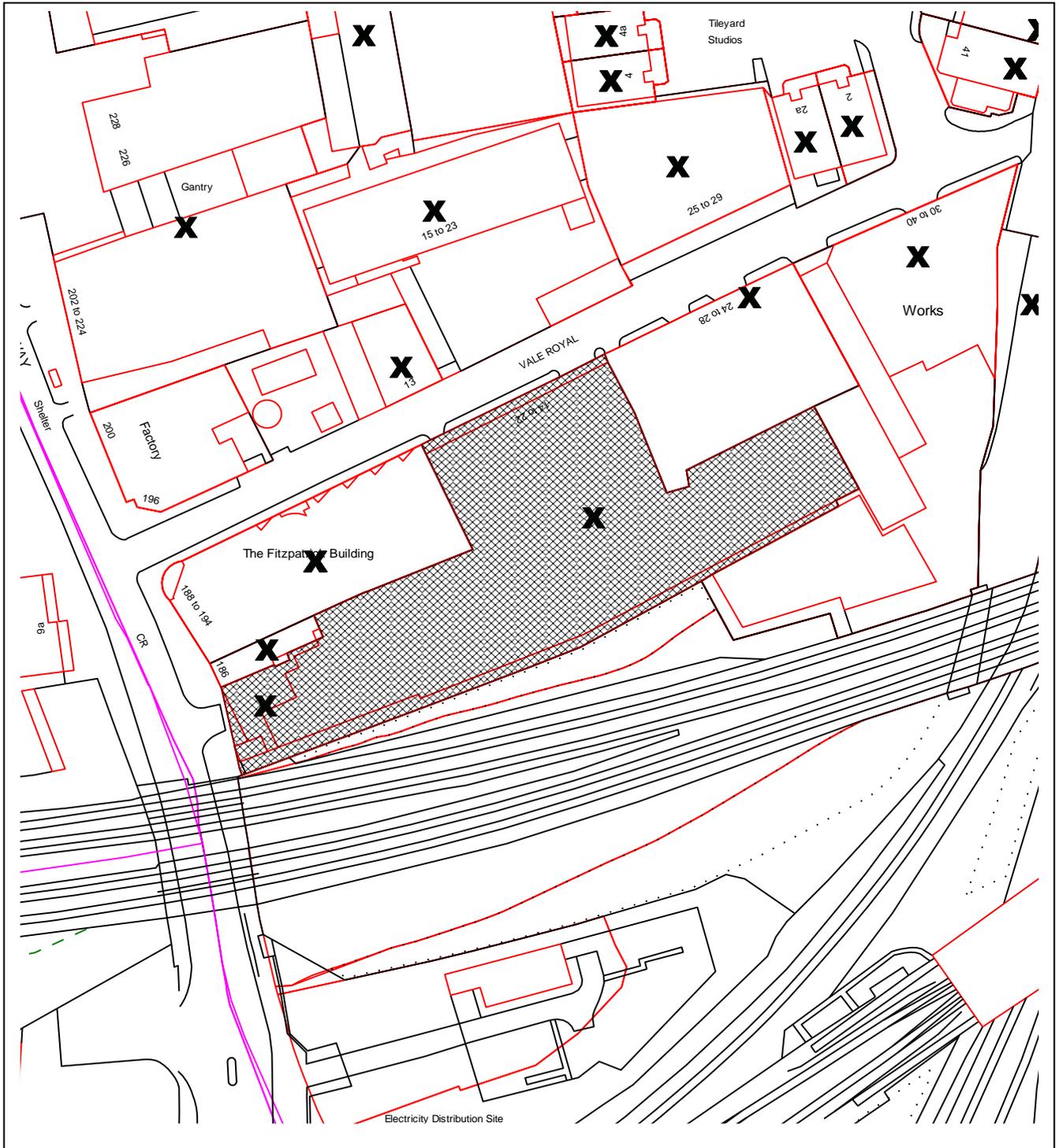
- Urban Design Guide 2017
- Environmental Design 2012
- Inclusive Design SPD

Emerging policies relevant to this application are set out below:

- Policy SP3: Vale Royal/Brewery Road Locally Significant Industrial Site
- Policy SC3 - Health Impact Assessment
- Policy B1 - Delivering business floorspace
- Policy B2 - New business floorspace
- Policy B3 - Existing business floorspace
- Policy B4 - Affordable workspace
- Policy G4 – Biodiversity, landscape design and trees
- Policy G5 – Green Roofs and vertical greening
- Policy S1- Delivering sustainable design
- Policy S2 - Sustainable design and construction
- Policy S4 - Minimising greenhouse gas emissions
- Policy S6 – Managing heat risk
- Policy S7 – Improving Air Quality
- Policy T2 - Sustainable transport choices
- Policy T4 – Public realm
- Policy T5 - Delivery, servicing and construction
- Policy DH1 - Fostering innovation and conserving and enhancing the historic environment
- Policy DH2 - Heritage assets
- Policy DH3 – Building heights

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